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NOVA SCOTIA
REGISTRY OF SHIPPING:

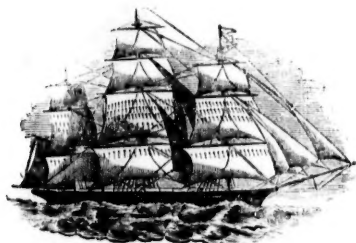
WITH
STANDARD RULES
FOR
CONSTRUCTION AND CLASSIFICATION.

COMPILED FROM OFFICIAL RETURNS,

BY

THOS. R. DEWOLF,
FIRST CLERK IN THE OFFICE OF THE FINANCIAL
SECRETARY.

PUBLISHED BY ORDER OF THE GOVERNMENT.



HALIFAX, N. S.
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1866.

Province of }
Nova-Scotia. }

BE IT REMEMBERED, That on this Eleventh day of December, in the Year of Our Lord One Thousand Eight Hundred and Sixty-five, THOMAS R. DEWOLF, of the City of Halifax, in the Province aforesaid, has deposited in this Office the title of a work, the copyright whereof he claims in the words following :

NOVA SCOTIA REGISTRY OF SHIPPING,

WITH

STANDARD RULES FOR CONSTRUCTION AND CLASSIFICATION,

In conformity with Chapter One Hundred and Sixteen of the Third Series of the Revised Statutes.

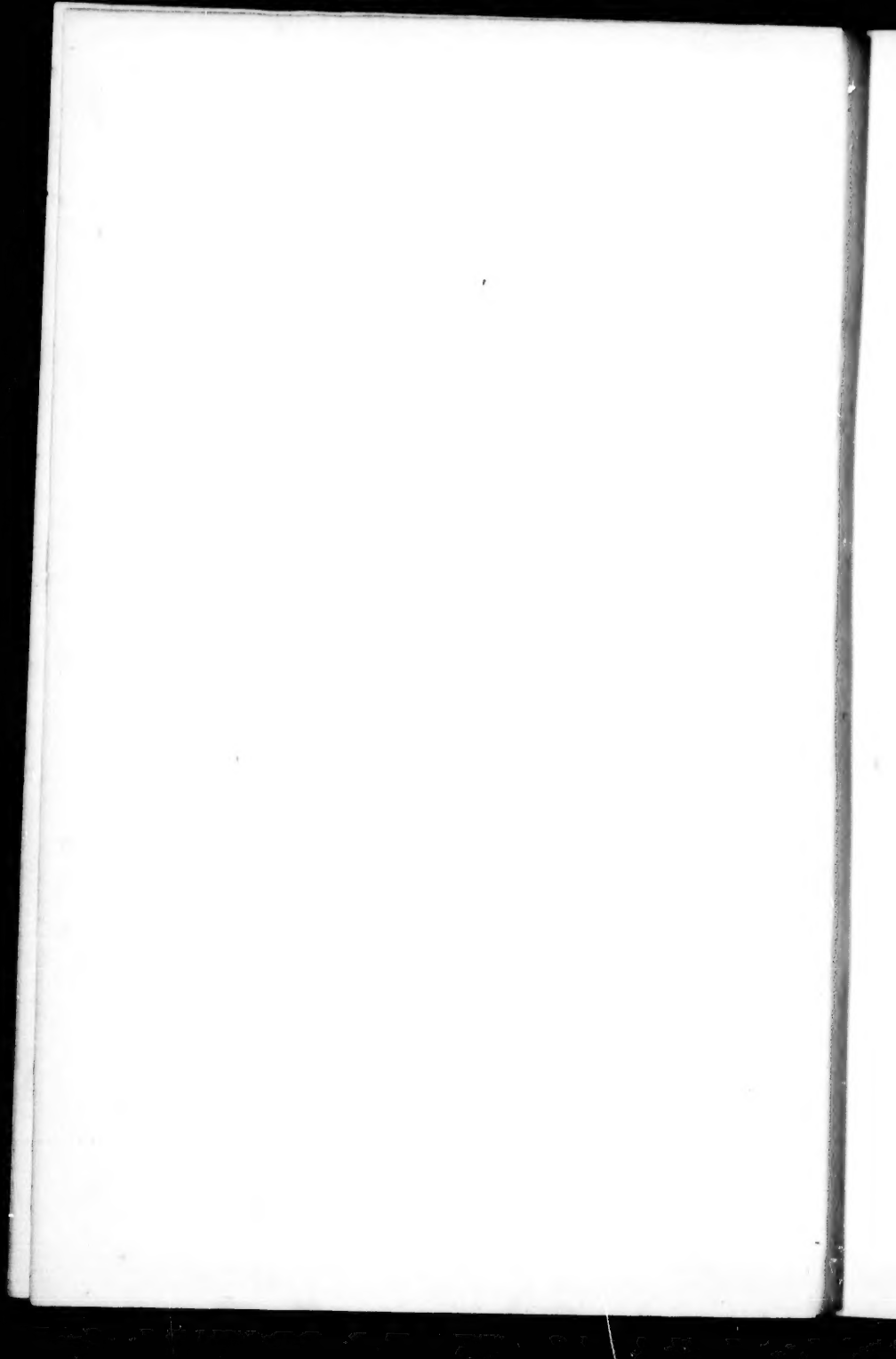
JAMES H. THORNE,
Deputy Provincial Secretary.

PROVINCIAL SECRETARY'S OFFICE. }
11th December, 1865. }

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PREFACE.

THE requirements for a work of this kind have long been felt by the Ship-Owners, Underwriters, and all persons interested in commercial pursuits; and it is hoped that the present work will, to a great extent, supply the deficiency. For the information of Shipbuilders, extracts from Lloyd's Rules, (English and American) have been added, with explanatory tables and plates, to which particular attention is directed, especially where the larger descriptions of vessels are built. From the almost insular position of Nova Scotia, maritime pursuits form the occupation of a large portion of the inhabitants, and as inexhaustible supplies of fish abound in the adjacent waters, a large proportion of the vessels built in this Province are intended expressly for this branch of industry, and are consequently of small tonnage. There may have been vessels sold or lost, or a change taken place in the ownership, and not reported to the Registrar at the date of the Returns from which this publication is compiled (30th September, 1865); yet it will be found sufficiently accurate for practical purposes. An Annual Return of all additional vessels, or vessels struck off, and changes in ownership, will be published, which may be incorporated into the body of this work, space having been left for that purpose. Imperfections are left to the consideration of an indulgent public.



REGISTRY OF NOVA SCOTIA SHIPPING.

STANDARD RULES FOR CONSTRUCTION AND CLASSIFICATION.

[FROM AMERICAN LLOYD'S REGISTRY OF SHIPPING.]

RULE I.

THE FIRST CLASS OR STANDARD

Will comprise vessels built of live or white oak frames, and locust, cedar, or white oak rail stanchions, with tops mixed with locust, red cedar, hachmatack, or white heart chestnut, having live, or white oak principal pieces; the timber butts distributed out of line; the timber scarphs not less than four and a half feet long, the floor timbers extending well towards the ends of the vessel, and the heels of cants stepped in the dead woods and bolted through.

RULE II.

STEM AND STERN POSTS.

The heel of stem must be a crook, and stepped in a hooked scarph on the keel, not less than three feet six inches long. The stern post must also be stepped in the keel.

TIMBERING ROOM.

All vessels of eight hundred tons and under must not be over twenty-two inches to centres; and all vessels over that tonnage not over thirty inches.

DECKS.

Spar decks to be of white or yellow pine, and no planks to be over five and a half inches wide and three inches thick, fastened with two spikes in the butt of each plank.

FLOORS AND FUTTOCKS.

The rising floors and the second futtocks, natural crooks preferred.

STANCHIONS

In the hold must be of oak or pitch pine, and securely fastened with oak knees or iron straps on the keelson, and be iron strapped to the beams. A screw bolt to pass through the between deck stanchions and beams, secured with a heavy washer at each end, or to be strapped on each end.

RULE III.

OUTSIDE PLANK.

From keel to strings to be of white oak of good quality. The first garboard streak to be at least three inches, and the second at least one and a half to two inches thicker than the rest of the covering. In all cases the nibs of the butt scarphs must be one-third the width of the planks, and reach the frames forward and aft of their centres. No butts on any part of the planking to be nearer than five feet to each other, unless there be a streak wrought between them, when a distance of four feet will be allowed; and all butts on the same timber must have three streaks between, as more particularly shown in diagram annexed.

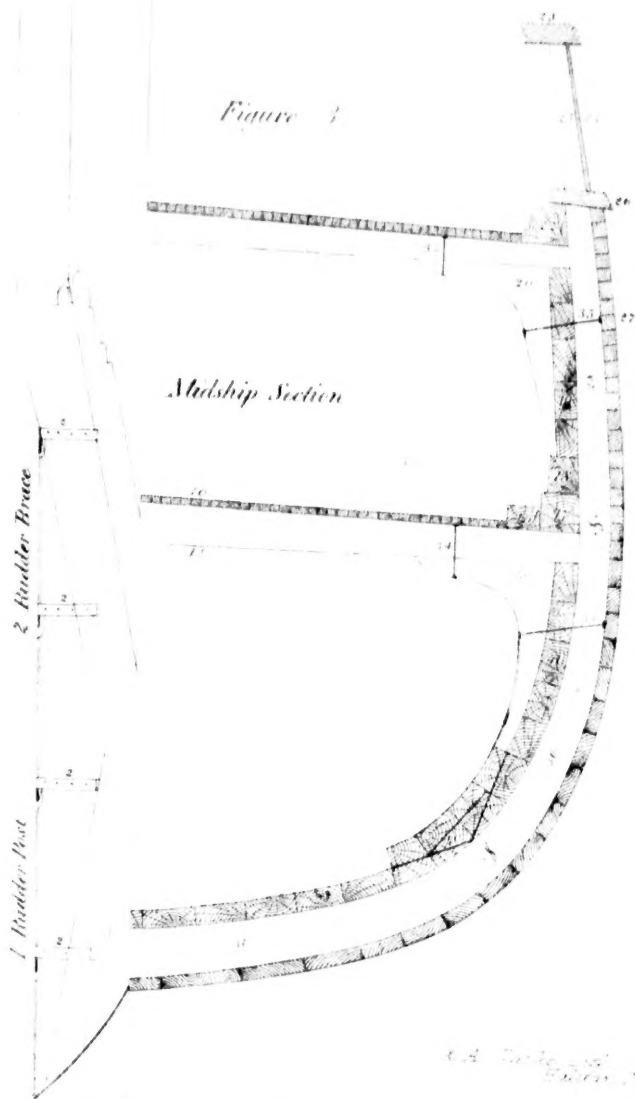
SKETCH DESCRIPTIVE OF THE REQUIRED SHIFTING OF PLANK.

| | |
|--|--|
| No butts to be nearer than five feet to each other, | |
| <i>Five feet,</i> | unless there be |
| a Streak wrought between them, | |
| <i>Four feet,</i> | and then a distance of four feet will be allowed. |
| And all Butts on the same timber shall have three Streaks between. | |
| | |
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| | |

This sketch shows the principle on which the Butts should be arranged, to avoid stepping, which is deemed bad workmanship.

0 Tons Scale 48

Figure 1 Rudder.



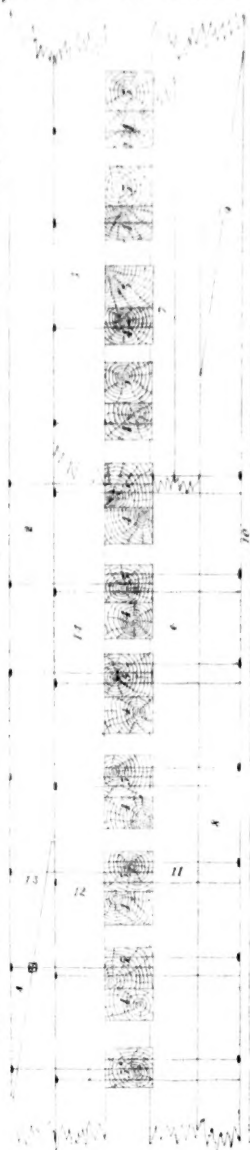
Sections of Principal parts of a Ship abo

Figure 1 Rudder



- 1 Rudder Post
- 2 Rudder Brace
- 3 Rudder Post
- 4 First Footboard
- 5 Floor Timber
- 6 Main keel
- 7 First deck Garboard
- 8 Lower keel
- 9 First Scarph
- 10 Stow
- 11 Floor Timber Bolt
- 12 Main keel Bolt
- 13 Ruler below Bolt
- 14 Main keel Bolt
- 15 Data represent Bolt

Figure 2. Foundation



- 1 Shear
- 2 Lower keel
- 3 Main keel
- 4 Main keel
- 5 Ruler below
- 6 Ruler below
- 7 Between Ruler below
- 8 Shear Deck
- 9 Shear Deck Beam
- 10 Main Deck
- 11 Main Deck Beam
- 12 Assistant below
- 13 Sealing on flat of floor
- 14 Edge Scarph
- 15 Between Ruler below
- 16 Inner Water Way
- 17 Main Water Way
- 18 Upper Water Way
- 19 Upper Clamps
- 20 Upper Hanging Piece
- 21 Thick Deck Plank
- 22 Shear Deck Water Way
- 23 Plank
- 24 Bottom Plank
- 25 3rd Footboard
- 26 1st Footboard
- 27 Thick backboard
- 28 Plank Shear
- 29 Thick backboard
- 30 Plank Shear
- 31 Thick backboard
- 32 2nd Footboard
- 33 Thick backboard
- 34 Plank Shear
- 35 Thick backboard
- 36 Plank Shear
- 37 Thick backboard
- 38 Plank Shear
- 39 Thick backboard
- 40 Plank Shear
- 41 Lower Clamps

Bolt represent frame Bolts.

1. Main Deck
2. Main Deck
3. Main Deck
4. Main Deck
5. Main Deck
6. Main Deck
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99. Main Deck
100. Main Deck

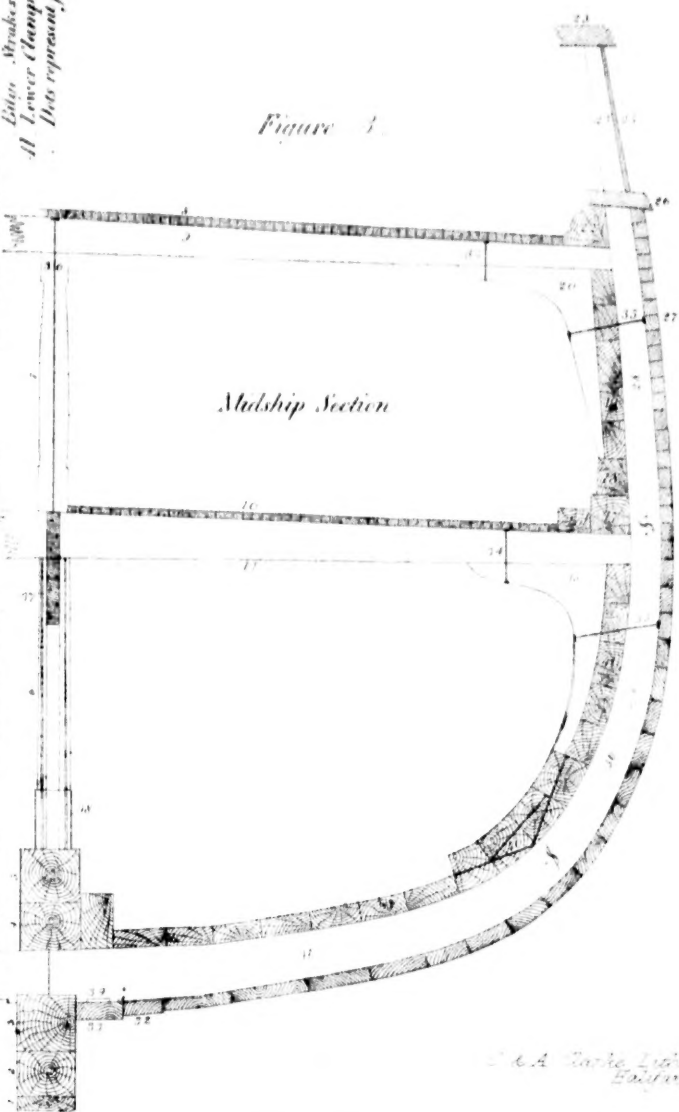
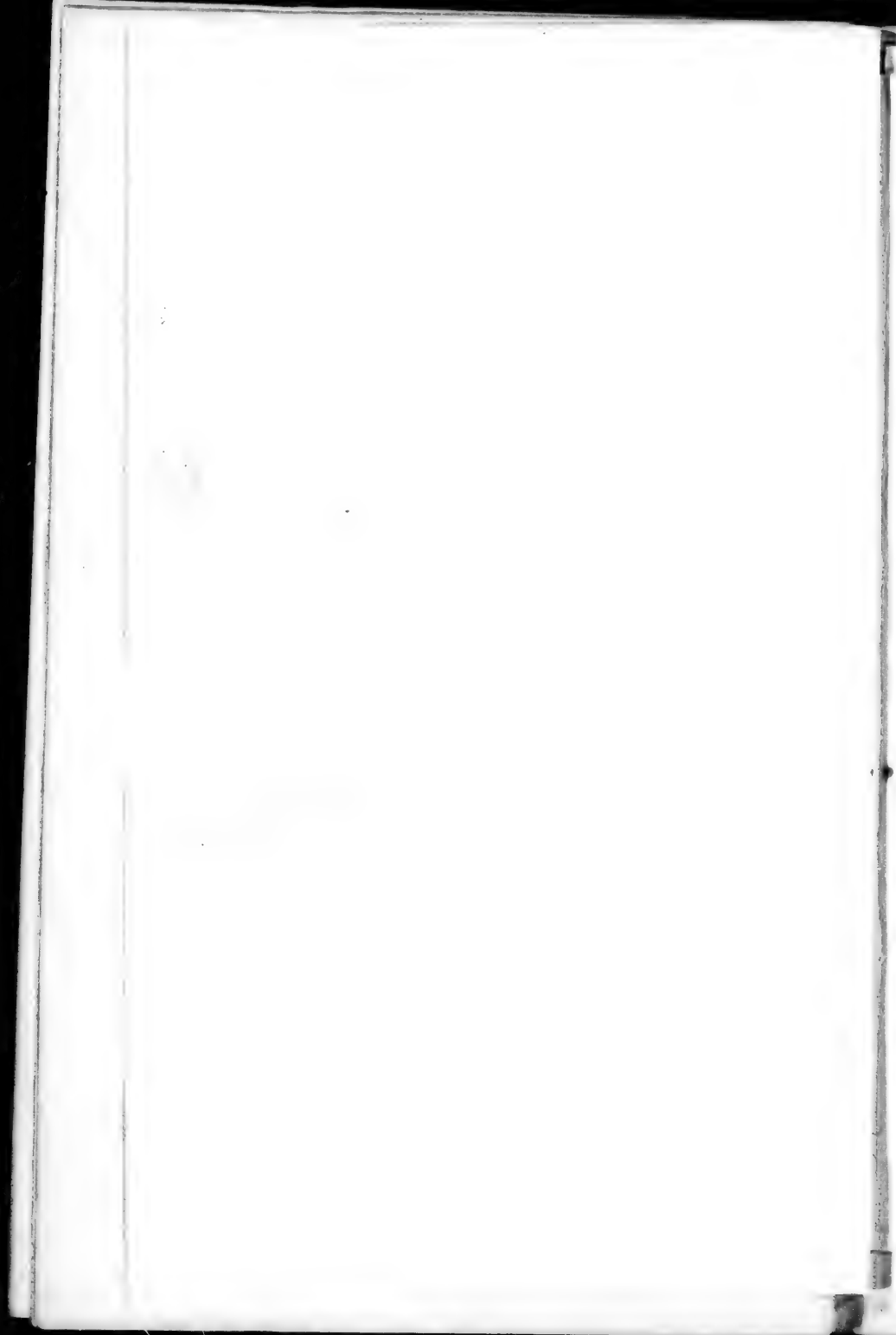


Figure 1

41 Lower Clamps
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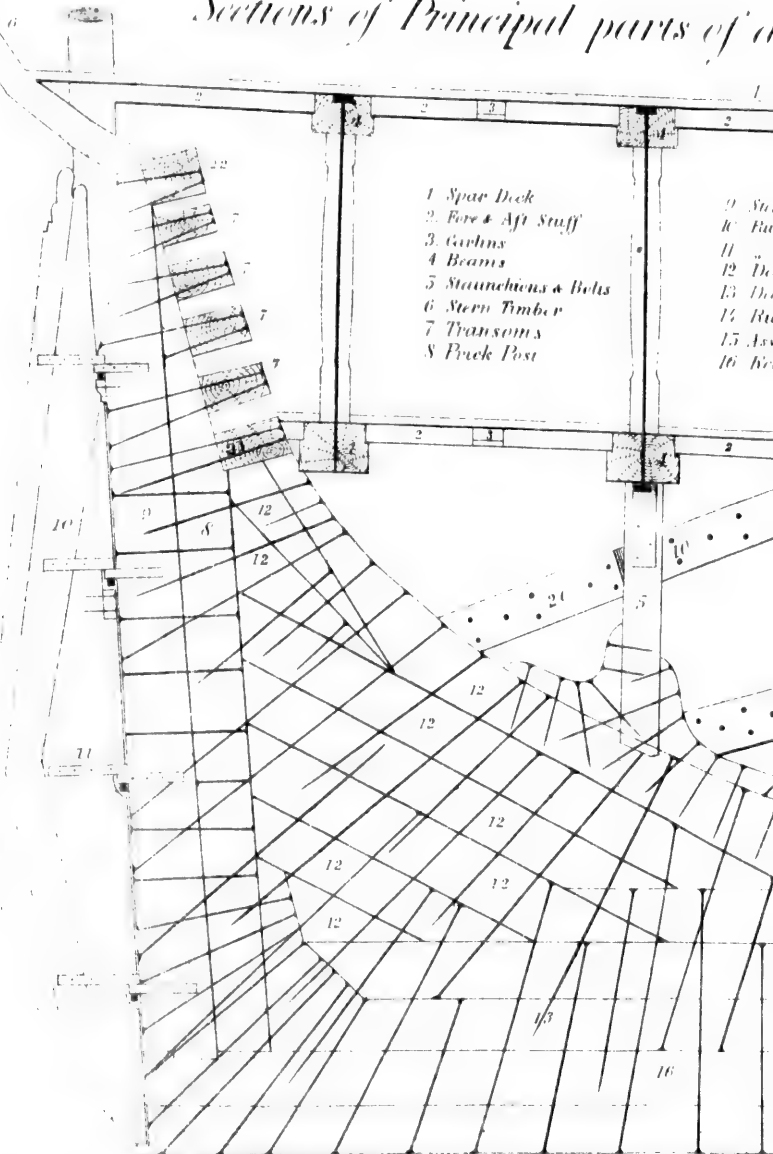
C. A. Cooke, Jr.
Baltimore, Md.



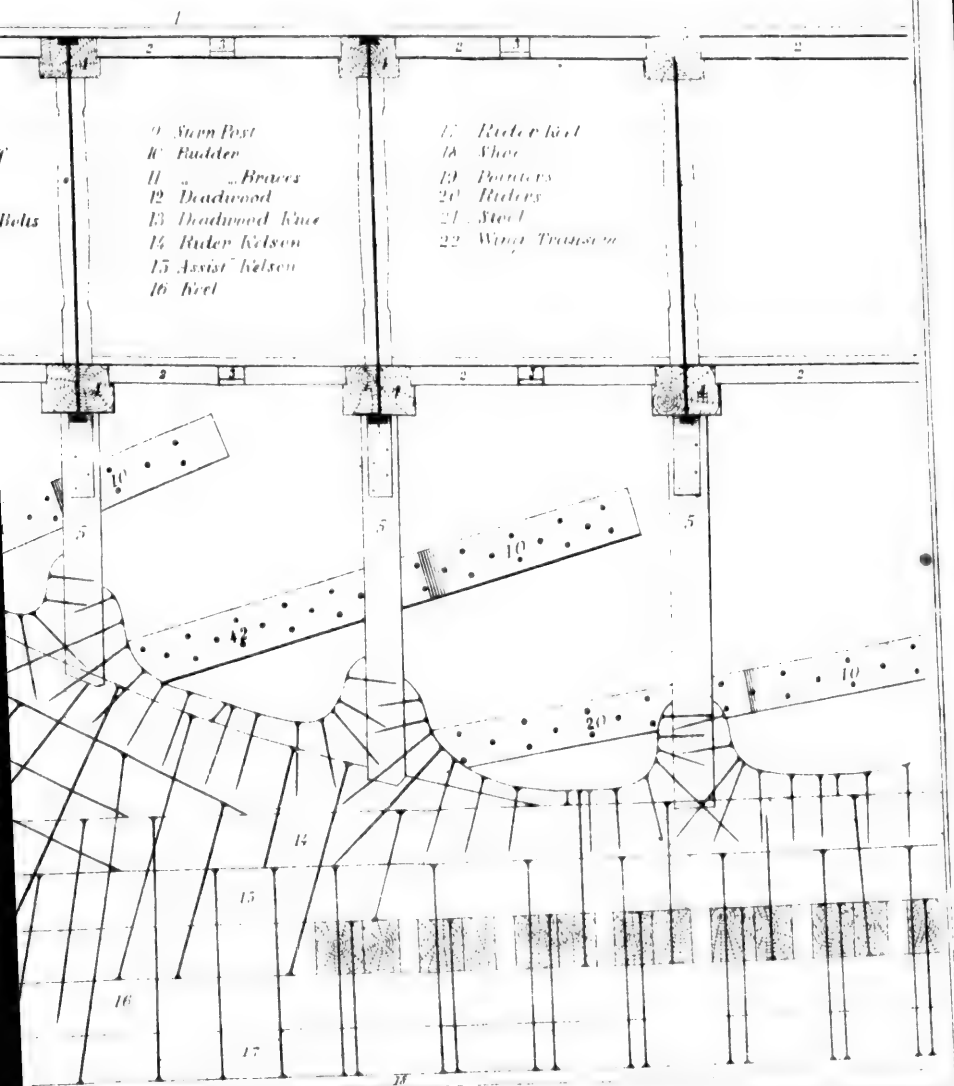
Sections of Principal parts of a

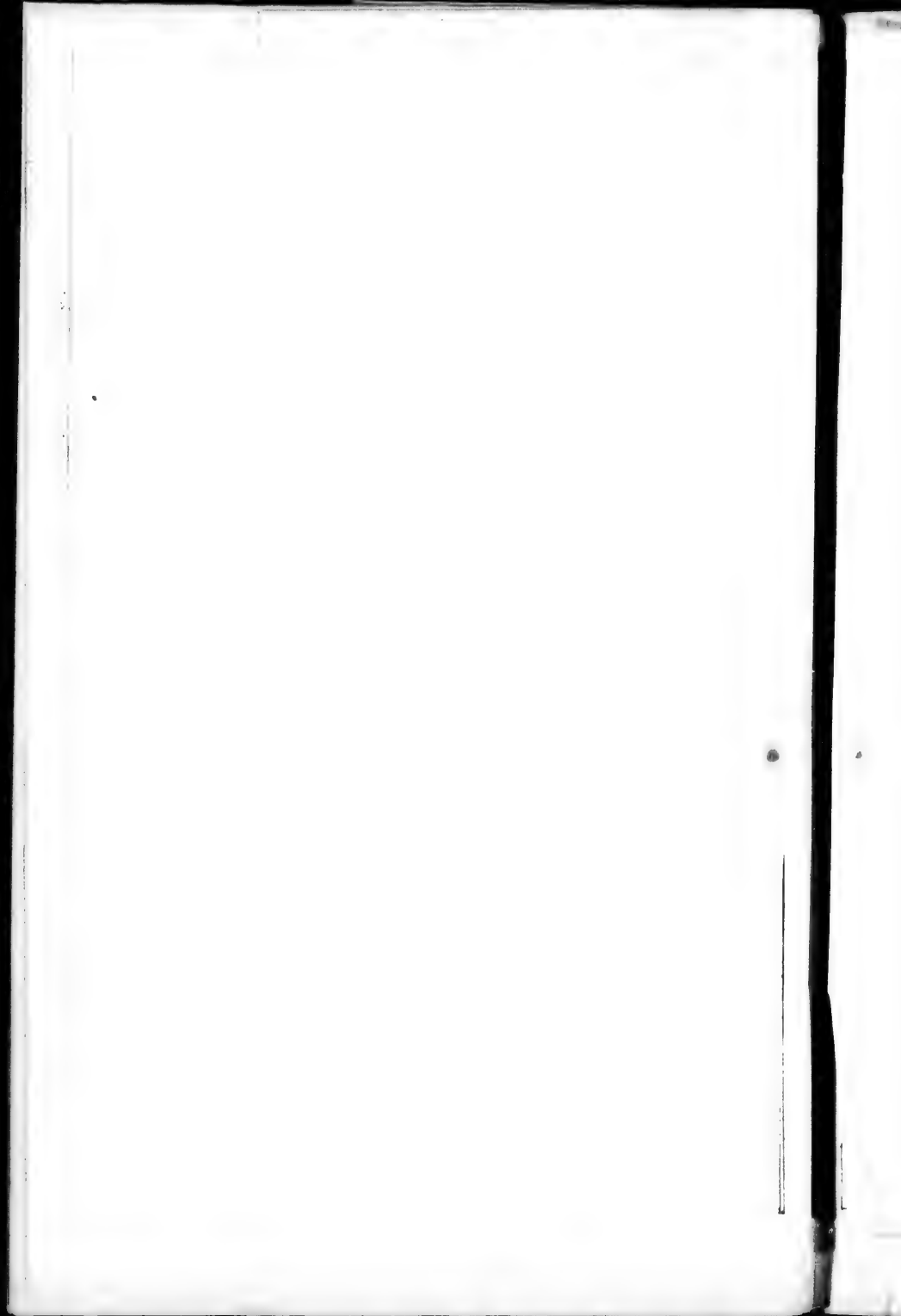
- 1 Spar Deck
- 2 Fore & Aft Stuffs
- 3 Girders
- 4 Beams
- 5 Stanchions & Belts
- 6 Stern Timber
- 7 Transoms
- 8 Back Post

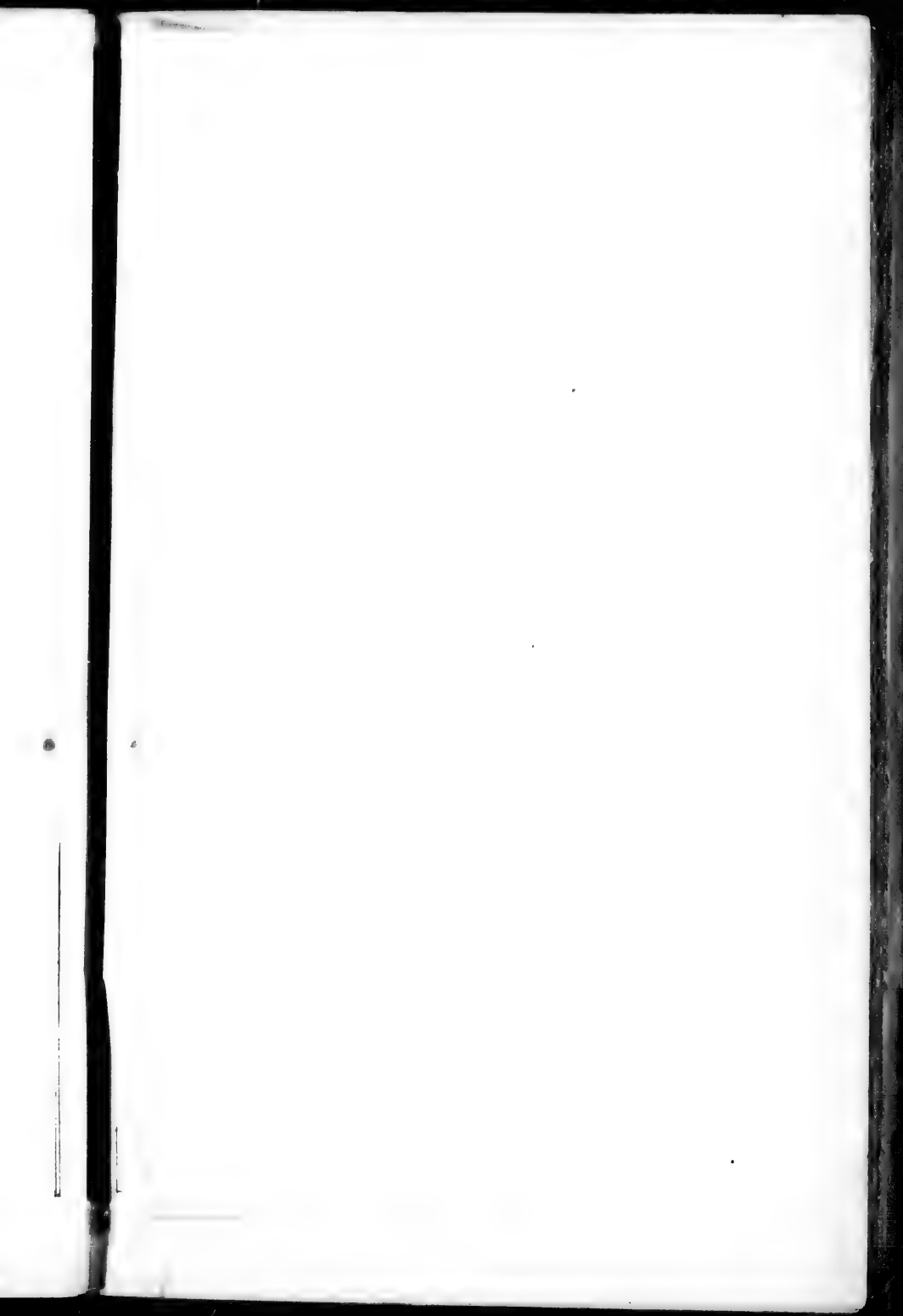
- 9 Stanchions
- 10 Ribs
- 11 Decks
- 12 Hull
- 13 Ribs
- 14 Ribs
- 15 Aft
- 16 Bow



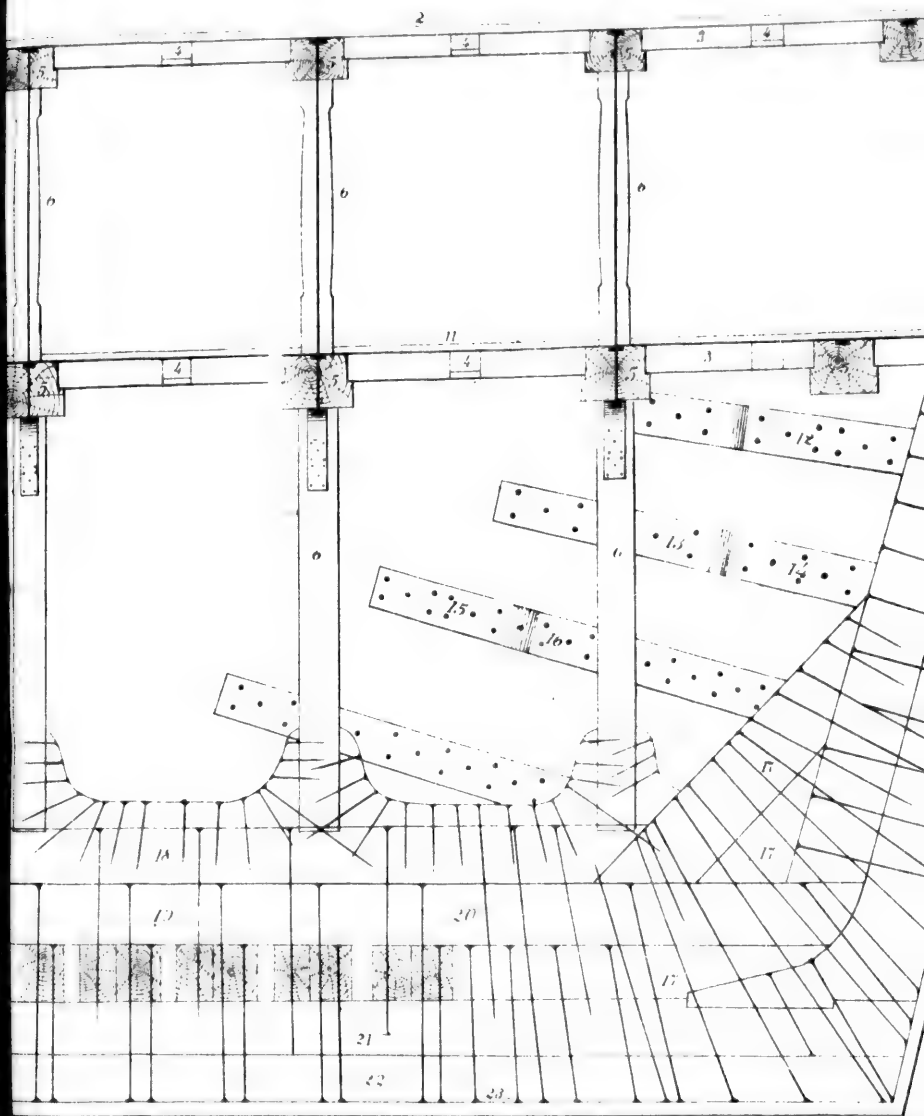
Notes



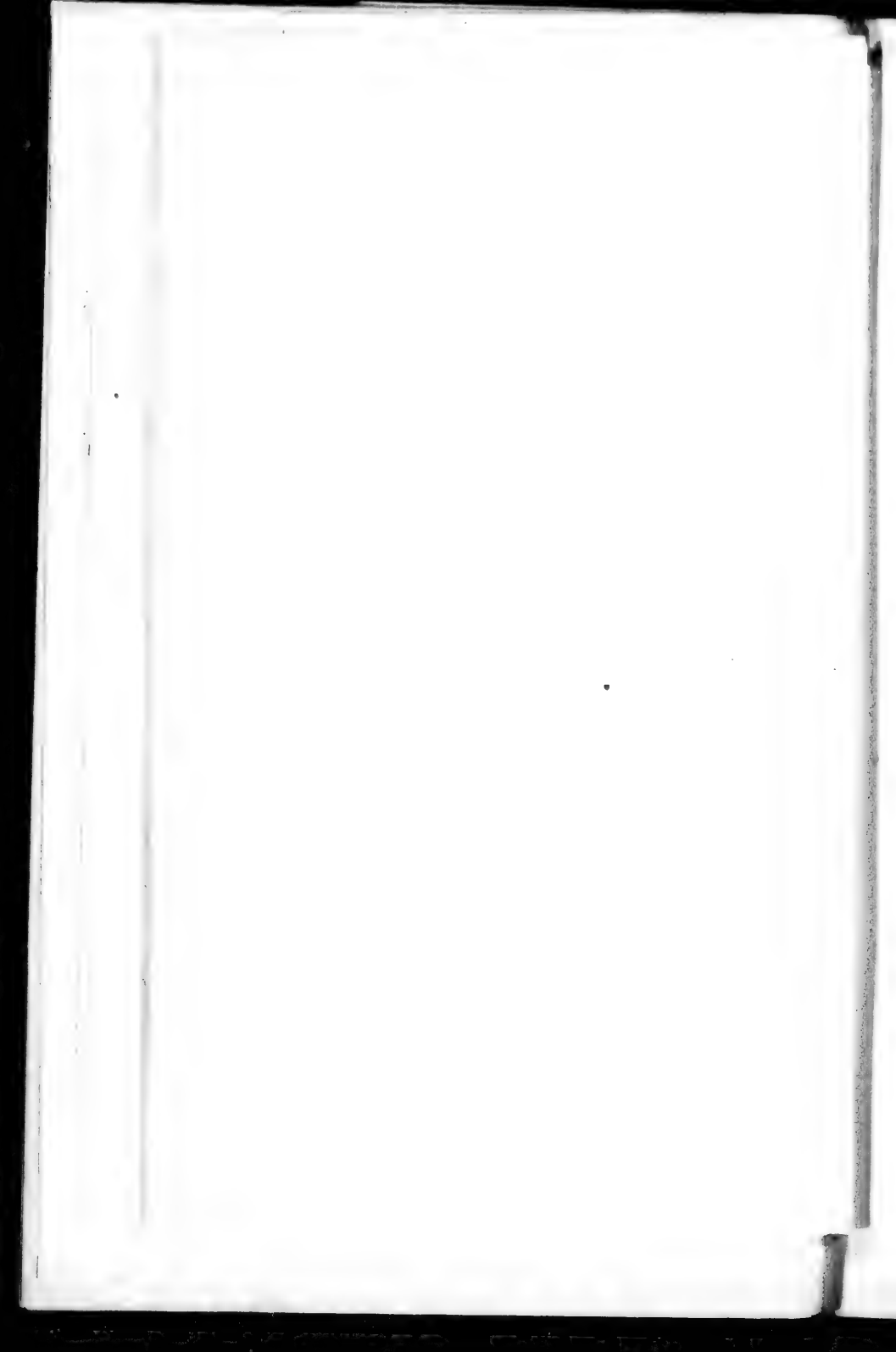




Sections of Principal parts of a Ship about 800 Tons



- 1 Rail
- 2 Spar Deck
- 3 Fore & Aft Stuffs
- 4 Carls
- 5 Beams
- 6 Stanchions & Screw Bolts
- 7 Trial Board
- 8 Outwater
- 9 Stem
- 10 Apron
- 11 Stern Deck
- 12 Breast Hook
- 13 Painter
- 14 Breast Hook
- 15 Painter
- 16 Rider
- 17 Deadwood
- 18 Rider Kelson
- 19 Assistant Kelson
- 20 Main Kelson
- 21 Kiel
- 22 Rider Kiel
- 23 Sheer



Vessels under two hundred tons will be exempted from the full operation of this rule, but a literal compliance will be insisted upon in ships of larger tonnage.

RULE IV.

INSIDE PLANK.

Clamp scarphs to be hooked or keyed, and arranged to suit timbers, and length not less than five times the width of the plank. The ceiling to be of white oak or yellow pine of good quality, the butts properly shifted, and the whole well fastened, and the edges bevelled to good calking seams.

RULE V.

BEAMS AND KNEES.

The beams of oak or yellow pine, sufficient in size, and not more than six feet from centres, except for hatchways, and securely fastened to the side by knees. All knees in the hold to be oak; main or spar deck knees oak or haematack; the lodger knees to be carefully fitted to the timbers, and the bosom knees to reach to and fit in the hollow of the belly of lodger knees. All vessels of one to two hundred tons to have a hanging knee under each end of each alternate beam; if over two hundred tons, to have a hanging knee under each end of each beam. The arms of the knees for vessels of five hundred tons and under must be three feet long, and for all vessels over five hundred tons, three feet six inches. In addition to the bolts driven from the outside, at least two bolts, not over one foot from the end of the arm, must be driven from the inside, and clinched on iron plates on the top of the beams.

RULE VI.

KEELS AND KEELSONS.

The keel always to be sided sufficiently large to admit of twice the thickness of the outside plank between the rabbets on the stem and stern-post; the rabbet to run as far as practicable on the stem, and at least six inches on the stern-post, to admit of fastening the wood ends thereto. The keelson never to be sided less than size of keel, and the scarphs not less than seven feet long; and if the

keelson is single—as it may be for a vessel of one hundred and fifty tons—the shifts of scarphs to be not less than one-third the length of the stick from the end of the scarph of keel. Where rider keelsons are added, the scarph must not be less than six feet long, and the shifts not less than one quarter the length of the respective sticks.

Assistant keelsons being of great advantage, inasmuch as they fit to the heels of the first futtocks, and are bolted thereto, in addition to the fastening they have by being bolted to the main keelson, thus adding materially to the strength of the vessel.

RULE VII.

MINIMUM SCANTLING OF FRAMES, BEAMS, KNEES, AND PLANK, IN INCHES, FOR VESSELS FROM 100 TO 2000 TONS.

| Tons. | Floors. | Futtock. | Beams. | HANGING KNEES. | | HORIZONTAL KNEES. | | Deck Plank. | Bottom Plank. |
|-------|---------|----------|--------|----------------|-------------|-------------------|-------------|-------------|---------------|
| | | | | Upper Deck. | Lower Deck. | Upper Deck. | Lower Deck. | | |
| 100 | 9 | 5 | 10½ | — | — | 4½ | — | 3 | 3 |
| 150 | 10 | 6 | 10½ | 6 | — | 5 | — | 3 | 3 |
| 200 | 10 | 7 | 11½ | 6½ | — | 5 | — | 3 | 3 |
| 300 | 11 | 9 | 11½ | 6½ | — | 5 | — | 3 | 3 |
| 350 | 11 | 9 | 12½ | 6½ | 7 | 5½ | 6 | 3½ | 3 |
| 400 | 11 | 9 | 13 | 7 | 7 | 5½ | 6 | 3½ | 3½ |
| 500 | 12 | 9 | 13½ | 7 | 7 | 5½ | 6 | 3½ | 3½ |
| 600 | 12 | 9 | 14 | 7 | 8 | 6 | 6½ | 3½ | 3½ |
| 700 | 12 | 9 | 14½ | 7 | 8 | 6½ | 7 | 3½ | 3½ |
| 800 | 12 | 9½ | 15 | 7½ | 8½ | 6 | 7 | 3½ | 4 |
| 900 | 12 | 10 | 16 | 8 | 9 | 6 | 7 | 3½ | 4 |
| 1000 | 12 | 10 | 17 | 8½ | 10 | 6 | 7 | 3½ | 4 |
| 1200 | 12 | 10 | 17 | 8½ | 10 | 6 | 7 | 3½ | 4 |
| 1400 | 12½ | 10 | 18 | 8½ | 11 | 6 | 7 | 3½ | 4 |
| 1600 | 13 | 10 | 19 | 9 | 11½ | 6 | 7 | 4 | 4½ |
| 1800 | 13 | 10 | 20 | 9½ | 12 | 6½ | 7½ | 4 | 4½ |
| 2000 | 14 | 11 | 22 | 10 | 14 | 7 | 8 | 4 | 5 |

The middle deck beams and knees to be the same in size and materials as the lower deck.

RULE VIII.

MINIMUM MOULDING SIZE OF BEAMS AND FRAMES, IN INCHES, FOR
VESSELS FROM 100 TO 2000 TONS.

| TONS. | UPPER DECK BEAMS. | | LOWER DECK BEAMS. | | FRAMES. | |
|-------|-------------------|----------|-------------------|---------|------------------|----------------------|
| | Ends. | Centres. | Ends. | Centres | Plank Shears. | Floors at Throat. |
| 100 | 5½ | 6 | .. | .. | 4½ | 8 |
| 150 | 6½ | 7 | .. | .. | 5 | 9 |
| 200 | 7 | 8 | .. | .. | 5 | 10 |
| 300 | 7 | 8 | 7 | 8½ | 5½ | 10½ |
| 350 | 7 | 8 | 7 | 8½ | 5½ | 11 |
| 400 | 7 | 8½ | 7 | 9 | 5½ | 11½ |
| 500 | 7 | 9 | 7½ | 10 | 6 | 12 |
| 600 | 7½ | 9½ | 8 | 11 | 6 | 12½ |
| 700 | 8 | 10 | 8½ | 12 | 6 | 13½ |
| 800 | 8 | 10 | 9 | 12½ | 6½ | 14½ |
| 900 | 8 | 10 | 9½ | 13½ | 6½ | 15 |
| 1000 | 8 | 11 | 10 | 15 | 6½ | 16 |
| 1200 | 8½ | 11½ | 10½ | 16 | 6½ | 17 |
| 1400 | 8½ | 11½ | 10½ | 16 | 6½ | 18 |
| 1600 | 8½ | 11½ | 10½ | 16 | 7 | 19 |
| 1800 | 8½ | 11½ | 11 | 16½ | 7½ | 20 |
| 2000 | 9 | 12 | 11 | 17 | 8 | 21 |

RULE IX.

FASTENING OUTSIDE PLANKS.

All blunt bolts going into the timbers to be driven within one inch of through, the plank well wrought to the timbers with copper or composition spikes and square fastened with locust treenails of good quality, driven through and properly wedged both outside and in. Where plank are less than six inches wide, then to be treenailed single and double in each frame, and no deviation from this rule will be allowed in first-class vessels, except where the treenails strike iron fastenings, knees or water-ways. Each plank must be fastened with at least one-headed copper or composition bolt, driven through the first frame forward and aft of the butt, and clinched on composition rings on the inside; the wood ends forward and aft fastened with at least one headed copper or composition bolt driven through where practicable, and clinched on rings. Clamps, thick streaks, and ceiling square fastened throughout.

KNEES.

Each hanging knee keyed to beams, in addition to the through bolts, driven from the outside and clinched inside, and the blunt bolts driven from the inside, must have at least two bolts driven through the arm not more than one foot from the end and clinched on iron plates on top of the beams.

BREAST HOOKS AND POINTERS.

Must be square fastened and one-third of the bolts driven from the outside through the timbers, and clinched over rings on the inside; all blunt bolts to be driven within one inch of through.

Copper or composition bolts must be driven through the apron and inner stern-post, not over twenty inches apart, and clinched over composition rings on the outside.

Copper or composition bolts not over twenty inches apart, must be driven through the dead-wood, passing through the heels and scarphs of stem and stern-post, and clinched over composition rings on the outside of the same and the lower side of the keel.

RULE X.**FASTENING OF FLOORS, KEELSONS, AND KEEL.**

Each alternate floor to be fastened to the keel with a copper bolt driven from above, and clinched on the underside of the keel. The intermediate floors to be fastened with a copper bolt driven through the main keelson and clinched on the underside of the keel, and be cross-fastened with one iron-bolt in each floor, driven through the keelson to within one inch of the under side of the keel. If a rider keelson is added, then cross-fastened with one iron bolt in each floor, through the rider to within one inch of the lower part of the keel. If the vessel has a double keel, then through the upper and three inches into the lower keel. The vertical bolts in the assistant keelson to be driven up through the first futtock and the horizontal bolts, as per plate No. 5, figure 2.

GARBOARD STREAKS.

First and second garboard streaks bolted to the floor with one copper or composition bolt driven through each frame and clinched, in addition to treenailing.

RULE XI.

RAIL SCARPHS

Must be hooked or keyed, and in length five times their width.

SPAR DECK WATERWAYS.

Fayed to the beams and timbers, and at least three plank next the waterway must be one inch thicker than adjoining plank, and alternately let one inch into and one inch over the beams and earlines, and edge bolted between beams and earlines through waterway and timbers and clinched.

LOWER DECK WATERWAY.

The main and inner waterway coggled to beams with three inch locust, must have one bolt driven through each beam and clinched, and one bolt driven from outside through each timber into main and inner waterway and clinched.

KEELSON STICKS.

To be of good length, and same in depth as width.

RULE XII.

CAULKING.

The oakum thread fifty feet in length to one pound in weight, and to be properly coiled in the seam, not less than one thread to each half inch of thickness of plank.

TREENAILS.

Long Island and Connecticut locust.

NOTE. — Minimum average length of deck plank thirty-five feet.

RULE XIII.

MINIMUM SIZE, IN INCHES, OF BOLTS FOR VESSELS FROM
100 TO 2000 TONS.

| Tons. | Floors. | Dead Wood. | Aprons. | Inner Stern Post. | Keelsons. | Thick strake. | Ceiling. | Breast Hooks. | Pointers. | Hanging knees. | Horizontal knees. | Butts, Wood Ends. |
|-------|-----------------|-----------------|----------------|-------------------|----------------|----------------|----------------|----------------|-----------------|----------------|-------------------|-------------------|
| 100 | $\frac{1}{2}$ | $\frac{1}{2}$ | $\frac{1}{2}$ | $\frac{1}{2}$ | $\frac{1}{2}$ | $\frac{1}{2}$ | $\frac{3}{16}$ | $\frac{1}{2}$ | $\frac{1}{2}$ | $\frac{1}{2}$ | $\frac{1}{2}$ | $\frac{1}{2}$ |
| 200 | 1 | 1 | 1 | 1 | 1 | $\frac{1}{2}$ | $\frac{3}{16}$ | ... | 1 | $\frac{1}{2}$ | $\frac{1}{2}$ | |
| 250 | 1 | 1 | 1 | 1 | 1 | " | $\frac{3}{16}$ | $\frac{1}{2}$ | 1 | " | " | |
| 300 | 1 | 1 | 1 | 1 | 1 | " | $\frac{1}{2}$ | " | 1 | " | " | |
| 400 | 1 | 1 | 1 | 1 | $1\frac{1}{2}$ | " | " | $\frac{3}{16}$ | $\frac{1}{2}$ | " | " | $\frac{1}{2}$ |
| 500 | $1\frac{3}{16}$ | $1\frac{3}{16}$ | $1\frac{1}{2}$ | $1\frac{1}{2}$ | " | $\frac{1}{2}$ | " | " | " | " | " | " |
| 600 | " | " | " | " | " | " | $\frac{1}{2}$ | $1\frac{1}{2}$ | $1\frac{3}{16}$ | " | " | " |
| 700 | " | " | " | " | " | " | " | " | " | " | " | " |
| 800 | " | " | " | " | $1\frac{1}{2}$ | " | " | " | " | " | " | " |
| 900 | " | " | " | " | " | " | " | " | " | " | " | " |
| 1000 | $1\frac{1}{2}$ | $1\frac{1}{2}$ | $1\frac{1}{2}$ | $1\frac{1}{2}$ | " | 1 | $\frac{1}{2}$ | $1\frac{1}{2}$ | $1\frac{1}{2}$ | 1 | $\frac{1}{2}$ | " |
| 1800 | $1\frac{1}{2}$ | $1\frac{1}{2}$ | $1\frac{1}{2}$ | $1\frac{1}{2}$ | $1\frac{1}{2}$ | 1 | " | $1\frac{1}{2}$ | $1\frac{1}{2}$ | $1\frac{1}{2}$ | " | $\frac{1}{2}$ |
| 2000 | " | " | " | " | " | $1\frac{1}{2}$ | " | " | " | $1\frac{1}{2}$ | " | " |

WEIGHT PER CUBIC FOOT—TIMBER.

| | Lbs. | | Lbs. |
|------------------|------|------------------|------|
| Teak Wood..... | 86 | Ash..... | 41 |
| Lignum Vite..... | 84 | Elm..... | 36 |
| Live Oak..... | 82 | Walnut..... | 35 |
| Mahogany..... | 65 | Red Cedar..... | 34 |
| Locust..... | 60 | Yellow Pine..... | 34 |
| Black Oak..... | 50 | Chestnut..... | 33 |
| White Oak..... | 49 | Hemlock..... | 31 |
| Red Oak..... | 47 | White Pine..... | 30 |
| Maple..... | 46 | Spruce..... | 29 |

Wood Ends,

18

12

"

"

"

"

"

"

4

"

Lbs.

.41

.36

.35

.34

.34

.33

.31

.30

29

Principal Parts and Fastnings of a Ship about 800 Tons

Scale 2 4

Figure 1.

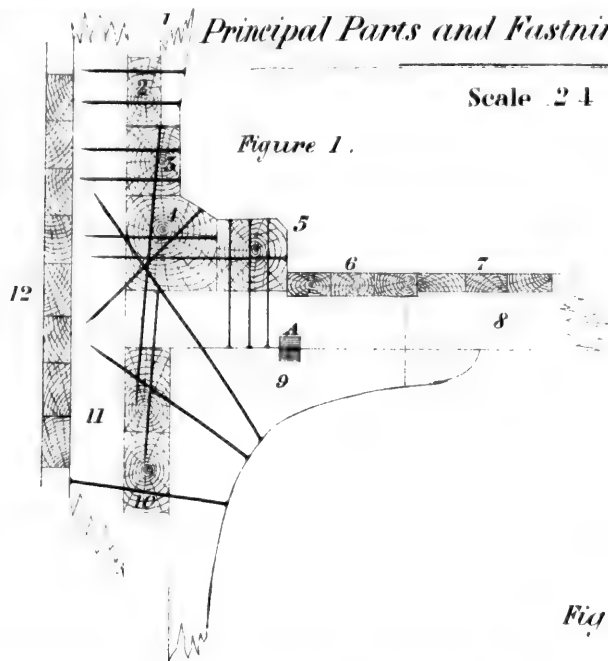


Figure 2.

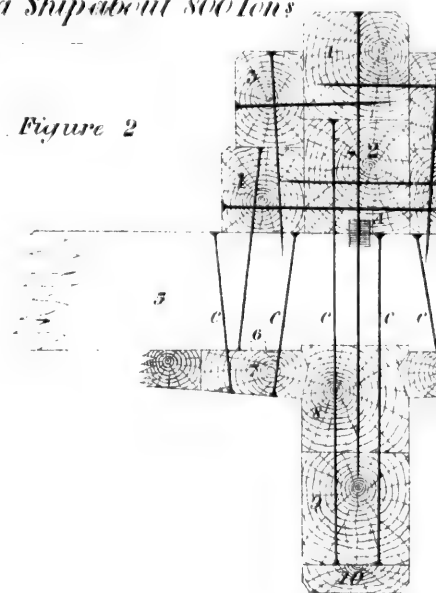
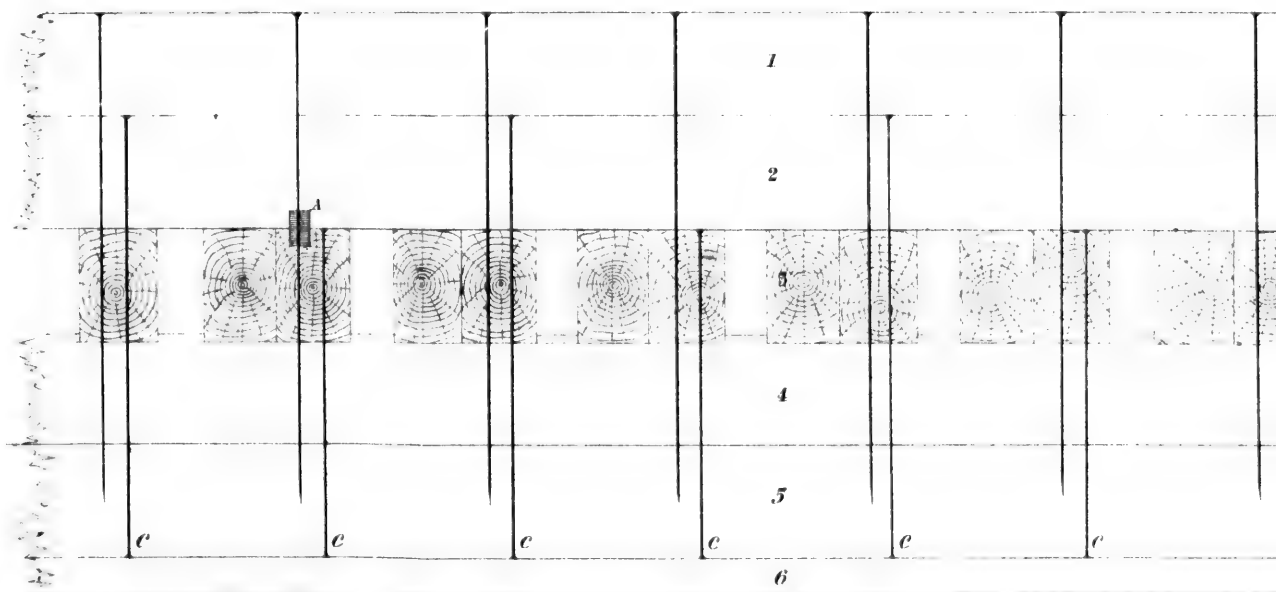
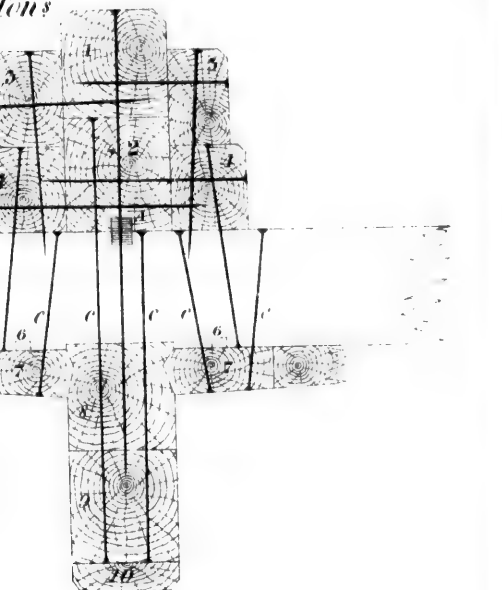


Figure 3.





NUMBER OF KEELSONS, BILGE PLANK AND MINIMUM DIMENSIONS OF VESSELS
FROM 100 TO 200 TONS.

| | 100 | 200 | 300 | 400 | 500 | 700 | 800 | 1000 | 1200 | 1400 | 1600 | 1800 | 2000 | No. for Tonnage, 100 to 2000, In inches. |
|--------------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|--|
| Rider Keelsons..... | ..... | 1 | 1 | 1 | 1 | 1 | 2 | 2 | 2 | 3 | 4 | 4 | 5 | No. from 100 to 2000, In inches. |
| Assistant Keelsons..... | ..... | ..... | ..... | ..... | ..... | ..... | ..... | ..... | ..... | ..... | ..... | ..... | ..... | " |
| Rudder Post..... | 10 | 11 | 12 | 13 | 14 | 15 | 16 | 17 | 17½ | 18 | 18½ | 19 | 20 | " |
| Ceiling on Flat..... | 2 | 2½ | 3 | 3½ | 4 | 4 | 4½ | 5 | 5 | 5 | 5 | 5 | 6 | " |
| Ceiling above Bilge..... | 2 | 2½ | 3 | 4 | 4½ | 5 | 5½ | 6 | 6 | 6 | 7 | 8 | 8 | " |
| Bilge Plank..... | 2 | 3 | 3 | 4 | 4 | 4 | 4 | 4 | 4 | 4 | 5 | 6 | 7 | No. from 100 to 2000, In inches. |
| Bilge Plank..... | 1 | 3½ | 4 | 6 | 7 | 5 | 10 | 12 | 12 | 12 | 12 | 12 | 12 | " |
| Hold Clamps..... | ..... | ..... | ..... | ..... | 4½ | 8 | 5½ | 6 | 6 | 6 | 7 | 8 | 8 | " |
| Deck Clamps..... | 2½ | 3 | 3½ | 4 | 4½ | 4½ | 5 | 6 | 6 | 6 | 6 | 6 | 7 | " |
| Upper Deck Waterway..... | 8x8 | 9x9 | 10x10 | 11x11 | 11x12 | 12x13 | 12x14 | 12x14 | 13x15 | 13x15 | 14x15 | 14x16 | 15x16 | " |
| Main Deck Waterway..... | ..... | ..... | ..... | ..... | 9x9 | 10x10 | 11x11 | 12x12 | 12x12 | 13x13 | 14x14 | 15x15 | 15x15 | " |
| Main Upper Waterway..... | ..... | ..... | ..... | ..... | 6x10 | 7x10 | 8x10 | 10x13 | 10x12 | 11x12 | 12x12 | 12x12 | 12x14 | " |
| Main Inner Waterway..... | ..... | ..... | ..... | ..... | 6x10 | 7x10 | 8x10 | 10x12 | 11x12 | 11x12 | 12x12 | 12x12 | 12x14 | " |
| Plank Sheer..... | 2½ | 2½ | 3 | 3½ | 4 | 4½ | 5 | 5½ | 6 | 6 | 6 | 6 | 12x14 | " |
| Rails..... | 3x8 | 3½x8 | 4x10 | 4x12 | 4½x12 | 5x12 | 5x12 | 6x12 | 6x13 | 6x14 | 6x14 | 6x14 | 7x15 | " |
| Wales..... | 3 | 3½ | 3½ | 4 | 4½ | 5 | 5 | 5½ | 6 | 6 | 6 | 6 | 7 | " |
| Wing Transom..... | 9 | 10 | 11 | 12 | 13 | 13 | 13 | 14 | 14 | 14 | 14 | 15 | 16 | " |
| Treenails..... | 1 | 1 | 1½ | 1½ | 1½ | 1½ | 1½ | 1½ | 1½ | 1½ | 1½ | 1½ | 1½ | " |

STEAM VESSELS.

No departure from a first class or standard will be allowed in the construction of steam vessels, and to have longitudinal strength proportionate to capacity. In addition thereto, their floors must be filled in solid as high up as the turn of the bilge, and fastened with one copper bolt through each floor timber, which must be clinched on the under side of the keel.

There must be two courses of longitudinal bolting on each side, and a horizontal chock against each timber butt, and one edge bolt driven through the ceiling in each timber room securing two or three planks together from keelson to deck the entire length of the vessel.

Each alternate timber must be diagonally iron strapped from the centre of bilge to the centre of upper deck clamps, and be bolted to each timber; they must cross and be riveted in each timber room.

MINIMUM SIZE OF IRON STRAPS.

| | | | | | | |
|-----------|----------------|---------------|--|-----------|----------------|---------------|
| 1000..... | 4 | $\frac{1}{8}$ | | 1600..... | $4\frac{3}{8}$ | $\frac{1}{2}$ |
| 1400..... | $4\frac{1}{2}$ | $\frac{1}{4}$ | | 2000..... | $4\frac{3}{8}$ | $\frac{1}{2}$ |

There must be iron water-tight bulkheads, from thirty to fifty feet of stem and stern, to reach at least two feet above deep load line, which must be well secured to strong stanchions on the after side.

Steam vessels engaged in the transportation of passengers are required to have sufficient life boats.

Steam vessels navigating the ocean or coast, are required to have a sufficient spread of canvas to make a port in case of derangement of machinery.

All steam vessels' bottoms to be examined annually; the sheathing stripped off the garboard, bilge, topsides and butts; the outside thread of oakum to be reefed out; the seams tried with a hawsing iron and beatle in order to ascertain if the oakum is sound.

Steam vessels constructed of standard materials will require

to be opened for inspection at the expiration of five years, owing to their liability to premature decay.

| | |
|---|-----------|
| Sea-going steamers of standard construction, having one or two decks, one or more engines, and sufficient spread of canvas..... | RATE A 1* |
| Sea-going steamers having one or two decks, one or more engines, and insufficient spread of canvas... | " A 1 |
| Steamers constructed for navigating Sounds, Lakes, and Rivers..... | " A 1½ 2 |
| The RATES to run from A 1 to A 2½, and to depend upon condition of hull and machinery. | |

The security and provision against fire are two, viz., *good and insufficient*, and will depend upon the character and condition of lining to chimney, rooms, coating of boilers and distance of fuel and wood work from boilers so as to avoid contact; and being provided with steam fire pumps, bilge, injections, feed and hand pumps, and sufficient length of hose to reach every part of the ship.

* Denotes sufficient canvas to make a port in case of disabled machinery.

RULES

FOR INSPECTION AND CLASSIFICATION.

[The character assigned will indicate real and intrinsic qualities.]

The classification of vessels will depend upon the model, general proportions, materials used and manner of construction, sparring and outfits. All vessels will retain the character first assigned them, so long as they show no defects or weakness, and are kept in efficient repair.

The kind, quality and dimensions of materials, and the equalization of strength in their distribution in scarphs, laps and butts, with mode and extent of fastening, must accord with the best practice of ship building.

Vessels built with scantling in any of their parts, inferior to the quality prescribed as the limits of A 1, will be liable to a reduction of classification, and the same if any of the materials are defective.

SHIPS,

When built of standard materials, will class A 1, seven years from the date of launching, subject to periodical surveys. A further continuation of character will be applied, in terms of the standard rules, if found sound and staunch at the expiration of aforesaid period. Not exceeding three years, after the expiration of the periods herein stated, no ship can be restored to her original character, only when rebuilt.

When built of standard materials, but not through treenailed, will class A 1.—

When built of mixed woods, in frames and covering, the tree-nails driven through, but with butt bolts in plank and with other parts fastened sufficiently, will class A 1½, five years from date of launching, subject to periodical surveys. A further continuation of character will depend on condition of frames, coverings, fasten-

ings and general efficiency, but not exceeding two years, which will terminate the period of assignment of original character.

In case of vessels being stranded, with damage, must be submitted to a new survey, in order to preserve their class.

Vessels not having been opened or bored at the expiration of time specified in the American Lloyd's, will receive a lower classification, or class be expunged, as the surveyors may determine.

Ships constructed with depth of hold exceeding twenty-three feet will be required to have orlop beams, secured with horizontal and hanging knees. When exceeding twenty-five feet, to have three full decks.

Ships exceeding four times their breadth in length should be cross (-|-) iron-strapped diagonally on the inside; outside strapping-leakage through the seams of outside plank will corrode and destroy the iron. The bolts through the straps either from out or inside should go through and clinch.

SINGLE DECK VESSELS

Built of standard materials, with depth of hold not exceeding eleven feet, and fastened in accordance with the rules set forth, will be classed A 1, seven years.

When built of mixed wood, through treenailed, and all other fastenings first class, will class A 1½ five years.

Centre-Board vessels, when built of oak frames and coverings, thoroughly fastened and moderately sparred, will class A2.

When built inferior to this grade, they will be classed according to their intrinsic qualities.

All vessels having detrimental overhang at the ends will be liable to a reduced classification.

Vessels exceeding twelve feet depth of hold must have sufficient number of hold beams efficiently connected and fastened with knees, and their masts wedged in partners.

Vessels having two decks must have scuppers through the waterways of lower deck.

NOTE.—In all cases when the treenails and butt bolts are not driven through and fastened on the ceiling, such to be stated by the Surveyor.

NOTE.—In all cases when vessels are not built according to the rules and system of classification, the inspectors are to class them in accordance with their intrinsic qualities.

GROUND TACKLING.

Tables of ground tackling, given on pp. 26, 27, are in accordance with the best outfitted ships in New York ; all vessels to have ground tackle in length, size, and weight in conformity to these rules.

When vessels are *wormed*, they will not be classed until the defects are removed.

Vessels oversparred, or deficient in stability, will be bad risks, and not classed until these defects are removed.

Vessels having their bowsprits stepped below the spar deck, or their hawser pipes through the between decks, will subject them to a lower classification.

The pumping facilities of vessels to be in the ratio of their tonnage, and so distributed that they may free the ship from water under all emergencies, or at any trim or inclination which may be given in loading or discharging, and the pump-well to be accessible by a passage into it at all times. Vessels with flat floors should have bilge pumps.

When the surveyors have reasonable doubts of the soundness of a vessel, they will notify the owner or master to expose the frame in such parts as may be desired, or to bore the ship to ascertain her qualities.

Cargo ports into the hold or lower 'tween decks of vessels will be considered detrimental to her classification.

An additional deck put on vessels after they are built, will be considered unprofitable risks for underwriters.

Vessels having their centre boards taken out will be required to have the floor timbers run across to meet the second futtocks, and checks put in to meet the first futtocks. If this rule is not complied with, they will be designated as centre boards on the American Lloyd's Register.

New vessels going on long voyages ought to be recaulked before being coppered in all cases ; or going from the port where built to another port in ballast, to load for a foreign voyage, should be recaulked above water line.

Vessels employed in the transportation of dry cargoes to have their masts coated.

All vessels to have their spars, rigging, sails, boats, pumps, compasses, deep sea and lead lines, in good order, and sufficient

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Fig. 1.

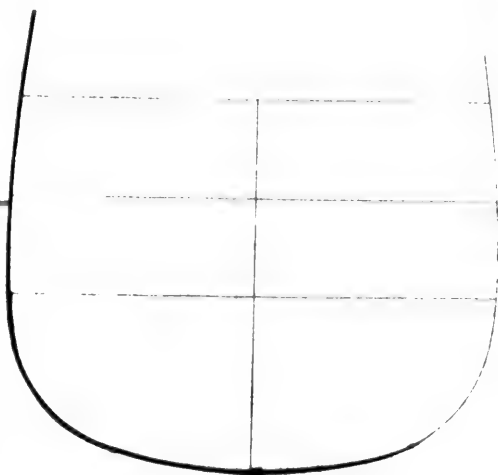


Fig. 3.



Fig. 2.

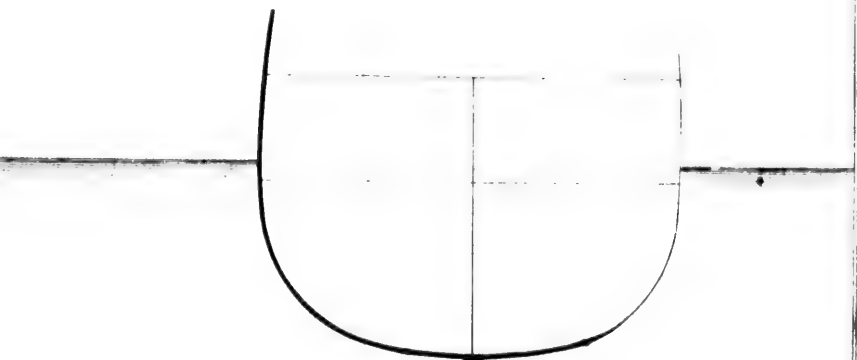


Fig. 1.



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charts of the latest surveys, including a list of new light-houses, &c.

Vessels trading to ports beyond the Cape of Good Hope, or Cape Horn, must have two suits of courses, topsails, jibs, spankers, and fore topmasts staysails, and to be metal sheathed within one foot of load line.

Vessels trading south of Europe, Western Africa, and islands not beyond Cape Horn or Cape of Good Hope, must have an extra foresail, fore topmast staysail, main topsail, and be metal sheathed.

Vessels on coasting voyages to have an extra topsail and foretopmast staysail.

All vessels should have lightning conductors, and three signal lights, — one at bowsprit end or under foretop, one forward part fore chains, each side, Red (Port) and Green (Starboard).

SURVEY OF CONSTRUCTION.

To be inspected with care, and a correct report, giving in detail the volume of scantling and fastening, the connection of the principal parts, the quality of the materials, the effects or good qualities of construction, to be surveyed in the following stages:—When the keel and keelson are united, when the frame is raised and keelsons in, when the deck frames are complete, when treenailed and butt bolted.

Scantling to be free from sap and defects; scarphs of keel not to be under the masts; the stem above forefoot to be one piece, or, if scarphed, to be above light water line; stern post to be one piece. Vessels over five hundred tons to have at least one pair of pointers, forward and aft, extending to the lower deck beams, and secured by knees; bilge streaks in volumes to be in the ratio of their tonnage; the planking to be bevelled to good caulking seams. Vessels of eight hundred tons, or over, to have a stringer under lower hanging knees, well bolted through frame.

The channel bolts to be driven through frames and ceiling, and clinched on rings, or keyed; the partial fastenings of the outside planking and ceiling to be complete before the treenail holes are bored. Care should be observed in driving the treenails to prevent splitting of plank.

LOAD DISPLACEMENT.

The present limit of load displacement is subject to conflicting opinions between Ship Owners and the Underwriters. The former are very sensitive with regard to a stipulated limit of load draught, which, if established, would produce many annoyances. We think the requirements might be realized without being detrimental to their interests. That vessels of all proportion, model, or build may retain their buoyant qualities. The vessel's register tonnage being no criterion for her capacity for carrying weight, the capacity for holding bulk and the capability for carrying weight must be recognized as distinct elements. We suggest the load draught to be regulated by the depth of hold, the required side out when loaded to be measured from the water up the side even with the lowest line of sheer of spar deck. Vessels having an additional deck put on after construction, the depth of hold to be measured from original deck.

TABLE FOR SHIPS OF 2 OR 3 DECKS.

| DEPTH OF HOLD IN FEET. | INCHES TO FOOT OF DEPTH. | SIDE OUT WHEN LOADED. |
|------------------------|--------------------------|-----------------------|
| 27 | 3½ | 7 10½ |
| 29 | — | 5 10 |

SINGLE DECK VESSELS.

| DEPTH OF HOLD IN FEET. | INCHES TO FOOT OF DEPTH. | SIDE OUT WHEN LOADED. |
|------------------------|--------------------------|-----------------------|
| 12 | 3 | 3 0 |
| 11 | 2½ | 2 3½ |
| 10 | 2¼ | 1 10½ |
| 9 | 2 | 1 6 |
| 8 | 1½ | 1 0 |

EXPLANATION TO PLATE 4.

Figure 1.—Three decks. Hold 27 feet. Side out at Load line, 7 feet 10½ inches.
 Figure 2.—Two decks. Hold 29 feet. Side at Load line, 5 feet 10 inches.
 Figure 3.—One deck. Hold 11 feet 6 inches. Side out at Load line, 2 feet 10½ inches.
 Figure 4.—One deck. Hold 9 feet. Side at Load line, 1 foot 6 inches.

EXPLANATION TO PLATE 5.

Figure 1.—1, Upper Hanging Knee; 2, Clamp; 3, Waterway; 4, Main Waterway; 5, Lock Streak or Inner Waterway; 6, Thick Deck Plank; Deck Plank; 8, Beam; 9, Lower Hanging Knee; 10, Lower Clamps; 11, Timber; 12, Wales; A, Key.

Figure 2.—1, Rider Keelson; 2, Main Keelson; 3, Rider Assistant Keelson; 4, Assistant Keelsons; 5, Floor Timbers; 6, Timbers; 7, Thick Garboard; 8, Keel; 9, Lower or Rider Keel; 6, Shoe; A, Coag; 6, Copper Bolts.

Figure 3.—1, Rider Keelson; 2, Main Keelson; 3, First Foothook; 4, Keel; 5, Lower or Rider Keel; 6, Shoe; A, Coag; C, Copper Bolts.

KEY OF CLASSIFICATION.

[Various degrees of confidence are represented.]

CLASS **A 1** }
 1- } will comprise the Degree of First Class.
 1¹/₂ }
 CLASS **A 1¹/₂-** } Degree of Second Class.
 2 }
 2⁻ } Degrees of Third Class.
 2¹/₂ }

The degree of First and Second Class will imply confidence for the transportation of perishable cargoes on long voyages. The degrees of Third Class will not imply confidence for the conveyance of cargoes in their nature subject to sea damage.

Ship owners desirous of having the character of their vessels restored on the American Lloyd's, must submit to a special examination of Frame, Planking and Fastenings, and put in a state of thorough repair.

| | | AMERICAN LLOYD'S. | BRITISH LLOYD'S. | FRENCH VERITAS. |
|------------------|---------------------------|--------------------------------------|---------------------|--------------------|
| First Class. | First Grade, First Class, | A 1 | A 1 | 3.3 1.1 |
| | Second " " | A 1- | " " | " " |
| | Third " " | A 1¹/₂ | " " | " " |
| Second Class. | First Grade, Second Class | A 1¹/₂- | A 1 | 5.6 1.1 |
| | Second " " | A 2 | *Æ 1 | 5.6 2.1 |
| Third Class. | First Grade, Third Class, | A 2 | Æ 1 | 3.4 2.1 |
| | Second " " | A 2¹/₂ | Æ 2 | 2.3 2.2 |

FOREIGN BUILT VESSELS.

Foreign built Vessels, when constructed within the requirements of the Standard Rules of the American Lloyd's, and secured with Iron Knees and Binders, will receive the highest Class.

British Colonial and other Foreign built Vessels, when built of mixed timber, fastenings efficient and secured with Iron Knees and Binders, will receive Class 1½ for five years from date of launching. When iron diagonal strapped on frame, 7 years' class, subject to a continuation of character, if found sound at expiration of aforesaid period, not exceeding two years, which will terminate the period of original character.

KEY TO ABBREVIATIONS.

USED UNDER THE FOLLOWING HEADS.

| Rig. | Decks. |
|--------------------------------|---------------------------------------|
| S —SHIP. | 1 —ONE DECK. |
| B —BARK. | 1PB —Do. & PARTNER BEAMS |
| Btn —BARKENTINE. | 1B —Do. and BEAMS. |
| Bg —BRIG. | 2 —TWO DECKS. |
| HB —HALF BRIG. | 2B —Do. and BEAMS. |
| Sc —SCHOONER. | 3 —THREE DECKS. |
| Sp —SLOOP. | |
| Glt —GALLIOT. | |
| P —POLLACCA. | |
| Trn —3 MASTED SCHOONER. | |
| Class. | Fastenings. |
| A 1 | I —IRON. |
| 1— | C —COPPER. |
| 1½— | CI —ESSENTIAL COPPER and IRON. |
| 1¾— | GI —GALVANIZED IRON. |
| 2— | |
| 2½— | M —METAL. |
| 2¾— | Z —ZINC. |
| | |
| | WS —WOOD SHEATHED. |
| | X —SPECIALLY EXAMINED. |
| | |
| | Eng —ENGINE. |
| | Cyl —CYLINDER. |
| | SP —STROKE PISTON. |
| | D F H —DECK FREIGHT HOUSE. |
| | |
| | Steamers. |

| Woods. | Remarks. |
|--|-------------------------------|
| O & L O —OAK and LIVE OAK | P C —POOP CABIN. |
| O —WHITE OAK. | L P —LONG POOP. |
| Lt —LOCUST. | H P —HALF POOP. |
| C —CEDAR. | B D —BREAK TO DECK. |
| Ch —CHESNUT. | L D —LOW DECK. |
| H —HACKMATAK. | F D —FLUSH DECK. |
| M —MIXED WOODS, BIRCH, BEECH, MAPLE, SPRUCE. | T C —TRUNK CABIN. |
| P —PINE. | D C —DECK CABIN. |
| T —TEAK. | C B —CENTRE BOARD. |
| F —FIR. | N T —NEW TOP SIDES. |
| | Rebt —REBUILT. |
| | Reps —REPAIRS. |
| | L Reps —LARGE REPAIRS. |
| | Op —OPENED. |
| | N D —NEW DECK. |
| | N K —NEW KEEL. |
| | N Kn —NEW KEELSON. |
| | N S —NOT SURVEYED.* |
| | Bpt —BOWPORT. |
| | H C —HOUSE CABIN. |
| | H K —HANG KNEED. |
| | F S —FIRE SECURITY. |

* "Not Surveyed" signifies that the vessel has not been seen by the Inspector since her launch.

FORMER RULES

FOR MEASURING AND CALCULATING TONNAGE IN THE UNITED STATES.

MEASUREMENT.

LENGTH, from forward side of stem to after side of stern post, on deck.

BREADTH of beam, outside of plank in the widest part of vessel.

DEPTH of hold, from ceiling alongside the main keelson to under side of deck plank.

RULE FOR CALCULATING.

Deduct 3-5 of breadth of beam from the length, multiply the remainder by the breadth of beam and that product by the depth of hold, and divide by 95.

Vessels having two or more decks proceed as above in measuring and calculating, with this difference—take half the breadth of beam for depth of hold.

LENGTH AND SIZES OF RIGGING.

| Ships of 1,800 Tons. | | FATHOMS. | SIZE. | STRANDS. |
|--|-----|------------------|-------|----------|
| Fore Shrouds | 98 | 10 | | 4 |
| Main Shrouds | 108 | 10 $\frac{1}{2}$ | | 4 |
| Mizen Shrouds and Stays | 74 | 8 $\frac{1}{2}$ | | 4 |
| Fore topmast backstays | 54 | 10 $\frac{1}{2}$ | | 4 |
| Main topmast backstays | 58 | 10 $\frac{1}{2}$ | | 4 |
| Mizen topmast backstays and stay, fish pendant and cargo span | 77 | 8 | | 4 |
| F. and M. stays | 58 | 10 $\frac{1}{2}$ | | 4 |
| F. and M. topmast stays | 89 | 9 | | 4 |
| Jibstay and guys, topsail runner, fish hook pendant | 67 | 7 $\frac{1}{2}$ | | 4 |
| F. and M. topgallant backstays..... | 75 | 7 | | 4 |
| Outer jibstay and guys, F. and M. lifts | 114 | 6 | | 4 |
| F. and M. topgallant stays, bumpkin pendants..... | 100 | 5 $\frac{3}{4}$ | | 4 |
| F. and M. topmast rigging and mizen topmast runner..... | 100 | 5 $\frac{1}{2}$ | | 4 |
| Mizen topmast rigging, topgallant backstays and brace pendants | 95 | 4 $\frac{3}{4}$ | | 4 |
| F. and M. royal backstays, stay sail pendants | 105 | 5 | | 4 |
| F. and M. royal stays, mizen royal backstays | 112 | 4 $\frac{1}{2}$ | | 4 |
| F. and M. topgallant rigging and boom topping lift..... | 100 | 4 $\frac{1}{2}$ | | 4 |
| F. and M. skysail backstays..... | 110 | 4 $\frac{1}{2}$ | | 4 |
| F. and M. skysail stays and mizen skysail backstays..... | 100 | 3 $\frac{1}{2}$ | | 4 |
| Lanyards, fore rigging and backstays..... | 110 | 5 $\frac{1}{2}$ | | 4 |
| Lanyards, main rigging and backstays..... | 110 | 5 $\frac{1}{2}$ | | 4 |
| Lanyards, mizen rigging and backstays and bowsprit shrouds. | 85 | 4 $\frac{1}{2}$ | | 4 |
| | 95 | 4 $\frac{1}{2}$ | | 4 |
| | 95 | 4 | | 4 |
| | 95 | 3 $\frac{3}{4}$ | | 4 |
| | 95 | 3 $\frac{1}{2}$ | | 4 |
| <i>Sundry purposes</i> | 95 | 3 $\frac{1}{4}$ | | 4 |
| | 95 | 3 | | 4 |
| | 100 | 2 $\frac{1}{2}$ | | 4 |
| | 100 | 2 $\frac{1}{2}$ | | 4 |
| | 100 | 2 $\frac{1}{2}$ | | 4 |

LENGTH AND SIZES OF RIGGING.

STRANDS.

| Ships of 1,000 Tons. | | FATHOMS. | SIZE. | STRANDS. |
|--|-----|-----------------|-------|----------|
| Fore shrouds and bobstay..... | 89 | 9 $\frac{1}{2}$ | 4 | |
| Main shrouds..... | 92 | 9 $\frac{1}{2}$ | 4 | |
| Mizen shrouds and stay..... | 63 | 8 | 4 | |
| F. and M. stays..... | 63 | 9 $\frac{1}{2}$ | 4 | |
| Fore topmast backstays..... | 48 | 9 $\frac{1}{2}$ | 4 | |
| Main topmast backstays..... | 51 | 9 $\frac{1}{2}$ | 4 | |
| Fore topmast stays and bowsprit shrouds..... | 56 | 8 $\frac{1}{2}$ | 4 | |
| Main and mizen topmast stay and backstays, fish pendant and cargo spans..... | 93 | 7 $\frac{1}{2}$ | 4 | |
| Jibstay and guys, topsail runners, fish hook pendant..... | 60 | 6 $\frac{1}{2}$ | 4 | |
| Outer jibstay and gups, and bumpkin pendant..... | 59 | 5 | 4 | |
| F. and M. topgallant backstays..... | 68 | 6 $\frac{1}{2}$ | 4 | |
| F. and M. topgallant stays and topsail lifts..... | 76 | 5 | 4 | |
| F. and M. lifts and jib pendants..... | 66 | 5 $\frac{3}{4}$ | 4 | |
| F. and M. topmast rigging and mizen topsail runner..... | 98 | 5 $\frac{1}{2}$ | 4 | |
| Mizen topmast rigging and mizen topgallant backstays..... | 78 | 4 $\frac{1}{2}$ | 4 | |
| F. and M. royal backstays and martingale gear..... | 100 | 4 $\frac{1}{2}$ | 4 | |
| F. and M. royal stays, mizen royal backstays..... | 98 | 4 | 4 | |
| F. and M. topgallant rigging and boom topping lifts..... | 100 | 3 $\frac{3}{4}$ | 4 | |
| Lanyards, fore rigging and backstays..... | 100 | 4 $\frac{3}{4}$ | 4 | |
| Lanyards, main rigging and backstays..... | 100 | 4 $\frac{1}{2}$ | 4 | |
| Lanyards, mizen rigging backstays and bowsprit shrouds..... | 95 | 4 | 4 | |
| <i>Sundry purposes</i> | 100 | 3 $\frac{1}{2}$ | 4 | |
| | 100 | 3 | 4 | |
| | 100 | 2 $\frac{3}{4}$ | 4 | |
| | 85 | 4 $\frac{1}{4}$ | 4 | |
| | 85 | 4 | 4 | |
| | 86 | 3 $\frac{3}{4}$ | 4 | |
| | 100 | 2 $\frac{1}{2}$ | 4 | |
| | 100 | 2 $\frac{1}{4}$ | 4 | |
| | 100 | 2 | 4 | |

LENGTH AND SIZES OF RIGGING.

| Ship of 700 Tons. | FATHOMS. | SIZE. | STRAIDS. |
|---|----------|-----------------|----------|
| Fore shrouds and mainstays..... | 92 | 8 | 4 |
| Main shrouds and forestays..... | 98 | 8 | 4 |
| Mizen shrouds and stay..... | 52 | 6 $\frac{3}{4}$ | 4 |
| F. and M. topmast backstays..... | 85 | 7 $\frac{3}{4}$ | 4 |
| F. and M. topmast stays, fish pendant and cargo span..... | 77 | 7 | 4 |
| Mizen topmast backstays and stay..... | 46 | 6 $\frac{1}{2}$ | 4 |
| Jibstay and guys and topsail runners..... | 59 | 5 $\frac{1}{4}$ | 4 |
| F. and M. lifts and bumpkin brace pendants..... | 50 | 5 | 4 |
| F. and M. topmast rigging and topsail lifts..... | 06 | 4 $\frac{1}{2}$ | 4 |
| Mizen topmast rigging, mizen topgallant backstays, spanker topping lifts..... | 75 | 4 $\frac{1}{4}$ | 4 |
| Outer jibstay and guys..... | 45 | 4 $\frac{3}{4}$ | 4 |
| F. and M. topgallant stays, swinging boom topping lifts..... | 65 | 4 $\frac{1}{4}$ | 4 |
| F. and M. topgallant backstays..... | 65 | 5 $\frac{1}{4}$ | 4 |
| Lanyards, fore rigging and backstays..... | 95 | 4 $\frac{1}{4}$ | 4 |
| Lanyards, main rigging and backstays..... | 95 | 4 $\frac{1}{4}$ | 4 |
| Lanyards, mizen rigging, backstays and bowsprit shrouds.... | 75 | 3 $\frac{3}{4}$ | 4 |
| Foot ropes for yards, royal and topgallant lifts, royal back- stays and lanyards for topgallant and royal backstays, jib and martingale guys, man ropes, vangs, &c. | 75 | 3 $\frac{3}{4}$ | 4 |
| | 75 | 3* | 4 |
| | 75 | 3 $\frac{1}{2}$ | 4 |
| | 75 | 3 | 4 |
| | 75 | 2 $\frac{3}{4}$ | 4 |
| | 75 | 3 $\frac{1}{2}$ | 4 |

LENGTH AND SIZES OF RIGGING.

| | FATHOMS. | SIZE. | STRANDS. |
|--|----------|-----------------|----------|
| Fore shrouds and mainstays... | 79 | 7 $\frac{3}{4}$ | 4 |
| Main shrouds and forestays..... | 80 | 7 $\frac{3}{4}$ | 4 |
| F. and M. topmast backstays..... | 82 | 7 $\frac{1}{2}$ | 4 |
| F. and M. topmast stays, fish pendant and cargo span..... | 67 | 6 $\frac{3}{4}$ | 4 |
| Mizen shrouds..... | 40 | 6 | 4 |
| Jib stays and guys, topsail runners and mizen stay..... | 54 | 5 $\frac{1}{4}$ | 4 |
| F. and M. topgallant and mizen topmast backstays..... | 83 | 5 | 4 |
| F. and M. topmast rigging and topsail lifts..... | 85 | 4 $\frac{1}{2}$ | 4 |
| F. and M. lifts, jib pendants, outer jib stay..... | 66 | 4 $\frac{3}{4}$ | 4 |
| F. and M. topgallant stays, flying jib guys, topsail brace, pendants, spanker topping lift..... | 70 | 4 | 4 |
| F. and M. royal backstays, topgallant and mizen topmast stays | 100 | 3 $\frac{1}{2}$ | 4 |
| Foot ropes, lower and topsail yards, mizen topmast stay..... | 41 | 3 $\frac{1}{2}$ | 4 |
| F. and M. topgallant lifts, main ropes..... | 75 | 3 $\frac{1}{4}$ | 4 |
| Topgallant foot ropes, royal stays, lanyards for jib guys..... | 75 | 3 | 4 |
| Royal foot ropes, lanyards for topgallant backstays, mizen top- gallant backstays..... | 100 | 2 $\frac{3}{4}$ | 4 |
| Lanyards for martingale guys, etc..... | 85 | 2 $\frac{1}{2}$ | 4 |
| Lanyards, fore rigging, and topmast backstays..... | 75 | 4 | 4 |
| Lanyards, main rigging, and topmast backstays..... | 75 | 4 | 4 |
| Lanyards, mizen rigging..... | 40 | 3 $\frac{1}{4}$ | 4 |

LENGTH AND SIZES OF RIGGING.

| Bark of 300 Tons. | | | |
|--|----------|-----------------|----------|
| | FATHOMS. | SIZE. | STRANDS. |
| Fore shrouds and main stays. | 63 | 6 $\frac{1}{2}$ | 4 |
| Main shrouds and forestays. | 66 | 6 $\frac{3}{4}$ | 4 |
| F. and M. topmast backstays. | 74 | 6 | 4 |
| Foretopmast stays, fish pendant and cargo span. | 40 | 5 $\frac{1}{2}$ | 4 |
| Jib stay and guys, main topmast stays, topsail runner. | 72 | 5 | 4 |
| Fore and main topgallant backstays, lower and topping lifts. | 111 | 4 $\frac{1}{2}$ | 4 |
| Topgallant stays, topsail lifts. | 50 | 3 $\frac{3}{4}$ | 4 |
| F. and M. topmast rigging. | 56 | 4 | 4 |
| Royal backstays, foot ropes, lower and topsail yards, flying jib guys. | 108 | 3 $\frac{1}{2}$ | 4 |
| Royal stays, topgallant rigging, topgallant lifts and foot ropes. | 100 | 3 | 4 |
| Lanyards, fore rigging, and backstays. | 75 | 3 $\frac{3}{4}$ | 4 |
| Lanyards, main rigging, and backstays. | 75 | 3 $\frac{3}{4}$ | 4 |
| <i>Sundry purposes.</i> —Royal lifts and foot ropes, lanyards for jib and martingale guys, vangs, man ropes, jib and stay-sail pendants. | 55 | 2 $\frac{3}{4}$ | 4 |
| | 55 | 2 $\frac{3}{4}$ | 4 |
| | 55 | 2 $\frac{1}{4}$ | 4 |
| | 55 | 2 | 4 |

| Brig of 300 Tons. | | | |
|---|----------|-----------------|----------|
| | FATHOMS. | SIZE. | STRANDS. |
| Fore shrouds and main stays. | 63 | 6 $\frac{3}{4}$ | 4 |
| Main shrouds and forestay. | 60 | 6 $\frac{3}{4}$ | 4 |
| Mizen shrouds and stay. | 39 | 5 $\frac{1}{2}$ | 4 |
| F. and M. topmast backstays. | 71 | 6 $\frac{1}{2}$ | 4 |
| F. and M. topmast stays, fish pendant and cargo span. | 69 | 6 $\frac{1}{2}$ | 4 |
| Jibstay and guys and topsail runners. | 53 | 5 $\frac{1}{2}$ | 4 |
| F. and M. topgallant backstays and lower lifts. | 91 | 4 $\frac{1}{2}$ | 4 |
| Flying jibstay and guys and spanker topping lifts. | 100 | 4 | 4 |
| F. and M. topmast rigging, mizen topmast backstays, jib pendants. | 100 | 4 $\frac{1}{2}$ | 4 |
| F. and M. royal backstays, foot ropes, lower and topsail yards. | 100 | 3 $\frac{3}{4}$ | 4 |
| F. and M. royal stays, topgallant rigging and lifts. | 100 | 3 $\frac{3}{4}$ | 4 |
| F. and M. topgallant stays and topsail lifts. | 52 | 4 $\frac{1}{2}$ | 4 |
| Lanyards, fore rigging, and backstays. | 85 | 3 $\frac{3}{4}$ | 4 |
| Lanyards, main rigging, and backstays. | 85 | 3 $\frac{3}{4}$ | 4 |
| Lanyards, mizen rigging, and bowsprit shrouds. | 75 | 3 $\frac{3}{4}$ | 4 |
| Royal lifts and foot ropes, lanyards for jib and martingale guys, mizen topmast rigging and stay, mizen topgallant backstays, man rope, vangs, &c., &c. | 75 | 3 | 4 |
| | 75 | 2 $\frac{3}{4}$ | 4 |
| | 75 | 2 $\frac{1}{2}$ | 4 |
| | 75 | 2 | 4 |

LENGTH AND SIZES OF RIGGING.

| Half Brig of 300 Tons. | | | |
|--|----------|-----------------|----------|
| | FATHOMS. | SIZE. | STRAIDS. |
| Fore shrouds, F. and M. stays | 92 | 6 $\frac{3}{4}$ | 4 |
| Main shrouds | 72 | 6 $\frac{3}{4}$ | 4 |
| Fore topmast backstays | 41 | 6 $\frac{3}{4}$ | 4 |
| Fore topmast stays | 32 | 5 $\frac{3}{4}$ | 4 |
| Jib stays, topsail runner, main boom topping lift | 43 | 5 | 4 |
| Jib guys and fore lifts | 33 | 4 $\frac{3}{4}$ | 4 |
| Fore topmast rigging, topgallant and main topmast backstays, topsail lifts | 91 | 4 $\frac{1}{2}$ | 4 |
| Fore topgallant and main topmast stays, flying jib guys | 51 | 3 $\frac{1}{2}$ | 4 |
| Fore topgallant and main topmast rigging, fore royal backstays | 65 | 3 $\frac{1}{2}$ | 4 |
| Fore topgallant backstays | 35 | 3 $\frac{1}{2}$ | 4 |
| Lanyards, F. and M. rigging | 88 | 3 $\frac{1}{2}$ | 4 |
| <i>Sundry purposes</i> | 45 | 3 $\frac{1}{2}$ | 4 |
| | 45 | 2 $\frac{1}{2}$ | 4 |
| | 45 | 2 $\frac{3}{4}$ | 4 |
| | 45 | 2 $\frac{3}{4}$ | 4 |

| Schooner of 400 Tons. | | | |
|--|----------|------------------|----------|
| | FATHOMS. | SIZE. | STRAIDS. |
| Fore shrouds and springstay | 90 | 7 $\frac{3}{4}$ | 4 |
| Main shrouds | 84 | 7 $\frac{3}{4}$ | 4 |
| Jib and forestays | 47 | 10 $\frac{1}{4}$ | 4 |
| Flying jib stay, main boom topping lift and boom pendants | 50 | 6 | 4 |
| Jib guys, fore boom topping lift and boom pendants | 35 | 5 | 4 |
| F. and M. topmast backstays, jib and jibstay and guys | 106 | 4 $\frac{1}{2}$ | 4 |
| Topmast stays | 43 | 4 $\frac{1}{2}$ | 4 |
| Topmast rigging | 60 | 3 $\frac{3}{4}$ | 4 |
| Lanyards for jib and martingale guys and bowsprit shrouds, backstays, &c., &c. | 30 | 3 $\frac{3}{4}$ | 4 |
| | 30 | 2 $\frac{1}{2}$ | 4 |
| | 25 | 2 $\frac{1}{2}$ | 4 |
| | 25 | 2 $\frac{1}{2}$ | 4 |

LENGTH AND SIZES OF RIGGING.

| Schooner of 350 Tons. | | | |
|---|---|-----------------|-------------------|
| | FATHOMS. | SIZE. | STRANDS. |
| Fore shrouds and springstay..... | 89 | 7 $\frac{1}{2}$ | 4 |
| Main shrouds..... | 82 | 7 $\frac{1}{2}$ | 4 |
| Jibstays..... | 34 | 10 | 4 |
| Flying jib stay, main boom topping lift and deckstay..... | 46 | 5 $\frac{3}{4}$ | 4 |
| Flying jib guys, fore boom topping lift and deckstay..... | 32 | 4 $\frac{1}{2}$ | 4 |
| F. and M. topmast backstays, jib and jibstays and guys..... | 106 | 4 $\frac{1}{2}$ | 4 |
| <i>Sundry purposes,</i> { | F. and M. topmast stays, 25 fathoms, 3 $\frac{1}{4}$ inch | 41 | 4 |
| | F. and M. topmast rigging, 25 " 3 " | 55 | 2 $\frac{1}{2}$ 4 |
| | Lanyards, F. and M. rigging, 25 " 2 $\frac{1}{2}$ " | 98 | 2 $\frac{1}{2}$ 4 |

| Schooner of 250 Tons. | | | |
|--|----------|-----------------|----------|
| | FATHOMS. | SIZE. | STRANDS. |
| Fore shrouds and spring stay..... | 82 | 6 $\frac{1}{2}$ | 4 |
| Main shrouds..... | 60 | 6 $\frac{1}{2}$ | 4 |
| Jibstays..... | 31 | 8 $\frac{1}{2}$ | 4 |
| Flying jibstay, main boom topping lift, jack and deckstay.... | 54 | 5 $\frac{1}{2}$ | 4 |
| Flying jib guys, fore boom topping lift, boom pendants..... | 31 | 4 $\frac{1}{2}$ | 4 |
| F. and M. topmast backstays..... | 67 | 3 $\frac{1}{2}$ | 4 |
| F. and M. topmast rigging and stays..... | 90 | 3 $\frac{1}{2}$ | 4 |
| Lanyards, lower rigging..... | 95 | 3 $\frac{1}{2}$ | 4 |
| Lanyards for bowsprit shrouds, jib and martingale guys, { foot ropes, &c. | 35 | 3 | 4 |
| | 25 | 2 $\frac{3}{4}$ | 4 |
| | 25 | 2 $\frac{1}{2}$ | 4 |

| Schooner of 150 Tons. | | | |
|---|----------|-----------------|----------|
| | FATHOMS. | SIZE. | STRANDS. |
| Fore shrouds..... | 76 | 6 $\frac{1}{2}$ | 4 |
| Main shrouds..... | 61 | 6 $\frac{1}{2}$ | 4 |
| Jibstays..... | 30 | 7 $\frac{1}{2}$ | 4 |
| Flying jibstay, main boom topping lift, sheet straps..... | 34 | 5 | 4 |
| Jib guys, fore boom topping lift, boom pendants..... | 34 | 4 $\frac{1}{2}$ | 4 |
| Topmast rigging and stay block straps..... | 75 | 3 $\frac{1}{2}$ | 4 |
| Block straps, lanyards for bowsprit shrouds..... | 35 | 3 | 4 |
| Lanyards, F. and M. rigging..... | 85 | 3 $\frac{1}{2}$ | 4 |
| Foot ropes for jib, main booms, &c. { | 25 | 2 $\frac{3}{4}$ | 4 |
| | 25 | 2 $\frac{1}{2}$ | 4 |

TABLE
OF WEIGHTS OF ANCHORS FOR TONNAGE FROM 100 TO 2000.

| Tons. | Bower. lbs. | 2 Bowers. lbs. | Tons. | Bower. lbs. | 2 Bowers. lbs. |
|-------|----------------|-------------------|-------|----------------|-------------------|
| 100 | 900 | 800 | 1100 | 4000 | 3900 |
| 200 | 1200 | 1000 | 1200 | 4100 | 4000 |
| 300 | 1500 | 1300 | 1300 | 4200 | 4100 |
| 400 | 1900 | 1500 | 1400 | 4400 | 4200 |
| 500 | 2300 | 2000 | 1500 | " | " |
| 600 | 2800 | 2500 | 1600 | 4600 | 4400 |
| 700 | 3200 | 2900 | 1700 | " | " |
| 800 | 3600 | 3300 | 1800 | 4800 | 4600 |
| 900 | 3700 | 3500 | 1900 | " | " |
| 1000 | 3900 | 3800 | 2000 | 5000 | 4800 |

TABLE
OF WEIGHT OF STREAM AND KEDGE ANCHORS FOR TONNAGE FROM
100 TO 2000 TONS.

| Tons. | Stream Anchor. lbs. | Kedge Anchor. lbs. | Tons. | Stream Anchor. lbs. | Kedge Anchor. lbs. |
|-------|---------------------------|--------------------------|-------|---------------------------|--------------------------|
| 100 | | 150 | 1100 | 1400 | 600 |
| 200 | | 200 | 1200 | " | " |
| 300 | 700 | 300 | 1300 | 1500 | 650 |
| 400 | 800 | 350 | 1400 | " | " |
| 500 | 900 | 400 | 1500 | 1600 | 700 |
| 600 | 1000 | 450 | 1600 | " | " |
| 700 | 1200 | 500 | 1700 | " | " |
| 800 | " | " | 1800 | 1800 | 750 |
| 900 | 1300 | 550 | 1900 | " | " |
| 1000 | " | " | 2000 | 2000 | 800 |

NOTE.—It is necessary that all vessels of 300 tons and over should be furnished with stream and kedge anchors of sufficient weight.

TABLE
OF SIZE AND LENGTH OF CHAIN CABLES FOR TONNAGE FROM 100 TO 2000.

| Tons. | 1st Size. Inch. | 2d Size. Inch. | Length, Fathom. | Size Stream Chain. Inch. |
|-------|--------------------|-------------------|--------------------|-----------------------------|
| 100 | $\frac{7}{8}$ | $\frac{3}{4}$ | 90 | |
| 200 | $1\frac{1}{8}$ | 1 | 120 | |
| 300 | $1\frac{1}{4}$ | $1\frac{1}{8}$ | 150 | $\frac{3}{4}$ |
| 400 | $1\frac{5}{16}$ | $1\frac{1}{4}$ | " | $\frac{7}{8}$ |
| 500 | $1\frac{3}{8}$ | " | 180 | $1\frac{1}{8}$ |
| 600 | $1\frac{9}{16}$ | $1\frac{7}{16}$ | " | 1 |
| 700 | $1\frac{5}{8}$ | $1\frac{1}{2}$ | " | 1 |
| 800 | $1\frac{11}{16}$ | $1\frac{5}{8}$ | " | $1\frac{1}{8}$ |
| 900 | $1\frac{3}{4}$ | $1\frac{3}{4}$ | " | $1\frac{3}{16}$ |
| 1000 | $1\frac{7}{8}$ | $1\frac{7}{8}$ | 225 | $1\frac{1}{4}$ |
| 1100 | " | " | " | " |
| 1200 | " | $1\frac{1}{2}$ | " | $1\frac{3}{8}$ |
| 1300 | 2 | " | " | " |
| 1400 | " | " | 245 | $1\frac{1}{2}$ |
| 1500 | $2\frac{1}{16}$ | 2 | " | " |
| 1600 | " | " | " | $1\frac{5}{8}$ |
| 1700 | " | " | 270 | " |
| 1800 | " | " | " | " |
| 1900 | $2\frac{3}{16}$ | $2\frac{1}{16}$ | " | $1\frac{3}{4}$ |
| 2000 | " | " | " | " |

NOTE.—It is necessary that all vessels corresponding to the above tonnage, from 100 to 2,000, should have their chain cables of sufficient size and length.

WEIGHT OF CHAINS PER FATHOM.

| | | | |
|-------------------------|---------|---------------------------|---------|
| $\frac{1}{2}$ Inch..... | 14 lbs. | $1\frac{3}{16}$ Inch..... | 78 lbs. |
| $\frac{9}{16}$ "..... | 18 " | $1\frac{1}{4}$ "..... | 80 " |
| $\frac{5}{8}$ "..... | 22 " | $1\frac{5}{16}$ "..... | 95 " |
| $1\frac{1}{8}$ "..... | 27 " | $1\frac{3}{8}$ "..... | 105 " |
| $1\frac{3}{8}$ "..... | 32 " | $1\frac{7}{8}$ "..... | 115 " |
| $1\frac{5}{8}$ "..... | 38 " | $1\frac{1}{2}$ "..... | 125 " |
| $1\frac{7}{8}$ "..... | 44 " | $1\frac{9}{16}$ "..... | 130 " |
| $1\frac{9}{8}$ "..... | 50 " | $1\frac{5}{8}$ "..... | 145 " |
| 1 "..... | 56 " | $1\frac{3}{4}$ "..... | 170 " |
| $1\frac{1}{16}$ "..... | 63 " | $1\frac{7}{8}$ "..... | 196 " |
| $1\frac{1}{8}$ "..... | 70 " | 2 "..... | 224 " |

STOWAGE OF MIXED CARGOES.

APPROVED BY THE COMMITTEE OF DIRECTION.

1. Owners, Commanders, &c., of Ships are considered at law the same as Common Carriers. It is therefore necessary that all due precautions be taken to receive and stow cargoes in good order, and deliver the same in like good order. The law holds the Ship Owner liable for the safe custody of the Goods when properly and legally received on board in good order, and for the "delivery" to parties producing the Bill of Lading. Goods are not unfrequently sent alongside in a damaged state, and letters of indemnity given to the Captain by the Shippers for signing in good order and condition. This is nothing more than conniving at fraud. Fine Goods are often damaged in the ship's hold, by Stevedores, if permitted to use Cotton Hooks in handling bales. The Stowage and Dunnage must be A 1, *and is often of more importance than the class of the Vessel*, as experience has proved.

2. The load draught must be regulated by the depth of hold, allowing three and a half inches to every foot of depth, measured from the lowest line of sheer of Deck amidship to the water, when upright. Vessels having an additional deck put on after construction, the depth of hold to be measured from original deck.

3. Wheat, Corn, Rice, Beans, Peas, &c., when in bulk, to be stowed in sections (not to contain more than 12,000 bushels,) on a good platform, grain tight, of not less than ten inches from flat of floor, and fourteen inches in bilges, graduated to the sides; which must be clapboard ceiled to the deck. Care must be taken to preserve a water-course between the ceiling and platform. Good shifting boards, secured to the stanchions extending at least six feet downwards and fitted tight to the deck. The stanchions not to be removed, but firmly secured. No loose grain to be stowed in the extreme end of the vessel. No admixture of other goods. Pumps and masts cased grain tight, with sufficient space on the pump well to admit the passage of a man to the heels of the pump.

4. Grain, when stowed in bags, must be dunnaged ten inches on floor, fourteen inches on bilges, two and a half inches on sides up to the deck; between decks, the dunnage must be laid 'thwart ships, at least two and a half inches from the deck. Shifting plank extending four feet from deck beams downward, secured to stanchions. The dunnage in the hold must be covered with boards, sails or mats, grain tight.

5. Vessels exceeding 500 tons register should not be entirely loaded with grain in bulk.

6. Hemp, Flax, Wool and Cotton should be dunnaged nine inches on the floors, and to the upper part of the bilge, the wing bales of the second tier kept six inches of the side at the lower corner, and two and a half inches at the sides. Sand or damp gravel ballast to be covered with boards. Pumps to be frequently sounded and attended to. Sharp-bottomed ships one-third less dunnage in floor and bilges.

7. Tea and Flour in barrels; Flax, Clover and Linseed, or Rice in tierces; Coffee and Cocoa in bags, should always have nine inches at least of good dunnage in the bottom, and fourteen to the upper part of the bilges, with two and a half inches at the sides: allowed to stow six height of tierces and eight heights of barrels. All ships above 600 tons should have 'twixt decks or platform laid for these cargoes, to ease the pressure, caulked 'twixt decks; should have scuppers in the sides, and two and a half inches of dunnage laid athwart ships and not fore-and-aft ways, when in bags or sacks; and when in boxes or casks, not less than one inch. Rice from Calcutta is not unfrequently damaged by Indigo, for want of care in stowing. Avoid using rags as dunnage or horn shavings.

8. Oil, Wine, Spirits, Beer, Molasses, Tar, &c., to be stowed bung up; to have good cross-beds at the quarters, (and not to trust to hanging beds) to be well choaked with wood, and allowed to stow three heights of pipes or butts, four heights of puncheons, and six heights of hogsheads or half puncheons. All moist goods and liquids, such as Salted Hides, bales of Bacon, Butter, Lard, Grease, Castor Oil, &c., should not be stowed too near Dry Goods, whose nature is to absorb moisture. Ship owners have often to pay heavy damages for leakage in casks of Molasses, arising from stowing too many heights without an intervening platform or 'twixt decks.

9. Entire cargoes of Sugar, Saltpetre, and Guano in bags, must have the dunnage carefully attended to, as laid down for other goods. Timber ships are better without 'twixt decks, if loading all Timber or Deals. Brown Sugar to be kept separate from White Sugar, and both kept from direct contact with Saltpetre.

10. Pot and Pearl Ashes, Oil Cake, Tobacco, Bark, Indigo, Madders, Gum, &c., whether in casks, cases or bales, to be dunnaged in the bottom and to the upper part of the bilges, at least nine inches, and two and a half inches at the sides.

11. Miscellaneous Goods, such as boxes of Cheese, kegs and tubs of Lard, or other slight made packages not intended for broken stowage, should be stowed by themselves and dunnaged as other goods.

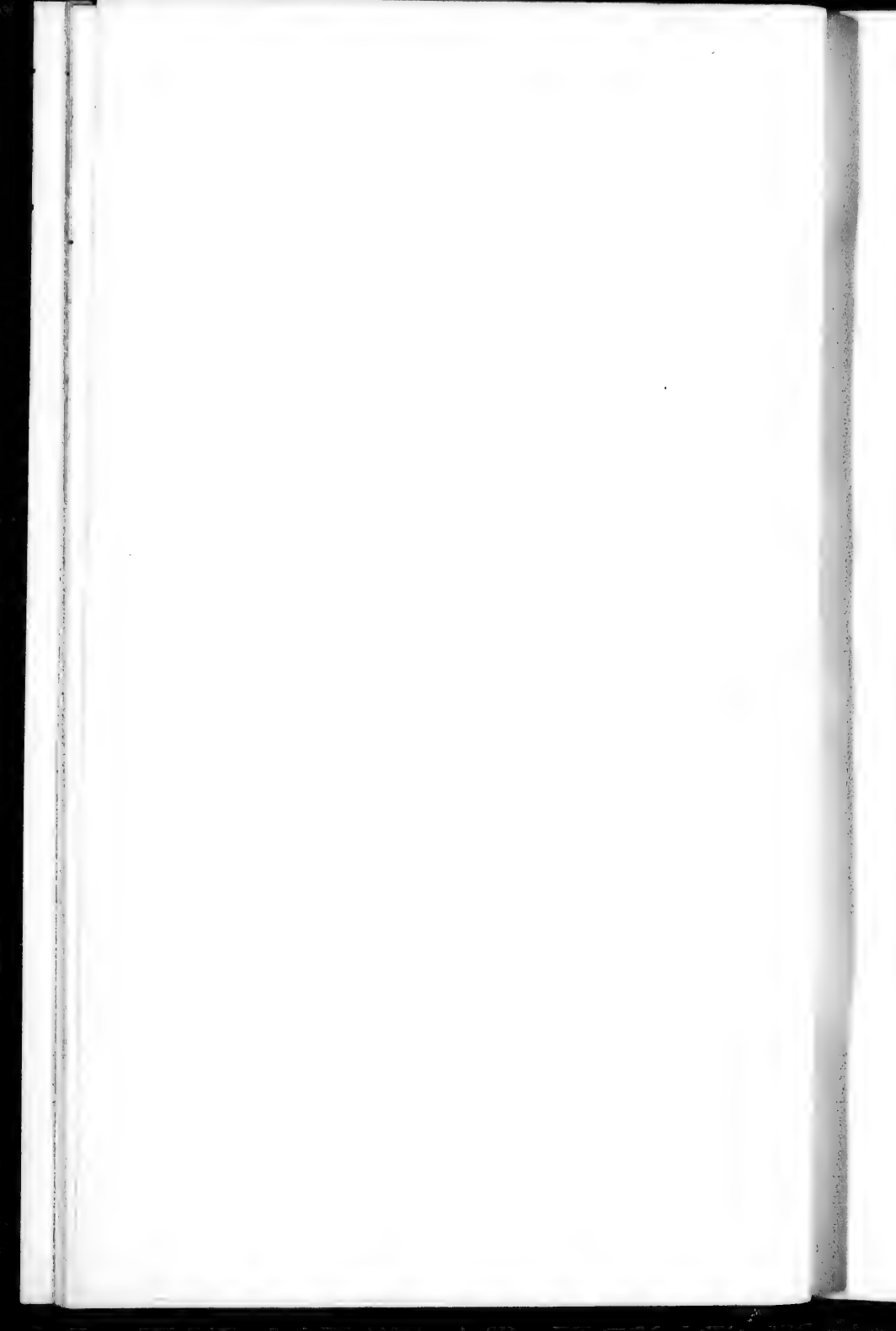
12. Barrels of Provisions and Tallow casks allowed to stow six heights. All metals should be stowed under and separated from goods liable to be damaged by contact.

13. All manufactured goods, also Dry Hides, bales of Silk or other valuable articles, should have two and a half inches of dunnage against the side, to preserve a water-course. Bundles of Sheet Iron, Rods, Pigs of Copper, or any rough, hard substance, should not be allowed to come in contact with bales or any soft package liable to be chafed. When mats can be procured they should be used at the sides for Silk, Tea, &c.

14. Tar, Turpentine, Rosin, &c., to have the flat beds of wood under the quarters, of an inch thick, and allowed to stow six heights.

Very frequent and serious loss falls on Merchants on the upper part of cargoes, particularly in vessels that bring Wheat, Corn, Tobacco, Oil Cake, &c., arising from vapor damage imbibed by Wheat, Flour and other goods stowed in the same vessel with Crude or Spirits of Turpentine or other strong-scented articles: the shippers are to blame for such negligence, for not making due inquiry before shipping.

15. Ships laden with full cargoes of Coal, bound round Cape Horn or Cape of Good Hope, to be provided with approved Ventilators as a preventive against ignition. Must have shifting boards.



e of good quality, properly seasoned, and free from defects.

| | OUTSIDE PLANK. | | | | | | INSIDE PLANK, ETC. |
|----|----------------------------------|------------------------------|------------------------------------|----------------------|--|--|--------------------|
| | Rudder and Indlass. Main pieces. | Keel to First Futtock Heads. | First Futtock Heads to Light Mark. | Light Mark to Wales. | Wales, Black-strakes, Topsides, and Sheer-strakes. | Upper deck Waterway, Spirk't'g & Plank-sheers. | |
| 1 | 12 | 12 | 12 | 12 | 12 | 12 | 12 |
| 2 | 10 | 12 | 12 | 10 | 10 | 10 | 12 |
| 3 | 7 | 12 | 12 | 9 | 8 | 9 | 10 |
| 4 | 7 | 12 | 10 | 8 | 7 | 7 | 9 |
| 5 | 7\|\| | 12 | 10 | 8 | 8 | 10 | 8 |
| 6 | 5 | .. | .. | .. | .. | 5 | 5 |
| 7 | .. | 10 | 9 | 8 | 7 | 10 | 8 |
| 8 | 5\|\| | 9 | 9 | 8 | 7 | 10 | 8 |
| 9 | 5\|\| | 10 | 7 | 4 | .. | .. | 5 |
| 10 | .. | 10 | 7 | 4 | .. | .. | 5 |
| 11 | 4 | 12§ | 8 | 6 | 5 | 5 | 6† |
| 12 | .. | 12§ | 8 | 5 | 4 | 4 | 4 |
| 13 | 4\|\| | 10 | 7 | 4 | 4 | 4 | 5 |
| 14 | 5\|\| | 6 | 6 | 5 | 5 | 5 | 5 |
| 15 | 4\|\| | 6 | 6 | 5 | 4 | 4 | 5 |
| 16 | 4\|\| | 12§ | 8 | 4 | .. | .. | 5 |
| 17 | .. | 6 | 5 | 5 | 5 | 5†† | 5 |
| 18 | .. | 4 | 4 | 4 | 4 | 4 | 4 |

(Mean Rock Elm, and Cowdie, allowed for Floors in Mid-ships, to an extent not exceeding Rule Keel, in ships of the 7 years' grade.

Stern allowed for First Futtocks amid-ships, to the same extent in ships of the 6 years' grade.

Waterways of Upper Deck in ships of the 7 years' grade, if properly fastened, as prescribed the beams are well secured independently of the waterways.

† Third Footlocks and Toptimbers in ships of the 7 years' grade. MEM.—The word growth of the United Kingdom.

years: under the head of "Rudder and Windlass," allowed in ships of 300 tons and under only.

Good Teak of approved quality is proposed to be used, application may be made to the committee allowed a higher grade (not exceeding two years) than as set forth above.

—Lloyd's Register of Shipping, London, 21st September, 1865.

Table A.

Exhibiting the number of Years to be assigned to the different descriptions of Timber used in

| | | TIMBERING. | | | | | |
|----|---|------------|----------------|-----------------|--------------------------------|--------------------------|--|
| | | Floors. | First Footh's. | Second Footh's. | Third Footh's and Top Timbers. | Main and Rider Keelsons. | Transom, Knight-heads, Hawse-Timbers, Aprons, Deadwood, Stem and Strn. Post. |
| 1 | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak; East India Teak, Morung Saul, Greenheart, Morra, and Iron Bark..... | 12 | 12 | 12 | 12 | 12 | 12 |
| 2 | Mahogany of Hard Texture, Cuba Sabicu, Pencil Cedar, Angely, and Venatica..... | 10 | 10 | 10 | 10 | 10 | 10 |
| 3 | Other Continental White Oak, Spanish Chestnut, and Blue Gum..... | 9 | 9‡ | 7 | 7 | 9 | 7 |
| 4 | North American White Oak, American Sweet Chestnut, Stringy Bark, and Red Cedar..... | 8 | 8‡ | 7 | 7 | 8 | 7 |
| 5 | Pitch Pine, Larch, Hackmatack, Tamarac, and Juniper..... | 7 | 7 | 7 | 7 | 8 | 7 |
| 6 | Second-hand English Oak, African Oak, and East India Teak §§ | 7 | 7 | 6 | 6 | 6 | 6 |
| 7 | Cowdie, Huon Pine..... | 6¶ | 6 | 6 | 7 | 7 | 6 |
| 8 | Baltic and American Red Pine..... | 5 | 5 | 5 | 7 | 7 | 5 |
| 9 | English Ash..... | 7 | 6 | 5 | 5 | 5 | 4 |
| 10 | Foreign Ash..... | 5 | 5 | 4 | 4 | 5 | 4 |
| 11 | American Rock Elm and Hickory..... | 6¶ | 6 | 5 | 5 | 6 | 5 |
| 12 | European and American Grey Elm..... | 5 | 5 | 4 | 4 | 4 | 4 |
| 13 | Black Birch and Black Walnut..... | 5¶ | 5** | 4 | 4 | 4 | 4 |
| 14 | Spruce Fir..... | 5 | 5** | 5 | 5 | 5 | 4 |
| 15 | White Cedar..... | 5 | 5 | 4 | 4‡‡ | 4 | 4 |
| 16 | Beech..... | 5¶ | 4 | .. | .. | 4 | .. |
| 17 | Yellow Pine..... | .. | .. | .. | 4 | 4 | 4 |
| 18 | Hemlock..... | 4 | 4 | 4 | 4 | .. | .. |

(1) The committee have recently had under their consideration the standing of Spruce in the Table A. attached to the Rules, and have determined to allow the use of this material in Ships of the 5 years' grade for all parts, except Stem, Sternpost, Transoms, Knightheads, Hawse-Timbers, Aprons, and Deadwood.

* This Table applies as to the Deadwood so far as regards the material to be used from the height of two feet above the rabbet of the keel.

† American Rock Elm allowed for Limber Strakes, Bilge Strakes, and ceiling between them in ships of the 7 years' grade.

‡ If the First Foothooks run up above the Light Watermark, the use of Foreign White Oak is allowed for the 7 years' grade only.

§ The use of Elm and Beech, in ships above the 8 years' grade, to be restricted to a height from the lower part of the Main Keel, of one-third of the internal depth of the ship measured, in midships, from the top of the Limber Strake to the top of the upper deck beams.

Table A.⁽⁴⁾

ns of Timber used in Ships, the same to be of good quality, properly seasoned, and free from defects.

| IMBERING. | | | | | | OUTSIDE PLANK. | | | | | INSIDE PLANK, ETC. |
|--|--------------------------------|--|------------------------|--------|--|---------------------------------------|--|----------------------------|--|--|---|
| Third Foot- hooks and Top Timbers. | Main and Rider Keelsons. | Transoms, Knight- heads, Hawse- Timbers, Apron & Dendw'd Stem and Strn. Post | Beams and Hooks. | Knees. | Rudder and Windlass. — Main Pieces. | Keel to First Futtock Heads. | First Futtock Heads to Light Mark. | Light Mark to Wales. | Wales, Black- Strakes, Topsides, and Sheer- strakes. | Upper deck Wa- terway, Spirk'd'g & Plank- sheers. | Shelves, Clamps, Limber and Bilge Strakes, Ceiling in Hold and betwixt Decks, also Spirketting and Waterway below the Upper Deck. |
| 12 | 12 | 12 | 12 | 12 | 12 | 12 | 12 | 12 | 12 | 12 | 12 |
| 10 | 10 | 10 | 12 | 12 | 10 | 12 | 12 | 10 | 10 | 10 | 12 |
| 7 | 9 | 7 | 8 | 8 | 7 | 12 | 12 | 9 | 8 | 9 | 10 |
| 7 | 8 | 7 | 7 | 7 | 7 | 12 | 10 | 8 | 7 | 7 | 9 |
| 7 | 8 | 7 | 8 | 8 | 7 | 12 | 10 | 8 | 8 | 10 | 8 |
| 6 | 6 | 6 | 6 | 6 | 5 | .. | .. | .. | .. | 5 | 5 |
| 7 | 7 | 6 | 7 | 7 | .. | 10 | 9 | 8 | 7 | 10 | 8 |
| 7 | 7 | 5 | 7 | 7 | 5 | 9 | 9 | 8 | 7 | 10 | 8 |
| 5 | 5 | 4 | 5 | 5 | 5 | 10 | 7 | 4 | .. | .. | 5 |
| 4 | 5 | 4 | 5 | 5 | .. | 10 | 7 | 4 | .. | .. | 5 |
| 5 | 6 | 5 | 5 | 5 | 4 | 12§ | 8 | 6 | 5 | 5 | 6† |
| 4 | 4 | 4 | 5 | 5 | .. | 12§ | 8 | 5 | 4 | 4 | 4 |
| 4 | 4 | 4 | 4 | 4 | 4 | 10 | 7 | 4 | 4 | 4 | 5 |
| 5 | 5 | 4 | 5 | 7 | 5 | 6 | 6 | 5 | 5 | 5 | 5 |
| 4†† | 4 | 4 | 4 | 7 | 4 | 6 | 6 | 5 | 4 | 4 | 5 |
| .. | 4 | .. | .. | .. | 4 | 12§ | 8 | 4 | .. | .. | 5 |
| 4 | 4 | 4 | 4 | 4 | .. | 6 | 5 | 5 | 5 | 5†† | 5 |
| 4 | .. | .. | 4 | 4 | .. | 4 | 4 | 4 | 4 | 4 | 4 |

le A. attached to the
parts, except Stem,
ght of two feet above
s of the 7 years' grade,
is allowed for the 7
rom the lower part of
the Limber Strake

¶ Black Birch, Beech, American Rock Elm, and Cowdie, allowed for Floors in Midships, to an extent not exceeding one-half the entire length of the Keel, in ships of the 7 years' grade.

** Black Birch and Spruce allowed for First Futtocks amidships, to the same extent in ships of the 6 years' grade.
†† Yellow Pine allowed for Waterways of Upper Deck in ships of the 7 years' grade, if properly fastened, as pre-
scribed in Table B, and provided the beams are well secured independently of the waterways.

‡ White Cedar allowed for Third Foot-hooks and Top-timbers in ships of the 7 years' grade. MEM.—The word
"English" includes timber the growth of the United Kingdom.

§ The materials marked thus § under the head of "Rudder and Windlass," allowed in ships of 300 tons and under only.
§§ In cases where second-hand Teak of approved quality is proposed to be used, application may be made to the com-
mittee with a view to its being allowed a higher grade (not exceeding two years) than as set forth above.

—Lloyd's Register of Shipping, London, 21st September, 1885.

TONNAGE***TIMBER A****Floors, sic****Double F****1st Futto**
squared**2nd Futto****3rd Futto****Top Timb****Top Timb****Breastho**
the mic**1Keel, Ste****Keelson,**
of Coun**§Wales....****||Bottom P****Sheer Str**
is no Sl**Ceiling b****¶Waterwa****Ceiling b****Ridge Pla**
Floorhe**Lower D**
Spirket**Upper D****Plankshe****Flat of U****Scarpis****Ditto, wh**

Mould
See Rules, No
*** Shoul**
† When
Cross Chock
ing allowed
‡ The ra
rabbit.

| Tonnage..... | 900 | 950 | 1050 | 1150 | 1250 | 1350 | 1500 | 1750 | 2000 |
|--|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|
| Timber and Space..... | 32 $\frac{1}{2}$ | 32 $\frac{1}{4}$ | 33 $\frac{1}{4}$ | 33 $\frac{1}{2}$ | 33 $\frac{1}{2}$ | 33 $\frac{3}{4}$ | 34 | 34 $\frac{1}{2}$ | 35 |
| Floors, sided and moulded at head | 14 | 14 $\frac{1}{4}$ | 14 $\frac{1}{2}$ | 14 $\frac{3}{4}$ | 15 | 15 $\frac{1}{4}$ | 15 $\frac{1}{4}$ | 15 $\frac{1}{2}$ | 15 $\frac{3}{4}$ |
| Double Floors, sided and moulded | 13 | 13 $\frac{1}{4}$ | 13 $\frac{1}{2}$ | 13 $\frac{3}{4}$ | 14 | 14 $\frac{1}{4}$ | 14 $\frac{1}{4}$ | 14 $\frac{1}{2}$ | 14 $\frac{3}{4}$ |
| 1st Futtocks, sided and moulded squared | 12 $\frac{1}{2}$ | 12 $\frac{3}{4}$ | 13 $\frac{1}{4}$ | 13 $\frac{1}{2}$ | 13 $\frac{3}{4}$ | 14 $\frac{1}{4}$ | 14 $\frac{1}{4}$ | 14 $\frac{1}{2}$ | 14 $\frac{3}{4}$ |
| 2nd Futtocks, sided, if squared | 11 $\frac{1}{2}$ | 11 $\frac{3}{4}$ | 12 $\frac{1}{4}$ | 12 $\frac{1}{2}$ | 12 $\frac{3}{4}$ | 13 $\frac{1}{4}$ | 13 $\frac{1}{4}$ | 13 $\frac{1}{2}$ | 13 $\frac{3}{4}$ |
| 3rd Futtocks and Long Top Timbers | 10 $\frac{1}{2}$ | 10 $\frac{3}{4}$ | 11 $\frac{1}{4}$ | 11 $\frac{1}{2}$ | 11 $\frac{3}{4}$ | 12 $\frac{1}{4}$ | 12 $\frac{1}{4}$ | 12 $\frac{1}{2}$ | 12 $\frac{3}{4}$ |
| Top Timbers (Short), sided, if squared | 9 $\frac{3}{4}$ | 10 | 10 | 10 $\frac{1}{4}$ | 10 $\frac{1}{2}$ | 10 $\frac{3}{4}$ | 10 $\frac{3}{4}$ | 11 | 11 $\frac{1}{4}$ |
| Top Timbers, moulded at head | 7 | 7 $\frac{1}{4}$ | 7 $\frac{1}{4}$ | 7 $\frac{1}{2}$ | 7 $\frac{3}{4}$ | 8 $\frac{1}{2}$ | 8 $\frac{1}{2}$ | 8 $\frac{3}{4}$ | 9 |
| Breasthooks and Wing Transoms the middle | 14 | 14 $\frac{1}{4}$ | 14 $\frac{1}{2}$ | 14 $\frac{3}{4}$ | 15 | 15 $\frac{1}{4}$ | 15 $\frac{1}{4}$ | 15 $\frac{1}{2}$ | 16 |
| Keel, Stem, Apron, and Sternpiece | 15 | 15 $\frac{1}{4}$ | 15 $\frac{1}{2}$ | 15 $\frac{3}{4}$ | 16 | 16 $\frac{1}{4}$ | 16 $\frac{1}{2}$ | 16 $\frac{3}{4}$ | 17 |
| Keelson, also the Mainpiece of Counter upwards, sided at head | 16 | 16 $\frac{1}{4}$ | 16 $\frac{1}{2}$ | 16 $\frac{3}{4}$ | 17 | 17 $\frac{1}{4}$ | 17 $\frac{1}{2}$ | 17 $\frac{3}{4}$ | 18 |
| Wales..... | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 | 6 | 6 | 6 $\frac{1}{4}$ | 6 $\frac{1}{2}$ | 6 $\frac{3}{4}$ | 7 |
| Bottom Plank, from Keel to Wing | 4 $\frac{1}{4}$ | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{1}{2}$ | 4 $\frac{1}{2}$ | 4 $\frac{1}{2}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 5 |
| Sheer Strakes, Topsides, Upper, is no Shelf fitted, and Lower | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 4 $\frac{3}{4}$ | 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{1}{2}$ |
| Ceiling below Hold Beam Clamps | 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ | 3 $\frac{3}{4}$ | 3 $\frac{3}{4}$ | 4 | 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{1}{2}$ |
| Waterway (Hardwood)..... | 7 $\frac{1}{2}$ | 7 $\frac{1}{2}$ | 7 $\frac{1}{2}$ | 7 $\frac{1}{2}$ | 8 | 8 | 8 $\frac{1}{2}$ | 8 $\frac{1}{2}$ | 9 |
| Waterway (Fir)..... | 9 | 9 | 9 | 9 | 9 $\frac{1}{2}$ | 9 $\frac{1}{2}$ | 9 $\frac{1}{2}$ | 9 $\frac{1}{2}$ | 10 |
| Ceiling betwixt Decks..... | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | 3 | 3 | 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ |
| Bidge Plank, inside, Thick Strakes, Floorheads, and Limber Strakes | 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 | 6 $\frac{1}{4}$ | 6 $\frac{1}{4}$ | 6 $\frac{1}{2}$ | 7 |
| Lower Deck Clamp where the Spirketting..... | 5 | 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{1}{2}$ | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 |
| Upper Deck Clamp where a shelf | 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ | 3 $\frac{3}{4}$ | 3 $\frac{3}{4}$ | 4 | 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 5 |
| Planksheer..... | 4 | 4 | 4 | 4 | 4 | 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 5 |
| Plat of Upper Deck..... | 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ | 4 | 4 | 4 | 4 | 4 | 4 | 4 |
| Scarp of Keelson without Rib | ft. in. 7 3 | ft. in. 7 6 | ft. in. 7 6 | ft. in. 7 9 | ft. in. 7 9 | ft. in. 8 0 | ft. in. 8 0 | ft. in. 8 0 | ft. in. 8 0 |
| Doitto, where Rib or Keelson is used | 6 3 | 6 6 | 6 6 | 6 9 | 6 9 | 7 0 | 7 0 | 7 0 | 7 0 |

Moulding of Futtocks and Transoms, see Section 45.
 See Rules, Sec. 38.
 * Should the timber and space be and inside, may be reduced one-sixth in thickness. Furrins are not allowed in this or
 † When the heels of 1st Footboards against Timbers is required, below the underside of the Planksheer, to receive in
 Cross Chocks properly butted, they with alternate through bolts in Shelf, and in Clamp where there is no Shelf.
 ing allowed for double Floors. Tops, Top-gallant forecables, and raised quarter decks, see Rules, Sec. 38. For requirements
 ‡ The rabbet of the Keel, Stem, and Apron, see Rules, Secs. 39, 45, and 62.
 rabbet.

Table B.

Minimum Dimensions of Timbers, Keelson, &c.

| NAME..... TONS. | 50 | 100 | 150 | 200 | 250 | 300 | 350 | 400 | 450 | 500 | 550 | 600 | 650 |
|---|-----------------|-----------------|-----------------|------------------|------------------|------------------|------------------|------------------|------------------|-----------------|------------------|------------------|------------------|
| TIMBER AND SPACE..... INCHES.. | 18 | 19 | 20 | 21 $\frac{1}{2}$ | 23 | 24 $\frac{1}{4}$ | 25 $\frac{3}{4}$ | 27 $\frac{1}{4}$ | 28 $\frac{1}{2}$ | 30 | 30 $\frac{1}{4}$ | 30 $\frac{1}{2}$ | 31 |
| Booms, sided and moulded at Keelson, if squared..... | 7 | 7 $\frac{1}{2}$ | 8 | 8 $\frac{3}{4}$ | 9 $\frac{1}{2}$ | 10 $\frac{1}{4}$ | 11 | 11 $\frac{1}{4}$ | 12 $\frac{1}{4}$ | 13 | 13 $\frac{1}{4}$ | 13 $\frac{1}{2}$ | 13 $\frac{3}{4}$ |
| Double Floors, sided and moulded at Keelson, if squared..... | 6 | 6 $\frac{1}{2}$ | 7 | 7 $\frac{3}{4}$ | 8 $\frac{1}{2}$ | 9 $\frac{1}{4}$ | 10 | 10 $\frac{1}{2}$ | 11 $\frac{1}{4}$ | 12 | 12 $\frac{1}{4}$ | 12 $\frac{1}{2}$ | 12 $\frac{3}{4}$ |
| Futtocks, sided and moulded at Floor Heads, if squared..... | 6 | 6 $\frac{1}{2}$ | 7 | 7 $\frac{3}{4}$ | 8 $\frac{1}{2}$ | 8 $\frac{3}{4}$ | 9 $\frac{1}{4}$ | 10 | 10 $\frac{1}{2}$ | 11 | 11 $\frac{1}{4}$ | 11 $\frac{1}{2}$ | 11 $\frac{3}{4}$ |
| Futtocks, sided, if squared..... | 5 $\frac{1}{2}$ | 6 | 6 $\frac{1}{2}$ | 7 | 7 $\frac{1}{2}$ | 8 | 8 $\frac{1}{2}$ | 9 | 9 $\frac{1}{2}$ | 10 | 10 $\frac{1}{4}$ | 10 $\frac{1}{2}$ | 10 $\frac{3}{4}$ |
| Futtocks and Long Top Timbers, sided, if squared..... | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 | 6 $\frac{1}{2}$ | 7 | 7 $\frac{1}{4}$ | 7 $\frac{3}{4}$ | 8 $\frac{1}{4}$ | 8 $\frac{1}{2}$ | 9 | 9 $\frac{1}{4}$ | 9 $\frac{1}{2}$ | 9 $\frac{3}{4}$ |
| Top Timbers (Short), sided, if squared..... | .. | .. | .. | .. | .. | .. | .. | .. | .. | 9 | 9 $\frac{1}{4}$ | 9 $\frac{1}{2}$ | 9 $\frac{3}{4}$ |
| Top Timbers, moulded at heads, if squared..... | 4 | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 5 | 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 | 6 | 6 $\frac{1}{4}$ | 6 $\frac{1}{2}$ | 6 $\frac{3}{4}$ |
| Footboards and Wing Transom, sided and moulded in the middle..... | 8 | 8 $\frac{1}{2}$ | 9 | 9 $\frac{3}{4}$ | 10 $\frac{1}{4}$ | 10 $\frac{3}{4}$ | 11 $\frac{1}{4}$ | 12 | 12 $\frac{1}{2}$ | 13 | 13 $\frac{1}{4}$ | 13 $\frac{1}{2}$ | 13 $\frac{3}{4}$ |
| Keel, Stem, Apron, and Sternpost, sided and moulded..... | 8 | 9 | 10 | 10 $\frac{3}{4}$ | 11 $\frac{1}{4}$ | 11 $\frac{3}{4}$ | 12 $\frac{1}{4}$ | 13 | 13 $\frac{1}{2}$ | 14 | 14 $\frac{1}{4}$ | 14 $\frac{1}{2}$ | 14 $\frac{3}{4}$ |
| Keelson, also the Mainpiece of Rudder, from lower part of Counter upwards, sided and moulded..... | 9 | 10 | 11 | 11 $\frac{3}{4}$ | 12 $\frac{1}{4}$ | 12 $\frac{3}{4}$ | 13 $\frac{1}{4}$ | 14 | 14 $\frac{1}{2}$ | 15 | 15 $\frac{1}{4}$ | 15 $\frac{1}{2}$ | 15 $\frac{3}{4}$ |
| Wales..... | 3 | 3 $\frac{1}{2}$ | 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 4 $\frac{3}{4}$ | 4 $\frac{3}{4}$ | 5 | 5 | 5 | 5 | 5 |
| Bottom Plank, from Keel to Wales..... | 2 | 2 $\frac{1}{4}$ | 2 $\frac{1}{2}$ | 2 $\frac{3}{4}$ | 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ | 3 $\frac{3}{4}$ | 3 $\frac{3}{4}$ | 4 | 4 | 4 | 4 |
| Deer Strakes, Topsides, Upper Deck Clamp where there is no shelf fitted, and Lower Deck Clamp with a shelf..... | 2 $\frac{1}{4}$ | 2 $\frac{1}{2}$ | 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ | 3 $\frac{3}{4}$ | 3 $\frac{3}{4}$ | 4 | 4 | 4 | 4 | 4 |
| Coiling below Hold Beam Clamp..... | 1 $\frac{1}{2}$ | 1 $\frac{3}{4}$ | 2 | 2 $\frac{1}{4}$ | 2 $\frac{1}{2}$ | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | 3 | 3 | 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ |
| Waterway { Hardwood..... | 3 $\frac{1}{2}$ | 4 | 4 $\frac{1}{2}$ | 5 | 5 | 5 $\frac{1}{2}$ | 5 $\frac{1}{2}$ | 6 | 6 | 6 $\frac{1}{2}$ | 6 $\frac{1}{2}$ | 6 $\frac{1}{2}$ | 7 |
| { Fir..... | 4 | 4 $\frac{1}{2}$ | 5 | 5 $\frac{1}{2}$ | 6 | 6 $\frac{1}{2}$ | 6 $\frac{1}{2}$ | 7 | 7 $\frac{1}{2}$ | 8 | 8 | 8 | 8 |
| Coiling betwixt Decks..... | 1 $\frac{1}{2}$ | 1 $\frac{3}{4}$ | 2 | 2 | 2 $\frac{1}{4}$ | 2 $\frac{1}{4}$ | 2 $\frac{1}{4}$ | 2 $\frac{1}{2}$ | 2 $\frac{1}{2}$ | 2 $\frac{1}{2}$ | 2 $\frac{1}{2}$ | 2 $\frac{1}{2}$ | 2 |
| Wing Plank, inside, Thick Strakes over long and short Floorheads, and Limber Strake..... | 2 $\frac{1}{2}$ | 2 $\frac{3}{4}$ | 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ | 3 $\frac{3}{4}$ | 3 $\frac{3}{4}$ | 4 | 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ |
| Lower Deck Clamp where there is no shelf fitted, and spiketting..... | .. | .. | 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ | 3 $\frac{3}{4}$ | 4 | 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 4 $\frac{3}{4}$ |
| Upper Deck Clamp where a shelf is also fitted..... | 2 | 2 $\frac{1}{4}$ | 2 $\frac{1}{2}$ | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | 3 | 3 | 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ |
| Planksheer..... | 2 | 2 $\frac{1}{4}$ | 2 $\frac{1}{2}$ | 2 $\frac{3}{4}$ | 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ | 3 $\frac{3}{4}$ | 3 $\frac{3}{4}$ | 4 | 4 | 4 | 4 |
| Wid of Upper Deck..... | 2 $\frac{1}{2}$ | 2 $\frac{1}{2}$ | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | 3 | 3 | 3 | 3 | 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ |
| Scarphs of Keelson without Rider..... | ft. in. 4 6 | ft. in. 4 9 | ft. in. 5 0 | ft. in. 5 3 | ft. in. 5 6 | ft. in. 5 10 | ft. in. 6 2 | ft. in. 6 6 | ft. in. 6 9 | ft. in. 7 0 | ft. in. 7 0 | ft. in. 7 0 | ft. in. 7 7 |
| Ortto, where Rider Keelson is added, also Scarphs of Keel..... | 4 0 | 4 3 | 4 6 | 4 9 | 5 0 | 5 2 | 5 4 | 5 6 | 5 9 | 6 0 | 6 0 | 6 0 | 6 6 |

Moulding of Futtocks and Top Timbers to diminish gradually from size given at Floor Heads to that at Top Timber Heads.—

See Rules, Sec. 38.

Should the timber and space be increased, the siding of the timbers to be increased in proportion.—See Rules, Sec. 39.

When the heels of 1st Footboards meet at the middle line on the Keel, under the Keelson, either with full moulding, or with Chocks properly butted, the siding of single Floors, and their moulding at the Keelson, may be reduced to the siding and moulding allowed for double Floors.

The rabbet of the Keel, Stem, and Sternpost to be made so as to leave sufficient substance of wood to form a substantial back for vessels of

§ For br

All the

In any other

This de

and out thro

MEM.—

for vessels of

Table B.

Timbers, Keelson, Keel, Planking, &c.

| 500 | 550 | 600 | 650 | 700 | 750 | 800 | 850 | 900 | 950 | 1050 | 1150 | 1250 | 1350 | 1500 | 1750 | 2000 |
|-----------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|
| 30 | 30 $\frac{1}{4}$ | 30 $\frac{1}{2}$ | 31 | 31 $\frac{1}{4}$ | 31 $\frac{1}{2}$ | 31 $\frac{3}{4}$ | 32 $\frac{1}{4}$ | 32 $\frac{1}{2}$ | 32 $\frac{3}{4}$ | 33 $\frac{1}{4}$ | 33 $\frac{1}{2}$ | 33 $\frac{3}{4}$ | 34 | 34 $\frac{1}{2}$ | 34 $\frac{3}{4}$ | 35 |
| 13 | 13 $\frac{1}{4}$ | 13 $\frac{1}{2}$ | 13 $\frac{3}{4}$ | 14 | 14 $\frac{1}{4}$ | 14 $\frac{1}{2}$ | 14 $\frac{3}{4}$ | 15 | 15 $\frac{1}{4}$ | 15 $\frac{1}{2}$ | 15 $\frac{3}{4}$ | 16 | 16 $\frac{1}{4}$ | 16 $\frac{1}{2}$ | 16 $\frac{3}{4}$ | 17 |
| 12 | 12 $\frac{1}{4}$ | 12 $\frac{1}{2}$ | 12 $\frac{3}{4}$ | 13 | 13 $\frac{1}{4}$ | 13 $\frac{1}{2}$ | 13 $\frac{3}{4}$ | 14 | 14 $\frac{1}{4}$ | 14 $\frac{1}{2}$ | 14 $\frac{3}{4}$ | 15 | 15 $\frac{1}{4}$ | 15 $\frac{1}{2}$ | 15 $\frac{3}{4}$ | 16 |
| 11 | 11 $\frac{1}{4}$ | 11 $\frac{1}{2}$ | 11 $\frac{3}{4}$ | 12 | 12 $\frac{1}{4}$ | 12 $\frac{1}{2}$ | 12 $\frac{3}{4}$ | 13 | 13 $\frac{1}{4}$ | 13 $\frac{1}{2}$ | 13 $\frac{3}{4}$ | 14 | 14 $\frac{1}{4}$ | 14 $\frac{1}{2}$ | 14 $\frac{3}{4}$ | 15 |
| 10 | 10 $\frac{1}{4}$ | 10 $\frac{1}{2}$ | 10 $\frac{3}{4}$ | 11 | 11 $\frac{1}{4}$ | 11 $\frac{1}{2}$ | 11 $\frac{3}{4}$ | 12 | 12 $\frac{1}{4}$ | 12 $\frac{1}{2}$ | 12 $\frac{3}{4}$ | 13 | 13 $\frac{1}{4}$ | 13 $\frac{1}{2}$ | 13 $\frac{3}{4}$ | 14 |
| 9 | 9 $\frac{1}{4}$ | 9 $\frac{1}{2}$ | 9 $\frac{3}{4}$ | 10 | 10 $\frac{1}{4}$ | 10 $\frac{1}{2}$ | 10 $\frac{3}{4}$ | 11 | 11 $\frac{1}{4}$ | 11 $\frac{1}{2}$ | 11 $\frac{3}{4}$ | 12 | 12 $\frac{1}{4}$ | 12 $\frac{1}{2}$ | 12 $\frac{3}{4}$ | 13 |
| 9 | 9 $\frac{1}{4}$ | 9 $\frac{1}{2}$ | 9 $\frac{3}{4}$ | 10 | 10 $\frac{1}{4}$ | 10 $\frac{1}{2}$ | 10 $\frac{3}{4}$ | 11 | 11 $\frac{1}{4}$ | 11 $\frac{1}{2}$ | 11 $\frac{3}{4}$ | 12 | 12 $\frac{1}{4}$ | 12 $\frac{1}{2}$ | 12 $\frac{3}{4}$ | 13 |
| 6 | 6 $\frac{1}{4}$ | 6 $\frac{1}{2}$ | 6 $\frac{3}{4}$ | 7 | 7 $\frac{1}{4}$ | 7 $\frac{1}{2}$ | 7 $\frac{3}{4}$ | 8 | 8 $\frac{1}{4}$ | 8 $\frac{1}{2}$ | 8 $\frac{3}{4}$ | 9 | 9 $\frac{1}{4}$ | 9 $\frac{1}{2}$ | 9 $\frac{3}{4}$ | 10 |
| 13 | 13 $\frac{1}{4}$ | 13 $\frac{1}{2}$ | 13 $\frac{3}{4}$ | 14 | 14 $\frac{1}{4}$ | 14 $\frac{1}{2}$ | 14 $\frac{3}{4}$ | 15 | 15 $\frac{1}{4}$ | 15 $\frac{1}{2}$ | 15 $\frac{3}{4}$ | 16 | 16 $\frac{1}{4}$ | 16 $\frac{1}{2}$ | 16 $\frac{3}{4}$ | 17 |
| 14 | 14 $\frac{1}{4}$ | 14 $\frac{1}{2}$ | 14 $\frac{3}{4}$ | 15 | 15 $\frac{1}{4}$ | 15 $\frac{1}{2}$ | 15 $\frac{3}{4}$ | 16 | 16 $\frac{1}{4}$ | 16 $\frac{1}{2}$ | 16 $\frac{3}{4}$ | 17 | 17 $\frac{1}{4}$ | 17 $\frac{1}{2}$ | 17 $\frac{3}{4}$ | 18 |
| 15 | 15 $\frac{1}{4}$ | 15 $\frac{1}{2}$ | 15 $\frac{3}{4}$ | 16 | 16 $\frac{1}{4}$ | 16 $\frac{1}{2}$ | 16 $\frac{3}{4}$ | 17 | 17 $\frac{1}{4}$ | 17 $\frac{1}{2}$ | 17 $\frac{3}{4}$ | 18 | 18 $\frac{1}{4}$ | 18 $\frac{1}{2}$ | 18 $\frac{3}{4}$ | 19 |
| 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 | 6 $\frac{1}{4}$ | 6 $\frac{1}{2}$ | 6 $\frac{3}{4}$ | 7 | 7 $\frac{1}{4}$ | 7 $\frac{1}{2}$ | 7 $\frac{3}{4}$ | 8 | 8 $\frac{1}{4}$ | 8 $\frac{1}{2}$ | 8 $\frac{3}{4}$ | 9 |
| 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 | 6 $\frac{1}{4}$ | 6 $\frac{1}{2}$ | 6 $\frac{3}{4}$ | 7 | 7 $\frac{1}{4}$ | 7 $\frac{1}{2}$ | 7 $\frac{3}{4}$ | 8 |
| 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 | 6 $\frac{1}{4}$ | 6 $\frac{1}{2}$ | 6 $\frac{3}{4}$ | 7 | 7 $\frac{1}{4}$ | 7 $\frac{1}{2}$ | 7 $\frac{3}{4}$ | 8 |
| 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ | 3 $\frac{3}{4}$ | 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 | 6 $\frac{1}{4}$ | 6 $\frac{1}{2}$ | 6 $\frac{3}{4}$ | 7 |
| 6 $\frac{1}{2}$ | 6 $\frac{1}{2}$ | 6 $\frac{1}{2}$ | 7 | 7 $\frac{1}{4}$ | 7 $\frac{1}{2}$ | 7 $\frac{3}{4}$ | 8 | 8 $\frac{1}{4}$ | 8 $\frac{1}{2}$ | 8 $\frac{3}{4}$ | 9 | 9 $\frac{1}{4}$ | 9 $\frac{1}{2}$ | 9 $\frac{3}{4}$ | 10 | 10 $\frac{1}{4}$ |
| 2 $\frac{1}{2}$ | 2 $\frac{1}{2}$ | 2 $\frac{1}{2}$ | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ | 3 $\frac{3}{4}$ | 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ |
| 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 | 6 $\frac{1}{4}$ | 6 $\frac{1}{2}$ | 6 $\frac{3}{4}$ | 7 | 7 $\frac{1}{4}$ | 7 $\frac{1}{2}$ | 7 $\frac{3}{4}$ | 8 |
| 4 $\frac{1}{2}$ | 4 $\frac{1}{2}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 4 $\frac{3}{4}$ | 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 | 6 $\frac{1}{4}$ | 6 $\frac{1}{2}$ | 6 $\frac{3}{4}$ | 7 | 7 $\frac{1}{4}$ | 7 $\frac{1}{2}$ | 7 $\frac{3}{4}$ |
| 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ | 3 $\frac{3}{4}$ | 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 | 6 $\frac{1}{4}$ | 6 $\frac{1}{2}$ | 6 $\frac{3}{4}$ | 7 |
| 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ | 3 $\frac{3}{4}$ | 3 $\frac{3}{4}$ | 4 | 4 $\frac{1}{4}$ | 4 $\frac{1}{2}$ | 4 $\frac{3}{4}$ | 5 | 5 $\frac{1}{4}$ | 5 $\frac{1}{2}$ | 5 $\frac{3}{4}$ | 6 | 6 $\frac{1}{4}$ | 6 $\frac{1}{2}$ | 6 $\frac{3}{4}$ |
| ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. |
| 7 0 | 7 0 | 7 0 | 7 0 | 7 0 | 7 3 | 7 3 | 7 3 | 7 3 | 7 6 | 7 6 | 7 9 | 7 9 | 8 0 | 8 0 | 8 0 | 8 0 |
| 6 0 | 6 0 | 6 0 | 6 0 | 6 0 | 6 3 | 6 3 | 6 3 | 6 3 | 6 6 | 6 6 | 6 9 | 6 9 | 7 0 | 7 0 | 7 0 | 7 0 |

r Heads.— § For breadth of Wales required in every case, see Section 45.
All the fore and after hoods, both outside and inside, may be reduced one-sixth in thickness. Furrers are not allowed in this or in any other part of a ship.
This depth of Waterway for Faying Surface against Timbers is required, below the underside of the Planksheer, to receive in and out through Bolts at alternate Timbers, with alternate through bolts in Shelf, and in Clamp where there is no Shelf.
MEM.—For relaxations in respect to Poops, Top-gallant forecastles, and raised quarter decks, see Rules, Sec. 38. For requirements for vessels of excessive length as compared with breadth and depth, see Rules, Secs. 39, 45, and 42.

Siding,

Length
of
Beam
and Ship

| Feet. |
|-------|
| 10 |
| 11 |
| 12 |
| 13 |
| 14 |
| 15 |
| 16 |
| 17 |
| 18 |
| 19 |
| 20 |
| 21 |
| 22 |
| 23 |
| 24 |
| 25 |
| 26 |
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| 30 |
| 31 |
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| 33 |
| 34 |
| 35 |
| 36 |
| 37 |
| 38 |
| 39 |
| 40 |

N. B.—T
dies here p

Table C

Siding, and Moulding of Beams. Section 46.

| Length of Beam in feet. | HOLD BEAMS. | | D. in. | 0 | 350 | 400 | 450 | 500 | 700 | 900 | 1350 |
|-------------------------|--------------------|------------------|--------|----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|
| | Sided and moulded. | Moulded at ends. | | | | | | | | | |
| 10 | .. | .. | 6 | | $1\frac{3}{16}$ | $1\frac{3}{16}$ | $1\frac{4}{16}$ | $1\frac{4}{16}$ | $1\frac{5}{16}$ | $1\frac{6}{16}$ | $1\frac{8}{16}$ |
| 11 | .. | .. | | | | | | | | | |
| 12 | .. | .. | | | | | | | | | |
| 13 | .. | .. | | | | | | | | | |
| 14 | .. | .. | 7 | | $1\frac{4}{16}$ | $1\frac{5}{16}$ | $1\frac{5}{16}$ | 1 | $1\frac{2}{16}$ | $1\frac{3}{16}$ | $1\frac{4}{16}$ |
| 15 | 8 | $6\frac{3}{4}$ | | | | | | | | | |
| 16 | $8\frac{1}{2}$ | $7\frac{1}{2}$ | | | | | | | | | |
| 17 | $8\frac{3}{4}$ | $7\frac{3}{4}$ | | | | | | | | | |
| 18 | 9 $\frac{1}{2}$ | $7\frac{3}{4}$ | | | 1 | $1\frac{1}{16}$ | $1\frac{2}{16}$ | $1\frac{2}{16}$ | $1\frac{3}{16}$ | $1\frac{4}{16}$ | $1\frac{6}{16}$ |
| 19 | 9 $\frac{1}{2}$ | 8 | | | | | | | | | |
| 20 | 10 | $8\frac{1}{2}$ | | | | | | | | | |
| 21 | $10\frac{1}{4}$ | $8\frac{3}{4}$ | | | | | | | | | |
| 22 | $10\frac{1}{2}$ | 9 | 8 | | $1\frac{2}{16}$ | $1\frac{3}{16}$ | $1\frac{3}{16}$ | $1\frac{4}{16}$ | $1\frac{4}{16}$ | $1\frac{5}{16}$ | 1 |
| 23 | 11 | $9\frac{1}{4}$ | | | | | | | | | |
| 24 | $11\frac{1}{4}$ | $9\frac{1}{2}$ | | | | | | | | | |
| 25 | $11\frac{3}{4}$ | $9\frac{3}{4}$ | | | | | | | | | |
| 26 | 12 | 10 | | | $1\frac{4}{16}$ | $1\frac{5}{16}$ | $1\frac{5}{16}$ | $1\frac{6}{16}$ | $1\frac{6}{16}$ | $1\frac{7}{16}$ | $1\frac{8}{16}$ |
| 27 | $12\frac{1}{4}$ | $10\frac{1}{4}$ | | | | | | | | | |
| 28 | $12\frac{1}{2}$ | $10\frac{1}{2}$ | | | | | | | | | |
| 29 | $12\frac{3}{4}$ | $10\frac{3}{4}$ | | | | | | | | | |
| 30 | 13 | 11 | 9 | | $1\frac{5}{16}$ | $1\frac{6}{16}$ | $1\frac{6}{16}$ | $1\frac{7}{16}$ | $1\frac{7}{16}$ | 1 | $1\frac{2}{16}$ |
| 31 | $13\frac{1}{4}$ | $11\frac{1}{4}$ | | | | | | | | | |
| 32 | $13\frac{1}{2}$ | $11\frac{1}{2}$ | | | | | | | | | |
| 33 | $13\frac{3}{4}$ | $11\frac{3}{4}$ | | | | | | | | | |
| 34 | 14 | $11\frac{3}{4}$ | 10 | $2\frac{3}{8}$ | $2\frac{3}{8}$ | 3 | 3 | $3\frac{1}{4}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ |
| 35 | $14\frac{1}{4}$ | 12 | | | | | | | | | |
| 36 | $14\frac{1}{2}$ | $12\frac{1}{4}$ | | | | | | | | | |
| 37 | $14\frac{3}{4}$ | $12\frac{1}{2}$ | | | | | | | | | |
| 38 | 15 | $12\frac{1}{2}$ | | | | | | | | | |
| 39 | $15\frac{1}{4}$ | $12\frac{3}{4}$ | 11 | $1\frac{1}{4}$ | $1\frac{1}{4}$ | $1\frac{1}{4}$ | $1\frac{3}{8}$ | $1\frac{3}{8}$ | $1\frac{3}{8}$ | $1\frac{1}{2}$ | $1\frac{1}{2}$ |
| 40 | $15\frac{1}{2}$ | 13 | | | | | | | | | |

N. B.—The size of Orlop Beams to be through and clenched, as prescribed in Section 46, and to be of good size here prescribed.

Lloyd's Regu

Lloyd's Register of Shipping, 10th May, 1861.

Design, and Moulding of Beams. Sec. 11.

| No. of ship. | HOLD BEAMS. | | DECK BEAMS. | |
|--------------|--------------------|------------------|--------------------|------------------|
| | Sided and moulded. | Moulded at ends. | Sided and moulded. | Moulded at ends. |
| | Inches. | Inches. | Inches. | Inches. |
| 10 | .. | .. | 4 $\frac{1}{2}$ | 3 $\frac{3}{4}$ |
| 11 | .. | .. | 5 | 4 |
| 12 | .. | .. | 5 $\frac{1}{4}$ | 4 $\frac{1}{2}$ |
| 13 | .. | .. | 5 $\frac{1}{2}$ | 4 $\frac{1}{2}$ |
| 14 | .. | .. | 5 $\frac{3}{4}$ | 4 $\frac{3}{4}$ |
| 15 | 8 | 6 $\frac{3}{4}$ | 6 $\frac{1}{2}$ | 5 $\frac{1}{4}$ |
| 16 | 8 $\frac{1}{2}$ | 7 | 6 $\frac{1}{2}$ | 5 $\frac{1}{2}$ |
| 17 | 8 $\frac{3}{4}$ | 7 $\frac{1}{2}$ | 6 $\frac{3}{4}$ | 5 $\frac{1}{2}$ |
| 18 | 9 $\frac{1}{4}$ | 7 $\frac{3}{4}$ | 7 | 5 $\frac{3}{4}$ |
| 19 | 9 $\frac{1}{2}$ | 8 | 7 $\frac{1}{4}$ | 6 |
| 20 | 10 | 8 $\frac{1}{2}$ | 7 $\frac{1}{2}$ | 6 $\frac{1}{4}$ |
| 21 | 10 $\frac{1}{4}$ | 8 $\frac{3}{4}$ | 7 $\frac{3}{4}$ | 6 $\frac{1}{2}$ |
| 22 | 10 $\frac{1}{2}$ | 9 | 8 | 6 $\frac{1}{2}$ |
| 23 | 11 | 9 $\frac{1}{4}$ | 8 $\frac{1}{4}$ | 6 $\frac{3}{4}$ |
| 24 | 11 $\frac{1}{4}$ | 9 $\frac{1}{2}$ | 8 $\frac{1}{2}$ | 7 |
| 25 | 11 $\frac{3}{4}$ | 9 $\frac{3}{4}$ | 8 $\frac{1}{2}$ | 7 $\frac{1}{4}$ |
| 26 | 12 | 10 | 8 $\frac{3}{4}$ | 7 $\frac{1}{4}$ |
| 27 | 12 $\frac{1}{4}$ | 10 $\frac{1}{4}$ | 9 | 7 $\frac{1}{2}$ |
| 28 | 12 $\frac{1}{2}$ | 10 $\frac{1}{2}$ | 9 | 7 $\frac{1}{2}$ |
| 29 | 12 $\frac{3}{4}$ | 10 $\frac{3}{4}$ | 9 $\frac{1}{4}$ | 7 $\frac{1}{4}$ |
| 30 | 13 | 11 | 9 $\frac{1}{2}$ | 8 |
| 31 | 13 $\frac{1}{4}$ | 11 $\frac{1}{4}$ | 9 $\frac{1}{2}$ | 8 |
| 32 | 13 $\frac{1}{2}$ | 11 $\frac{1}{2}$ | 9 $\frac{3}{4}$ | 8 $\frac{1}{4}$ |
| 33 | 13 $\frac{3}{4}$ | 11 $\frac{3}{4}$ | 10 | 8 $\frac{1}{4}$ |
| 34 | 14 | 11 $\frac{3}{4}$ | 10 | 8 $\frac{1}{2}$ |
| 35 | 14 $\frac{1}{4}$ | 12 | 10 $\frac{1}{4}$ | 8 $\frac{1}{2}$ |
| 36 | 14 $\frac{1}{2}$ | 12 $\frac{1}{4}$ | 10 $\frac{1}{4}$ | 8 $\frac{1}{2}$ |
| 37 | 14 $\frac{3}{4}$ | 12 $\frac{1}{2}$ | 10 $\frac{1}{2}$ | 8 $\frac{3}{4}$ |
| 38 | 15 | 12 $\frac{1}{2}$ | 10 $\frac{1}{2}$ | 8 $\frac{3}{4}$ |
| 39 | 15 $\frac{1}{4}$ | 12 $\frac{3}{4}$ | 10 $\frac{1}{2}$ | 9 |
| 40 | 15 $\frac{1}{2}$ | 13 | 10 $\frac{3}{4}$ | 9 |

the siding and moulding of all the Beams to be the same as those main-ships, except those at the *after end* of the Ship, which may be reduced in proportion to their diminished length.

MEAS.—When SPURCE, WHITE CEDAR, or YELLOW PINE, is used for Beams, the dimensions are to be increased. *See Table, No. 10.*

LEM.—When SPRUCE, WHITE CEDAR, or YELLOW PINE is used for Beams, the dimensions are to be increased.—*See Rule*; *See*, (6), in proportion to their diminished length.

| TONNAGE | 50 | 100 |
|--|-------|-------|
| Heel-Knee, Stemson, and Deadwood Bolts | 1 1/2 | 1 1/2 |
| Bolts in Sister Keelsons, Scarphs of Keel,* Arms of Breast Hooks, Pointers, Crutches, Riders, Hanging and Lodging Knees to Hold or Lower Deck Beams (except in and out Throat Bolts of Hanging Knees, which must be larger), also in and out Bolts of Shelf, Clamp, and Waterway of Hold or Lower Deck Beams, and the in and out Throat Bolts of Upper Deck Hanging Knees. | 1 1/2 | 1 3/4 |
| Keelson Bolts (one through Keel at each Floor), Throats of Transoms, Throats of Breasthooks, and Throats of Hanging Knees to Hold or Lower Deck Beams.....) | 1 1/2 | 1 3/4 |
| Bilge, Limber Strake, and Through Butt Bolts | 1 1/2 | 1 3/4 |
| Other Butt Bolts | 1 1/2 | 1 3/4 |
| Bolts through heels of cant timbers at fore and after Deadwood. In and out Bolts of Upper Deck Waterway, Shelf and Clamp, also Arms of Hanging and Lodging Knees, except in and out Throat Bolts of Hanging Knees, which must be larger | 1 1/2 | 1 1/2 |
| Pintles of Rudder { The Braces of which must extend so as to receive not less than Two Bolts on the Planking on each side.....) | 1 7/8 | 2 |
| Hardwood Treennails | 1 | 1 |

—The size of Orlop Beams to be the mean of the
re prescribed.

Lloyd's Register of Shipping.
26th May, 1863

* NUMBER OF BOLTS IN SCARPHS OF KEEL:—

| | |
|---|-------|
| In Ships of 150 Tons and under ..... | ..... |
| “ above 150 Tons and under 500 Tons ..... | ..... |
| “ 500 Tons and above ..... | ..... |

Table D.

Sizes of Bolts, Pivots of Rudder, and Treennails. Section 46.

| | 50 | 100 | 150 | 200 | 250 | 300 | 350 | 400 | 450 | 500 | 700 | 900 | 1350 |
|---|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| ches | 1 1/2 | 1 1/2 | 1 | 1 | 1 1/8 | 1 3/8 | 1 3/8 | 1 3/8 | 1 3/8 | 1 3/8 | 1 3/8 | 1 3/8 | 1 3/8 |
| fast lg- in fast and in es. | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 | 1 1/8 | 1 1/8 | 1 1/8 |
| nts of | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 | 1 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 |
| | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 | 1 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 |
| | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 |
| | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 |
| ad- py. ing ng | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/8 | 1 | 1 1/8 |
| as on | 1 1/8 | 2 | 2 | 2 1/2 | 2 3/8 | 2 1/2 | 2 3/8 | 2 3/4 | 3 | 3 | 3 1/4 | 3 1/2 | 3 1/2 |
| | 1 | 1 | 1 | 1 1/8 | 1 1/8 | 1 1/8 | 1 1/4 | 1 1/4 | 1 1/4 | 1 1/4 | 1 1/4 | 1 1/4 | 1 1/4 |

6 Bolts.
7 Tons
8 "

N. B.—Bolts to be through and clenched, as prescribed in Section 46, and to be of good quality, well made, with suitable heads, and be tightly driven.

Lloyd's Register of Shipping, 16th May, 1861.

Ta

No. of E

Tons.

150

200

250

300

350

400

450

500

550

600

650

700

750

800

900

1000

1100

1350

Table E.

No. of Hanging Knees.
Section 41.

uilt Ships and Fir Ships.

| Tons. | To Hold Beams. | To Upper Deck Beams. | 000 | 1100 | 1200 | 1300 | 1400 | 1500 | 1600 | 1700 | 1800 | 1900 | 2000 |
|-------|----------------|----------------------|----------------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|
| | Pairs. | Pairs. | erSec | tion | 2 | | | | | | | | |
| 150 | 4 | 4 | s, one to every Beam | | | | | | | | | | |
| 200 | 4 | 6 | | | | | | | | | | | |
| 250 | 5 | 7 | | | | | | | | | | | |
| 300 | 6 | 8 | $4\frac{1}{4}$ | $4\frac{1}{2}$ | $4\frac{1}{2}$ | $4\frac{3}{4}$ | $4\frac{3}{4}$ | 5 | 5 | $5\frac{1}{4}$ | $5\frac{1}{4}$ | $5\frac{1}{2}$ | $5\frac{1}{2}$ |
| 350 | 7 | 9 | | | | | | | | | | | |
| 400 | 8 | 10 | 4 | $4\frac{1}{4}$ | $4\frac{1}{4}$ | $4\frac{1}{2}$ | $4\frac{1}{2}$ | $4\frac{1}{2}$ | $4\frac{1}{2}$ | $4\frac{3}{4}$ | $4\frac{3}{4}$ | $4\frac{3}{4}$ | $4\frac{3}{4}$ |
| 450 | 8 | 11 | | | | | | | | | | | |
| 500 | 9 | 12 | $2\frac{3}{4}$ | 3 | 3 | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{3}{4}$ | $3\frac{3}{4}$ |
| 550 | 9 | 13 | | | | | | | | | | | |
| 600 | 10 | 14 | $4\frac{1}{4}$ | $4\frac{1}{2}$ | $4\frac{1}{2}$ | $4\frac{3}{4}$ | $4\frac{3}{4}$ | 5 | 5 | $5\frac{1}{4}$ | $5\frac{1}{4}$ | $5\frac{1}{2}$ | $5\frac{1}{2}$ |
| 650 | 10 | 15 | | | | | | | | | | | |
| 700 | 11 | 16 | 3 | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{3}{4}$ | $3\frac{3}{4}$ |
| 750 | 11 | 17 | | | | | | | | | | | |
| 800 | 12 | 18 | $2\frac{3}{4}$ | 3 | 3 | 3 | 3 | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ |
| 900 | 13 | 20 | | | | | | | | | | | |
| 1000 | 14 | 22 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 |
| 1100 | 15 | 24 | | | | | | | | | | | |
| 1250 | 17 | 26 | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. | ft. in. |
| | | | 3 9 | 4 0 | 4 0 | 4 0 | 4 0 | 4 0 | 4 0 | 4 0 | 4 0 | 4 0 | 4 0 |

Idle Deck Knees may be three inches shorter than those of the Lower Deck, to be less in length than one and a half the length of their Beam Arms. rs, which are 3 ft. 6 in. in length, to have not less than four Bolts; and shorter in three Bolts. Side Arms of all Hanging Knees to have at least one Bolt more

Table E.

of Hanging Knees.

Section 41.

| Tons. | To Hold Beams. | To Upper Deck Beams. |
|-------|----------------|----------------------|
| | Pairs. | Pairs. |
| 50 | 4 | 4 |
| 200 | 4 | 6 |
| 250 | 5 | 7 |
| 300 | 6 | 8 |
| 350 | 7 | 9 |
| 400 | 8 | 10 |
| 450 | 8 | 11 |
| 500 | 9 | 12 |
| 550 | 9 | 13 |
| 600 | 10 | 14 |
| 650 | 10 | 15 |
| 700 | 11 | 16 |
| 750 | 11 | 17 |
| 800 | 12 | 18 |
| 900 | 13 | 20 |
| 1000 | 14 | 22 |
| 1100 | 15 | 24 |
| 1250 | 17 | 26 |

Table

Minimum Dimensions of Iron Knees and Knee Riders for

Section

| TONNAGE TONS | 150 | 200 | 250 | 300 | 350 | 400 | 450 | 500 | 550 | 600 | 650 |
|---|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-------------|
| Number of Hanging Knees to Hold or Lower Deck Beams..... Pairs | 3* | 4 | 6 | 8 | 9 | Upwards, one | Knee | Rider | to every | | |
| Number of Hanging Knees to Upper and Middle Deck Beams Pairs | 4 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | 13 | 14 | |
| Breadth of Knees and Riders to Hold or Lower Deck Beams..... Inches | 3 | 3 | 3 | 3 | 3 | 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ | |
| Breadth of Upper Deck Knees, where there are two Decks, and of Middle Deck Knees, where there are three Decks..... Inches | 3 | 3 | 3 | 3 | 3 | 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ | |
| Thickness of Riders at the joints or butts of the Timbers Inches | 1 $\frac{1}{4}$ | 1 $\frac{1}{4}$ | 1 $\frac{1}{2}$ | 1 $\frac{1}{2}$ | 1 $\frac{1}{2}$ | 1 $\frac{1}{2}$ | 1 $\frac{3}{4}$ | 1 $\frac{3}{4}$ | 2 | 2 | |
| Thickness of Knees to Lower Deck or Hold Beams and Knee Riders at the Angle of the Throat..... Inches | 2 $\frac{1}{2}$ | 2 $\frac{1}{2}$ | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | 3 | 3 | 3 $\frac{1}{4}$ | 3 $\frac{1}{4}$ | 3 $\frac{1}{2}$ | 3 $\frac{1}{2}$ | |
| Thickness of Knees to Lower Deck or Hold Beams and Knee Riders at the Throat Bolts Inches | 1 $\frac{3}{4}$ | 1 $\frac{3}{4}$ | 2 | 2 | 2 $\frac{1}{4}$ | 2 $\frac{1}{4}$ | 2 $\frac{1}{2}$ | 2 $\frac{1}{2}$ | 2 $\frac{3}{4}$ | 2 $\frac{3}{4}$ | |
| Thickness of Knees to Upper or Middle Deck at the Throat Bolts† Inches | 1 $\frac{1}{2}$ | 1 $\frac{1}{2}$ | 1 $\frac{3}{4}$ | 1 $\frac{3}{4}$ | 2 | 2 | 2 $\frac{1}{4}$ | 2 $\frac{1}{4}$ | 2 $\frac{1}{2}$ | 2 $\frac{1}{2}$ | |
| Thickness of Hanging Knees (not Riders) at the ends..... Inches | $\frac{5}{8}$ | $\frac{5}{8}$ | $\frac{3}{4}$ | $\frac{3}{4}$ | $\frac{3}{4}$ | $\frac{3}{4}$ | $\frac{7}{8}$ | $\frac{7}{8}$ | $\frac{7}{8}$ | $\frac{7}{8}$ | |
| Length of Beam Arms of Knees and Knee Riders for Lower Deck or Hold Beams† | ft. in. 2 6 | ft. in. 2 6 | ft. in. 2 9 | ft. in. 2 9 | ft. in. 3 0 | ft. in. 3 0 | ft. in. 3 3 | ft. in. 3 3 | ft. in. 3 3 | ft. in. 3 6 | ft. in. 3 6 |

NOTE—The Bolts in all Iron Riders in Hold to be not more than twenty-one inches apart on the average. Standards upon the Beams of such Ships are not admitted as substitutes for Hanging Knees below them. For sizes of Bolts, see Table D.

* Provided the depth of Hold be 13 feet or upwards.

† Breadth and thickness of Knees for Upper Deck, where there are three Decks, may be one-sixth less.

Table F.

Knees and Knee Riders for British North American built Ships and Fir Ships.

Section 62.

| 50 | 500 | 550 | 600 | 650 | 700 | 750 | 800 | 900 | 1000 | 1100 | 1200 | 1300 | 1400 | 1500 | 1600 | 1700 | 1800 | 1900 | 2000 |
|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|-----------------------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|
| one | Knee | Rider | to every | Beam, or | Knees | and Riders | as per | Section 62 | | | | | | | | | | | |
| 1 | 12 | 13 | 14 | 15 | 16 | 17 | 18 | Upwards, one to every Beam. | | | | | | | | | | | |
| $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{3}{4}$ | $3\frac{3}{4}$ | 4 | 4 | $4\frac{1}{4}$ | $4\frac{1}{4}$ | $4\frac{1}{2}$ | $4\frac{1}{2}$ | $4\frac{3}{4}$ | $4\frac{3}{4}$ | 5 | 5 | $5\frac{1}{4}$ | $5\frac{1}{4}$ | $5\frac{1}{2}$ | $5\frac{1}{2}$ |
| $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{3}{4}$ | $3\frac{3}{4}$ | 4 | 4 | $4\frac{1}{4}$ | $4\frac{1}{4}$ | $4\frac{1}{2}$ | $4\frac{1}{2}$ | $4\frac{1}{2}$ | $4\frac{1}{2}$ | $4\frac{3}{4}$ | $4\frac{3}{4}$ | $4\frac{3}{4}$ | $4\frac{3}{4}$ |
| $3\frac{1}{4}$ | $1\frac{3}{4}$ | 2 | 2 | $2\frac{1}{4}$ | $2\frac{1}{4}$ | $2\frac{1}{2}$ | $2\frac{1}{2}$ | $2\frac{3}{4}$ | $2\frac{3}{4}$ | 3 | 3 | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{3}{4}$ | $3\frac{3}{4}$ |
| $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{3}{4}$ | $3\frac{3}{4}$ | 4 | 4 | $4\frac{1}{4}$ | $4\frac{1}{4}$ | $4\frac{1}{2}$ | $4\frac{1}{2}$ | $4\frac{3}{4}$ | $4\frac{3}{4}$ | 5 | 5 | $5\frac{1}{4}$ | $5\frac{1}{4}$ | $5\frac{1}{2}$ | $5\frac{1}{2}$ |
| $2\frac{1}{2}$ | $2\frac{1}{2}$ | $2\frac{3}{4}$ | $2\frac{3}{4}$ | $2\frac{3}{4}$ | $2\frac{3}{4}$ | 3 | 3 | 3 | 3 | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ | $3\frac{3}{4}$ | $3\frac{3}{4}$ |
| $2\frac{1}{4}$ | $2\frac{1}{4}$ | $2\frac{1}{2}$ | $2\frac{1}{2}$ | $2\frac{1}{2}$ | $2\frac{1}{2}$ | $2\frac{3}{4}$ | $2\frac{3}{4}$ | $2\frac{3}{4}$ | $2\frac{3}{4}$ | 3 | 3 | 3 | 3 | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{4}$ | $3\frac{1}{2}$ | $3\frac{1}{2}$ |
| $\frac{7}{8}$ | $\frac{7}{8}$ | $\frac{7}{8}$ | $\frac{7}{8}$ | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 |
| ft. 3 | ft. in. 3 3 | ft. in. 3 3 | ft. in. 3 6 | ft. in. 3 6 | ft. in. 3 6 | ft. in. 3 9 | ft. in. 3 9 | ft. in. 3 9 | ft. in. 3 9 | ft. in. 4 0 | ft. in. 4 0 | ft. in. 4 0 | ft. in. 4 0 | ft. in. 4 0 | ft. in. 4 0 | ft. in. 4 0 | ft. in. 4 0 | ft. in. 4 0 | ft. in. 4 0 |

Knees apart on the average.
Hanging Knees below them.

one-sixth less.

‡ Beam Arms of Upper and Middle Deck Knees may be three inches shorter than those of the Lower Deck. Side Arms of Hanging Knees not to be less in length than one and a half the length of their Beam Arms. Beam Arms of Knees and Knee Riders, which are 3 ft. 6 in. in length, to have not less than four Bolts; and shorter than that length, to have not less than three Bolts. Side Arms of all Hanging Knees to have at least one Bolt more than in the Beam Arms.

Lloyd's Register of Shipping, 27th May, 1858.

PARTS OF
OF

LOORS

1st FUT

2nd FUT

3rd FUT
a
TOP TIM

MAIN a
KEELSC

STEM a
POSTS

TRANS
KNIGHT
HAWSE
D
APRON
DEADV

BEAMS
HOOKS

KNEES

* This table
of the keels
If the F
made only
MEM. -1

No. 1 the same will be applicable to the several Terms of Years

| PARTS OF THE HULL OF VESSEL. | SIX YEARS. | FIVE YEARS. | FOUR YEARS. |
|--|--|---|---|
| LOORS..... | The same as in the preceding Class, and admit— *Cordie, Huon Pine, *American Rock Elm, Hickory. | The same as in the preceding Class, and admit— Baltic and Amer. Red Pine, Foreign Ash, Europ. and Am. Grey Elm, *B. Birch, B. Walnut, Spruce Fir, White Cedar, *Beech. | The same as in the preceding Class, and admit— Hemlock. |
| 1st FUTTOCKS... | The same as in the preceding Class, and admit— Cordie, Huon Pine, English Ash, American Rock Elm, Hickory. | The same as in the preceding Class, and admit— Baltic and Amer. Red Pine, Foreign Ash, Europ. and Am. Grey Elm, **B. Birch, B. Walnut, **Spruce Fir, and White Cedar. | The same as in the preceding Class, and admit— Beech, Hemlock. |
| 2nd FUTTOCKS... | The same as in the preceding Class, and admit— Second-hand English Oak, African Oak, §§East India Teak, Cordie, Huon Pine. | The same as in the preceding Class, and admit— Baltic and Amer. Red Pine, English Ash, American Rock Elm, Hickory. | The same as in the preceding Class, and admit— Foreign Ash, European and Amer. Grey Elm, Black Birch and Black Walnut, Spruce, Fir, White Cedar, and Hemlock. |
| 3rd FUTTOCKS... and TOP TIMBERS... | The same as in the preceding Class, and admit— Second-hand English Oak, African Oak, §§East India Teak. | The same as in the preceding Class, and admit— English Ash, American Rock Elm, Hickory. | The same as in the preceding Class, and admit— Foreign Ash, European and Amer. Grey Elm, Black Birch, Bk Walnut, Spruce, Fir, White Cedar, Yellow Pine, and Hemlock. |
| MAIN and RIDE KEELSONS | The same as in the preceding Class, and admit— American Rock Elm, Hickory, Second-hand English Oak, African Oak, §§East India Teak. | The same as in the preceding Class, and admit— English Ash, Foreign Ash. | The same as in the preceding Class, and admit— European and Amer. Grey Elm, Black Birch, Black Walnut, Spruce Fir, White Cedar, Beech, and Yellow Pine. |
| STEM and STEEL POSTS..... | The same as in the preceding Class, and admit— Cordie, Huon Pine. | The same as in the preceding Class, and admit— Baltic and Amer. Red Pine, American Rock Elm, Hickory, Second-hand English Oak, African Oak, §§East India Teak. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, European and Amer. Grey Elm, Black Birch, Black Walnut, Spruce Fir, White Cedar, and Yellow Pine. |
| TRANSOMS KNIGHT HEAD HAWSE TIMBERS APRON | The same as in the preceding Class, and admit— Cordie, Huon Pine, Second-hand English Oak, African Oak, §§East India Teak. | The same as in the preceding Class, and admit— Baltic and Amer. Red Pine, American Rock Elm, Hickory. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, European and Amer. Grey Elm, Black Birch, Black Walnut, Spruce Fir, White Cedar, and Yellow Pine. |
| DEADWOOD*... | The same as in the preceding Class, and admit— Second-hand English Oak, African Oak, §§East India Teak. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, American Rock Elm, Hickory, European and American Grey Elm. | The same as in the preceding Class, and admit— Black Birch, Black Walnut, Spruce Fir, White Cedar, Yellow Pine, Hemlock. |
| BEAMS | The same as in the preceding Class, and admit— Second-hand English Oak, African Oak, §§East India Teak. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, American Rock Elm, Hickory, European and American Grey Elm. | The same as in the preceding Class, and admit— Black Birch, Black Walnut, Spruce Fir, White Cedar, Yellow Pine, Hemlock. |
| HOOKS | The same as in the preceding Class, and admit— Second-hand English Oak, African Oak, §§East India Teak. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, American Rock Elm, Hickory, European and American Grey Elm. | The same as in the preceding Class, and admit— Black Birch, Black Walnut, Yellow Pine, Hemlock. |
| KNEES..... | The same as in the preceding Class, and admit— Second-hand English Oak, African Oak, §§East India Teak. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, American Rock Elm, Hickory, European and American Grey Elm. | The same as in the preceding Class, and admit— Black Birch, Black Walnut, Yellow Pine, Hemlock. |

* This table applies to the keel.
If the First Part is made only of one piece.
Note.—The word

allowed for Floors in midships, to an extent not exceeding one-half the entire length of the keel.
** Black Birch and Spruce allowed for First Futtocks amidships, to the same extent in Cedar allowed for Third Futtocks and Top-timbers in Ships of the SEVEN YEARS' grade, quality is proposed to be used, application may be made to the Committee with a view to its (ours) than as set forth above.

No. 1.—A TABLE, exhibiting the different Descriptions of TIMBER, of good Quality, to be used in the Timber appointed for Ships to remain on the Character

| Parts of the Frame of Vessel. | TWELVE YEARS. | TEN YEARS. | NINE YEARS. | EIGHT YEARS. | SEVEN YEARS. |
|---|---|--|---|--|---|
| POORS..... | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Morra, and Iron Bark. | The same as in the preceding Class, and admit—Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Angelly and Venatica. | The same as in the preceding Class, and admit—Other Continental White Oak, Spanish Chesnut, Blue Gum. | The same as in the preceding Class, and admit—North American White Oak, American Sweet Chesnut, Stringy Bark, Red Cedar. | The same as in the preceding Class, and admit—Pitch Pine, Tamarac, Second-growth Oak, Teak. |
| FUTTOCKS..... | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Morra, and Iron Bark. | The same as in the preceding Class, and admit—Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Angelly and Venatica. | The same as in the preceding Class, and admit—Other Continental White Oak, Spanish Chesnut, Blue Gum. | The same as in the preceding Class, and admit—North American White Oak, American Sweet Chesnut, Stringy Bark, Red Cedar. | The same as in the preceding Class, and admit—Pitch Pine, Tamarac, English Oak, East India Teak. |
| and FUTTOCKS..... | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Morra, and Iron Bark. | The same as in the preceding Class, and admit—Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Angelly and Venatica. | The same as in the preceding Class. | The same as in the preceding Class. | The same as in the preceding Class, and admit—Other Continental White Oak, Spanish Chesnut, Blue Gum, Pitch Pine, Tamarac. |
| and FUTTOCKS..... and TOP TIMBERS..... | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Morra, and Iron Bark. | The same as in the preceding Class, and admit—Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Angelly and Venatica. | The same as in the preceding Class. | The same as in the preceding Class. | The same as in the preceding Class, and admit—Other Continental White Oak, Spanish Chesnut, Blue Gum, Pitch Pine, Tamarac, N. American Sweet Chesnut, Red Cedar, Hickmatack, Cordie, Amer. Birch. |
| MAIN and RIDER KEELSONS..... | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Morra, and Iron Bark. | The same as in the preceding Class, and admit—Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Angelly and Venatica. | The same as in the preceding Class, and admit—Other Continental White Oak, Spanish Chesnut, Blue Gum. | The same as in the preceding Class, and admit—North American White Oak, American Sweet Chesnut, Stringy Bark, Red Cedar, Baltic and Pitch Pine, Larch, Hickmatack, Tamarac, Juniper. | The same as in the preceding Class, and admit—Huan Pine, Sweet Oak, and other. |
| STEM and STERN POSTS..... | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Morra, and Iron Bark. | The same as in the preceding Class, and admit—Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Angelly and Venatica. | The same as in the preceding Class. | The same as in the preceding Class. | The same as in the preceding Class, and admit—Other Continental White Oak, Spanish Chesnut, Blue Gum, Pitch Pine, Tamarac, Juniper. |
| TRANSOMS, KNIGHT HEADS, SAWSE, TIMBERS, APRON, and DEADWOOD*..... | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Morra, and Iron Bark. | The same as in the preceding Class, and admit—Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Angelly and Venatica. | The same as in the preceding Class. | The same as in the preceding Class. | The same as in the preceding Class, and admit—Other Continental White Oak, Spanish Chesnut, Blue Gum, Pitch Pine, Tamarac, Juniper. |
| BEAMS..... and HOOKS..... | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Morra, Iron Bark, Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Angelly, and Venatica. | The same as in the preceding Class. | The same as in the preceding Class. | The same as in the preceding Class, and admit—Other Continental White Oak, Spanish Chesnut, Blue Gum, Pitch Pine, Larch, Hickmatack, Tamarac, Juniper. | The same as in the preceding Class, and admit—North American White Oak, American Sweet Chesnut, Red Cedar, Pitch Pine, Red Pine. |
| KNEES..... | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Morra, Iron Bark, Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Angelly, and Venatica. | The same as in the preceding Class. | The same as in the preceding Class. | The same as in the preceding Class, and admit—Other Continental White Oak, Spanish Chesnut, Blue Gum, Pitch Pine, Larch, Hickmatack, Tamarac, Juniper. | The same as in the preceding Class, and admit—North American White Oak, American Sweet Chesnut, Pitch Pine, Cedar. |

This table applies to the Deadwood so far as regards the material to be used from the height of two feet above the rabbet to the keel. If the First Futtocks run up above the Light Water-mark, the use of Foreign White Oak is allowed for the SEVEN YEARS' service only. REM.—The word "English" includes Timber the growth of the United Kingdom.

* Black Birch, Beech, Ash, &c. are not to be used in the keel in Ships of the six years' service. \$5 in cases where some being allowed a higher grade.

to be used in the Timbering of Ships, as the same will be applicable to the several Terms of Years remain on the Character A.

| ONE YEAR. | SEVEN YEARS. | SIX YEARS. | FIVE YEARS. | FOUR YEARS. |
|--|---|--|---|--|
| as in the preceding admit— merican White Oak, Sweet Chestnut, Hickory. | The same as in the preceding Class, and admit— Pitch Pine, Larch, Hackmatack, Tamarac, Juniper, English Ash, Second-hand English Oak, Afri- can Oak, and §§ East India Teak. | The same as in the preceding Class, and admit— Cowdrie, Huon Pine, American Rock Elm, Hickory. | The same as in the preceding Class, and admit— Baltic and Amer. Red Pine, Foreign Ash, Europ. and Am. Grey Elm, **B. Birch, B. Walnut, Spruce Fir, White Cedar, *Beech. | The same as in the preceding Class, and admit— Hemlock. |
| as in the preceding merican White Oak, Sweet Chestnut, Hickory. | The same as in the preceding Class, and admit— Pitch Pine, Larch, Hackmatack, Tamarac, Juniper, Second-hand English Oak, African Oak, and §§ East India Teak. | The same as in the preceding Class, and admit— Cowdrie, Huon Pine, English Ash, American Rock Elm, Hickory. | The same as in the preceding Class, and admit— Baltic and Amer. Red Pine, Foreign Ash, Europ. and Am. Grey Elm, **B. Birch, B. Walnut, **Spruce Fir, and White Cedar. | The same as in the preceding Class, and admit— Beech, Hemlock. |
| as in the preceding merican White Oak, Sweet Chestnut, Hickory. | The same as in the preceding Class, and admit— Other Cont. White Oak, Spanish Chestnut, Blue Gum, N. Amer. White Oak, Amer. Sweet Ches- nut, Stringy Bark, Red Cedar, Pitch Pine, Larch Hackmatack, Tamarac, and Juniper. | The same as in the preceding Class, and admit— Second-hand English Oak, African Oak, §§ East India Teak, Huon Pine. | The same as in the preceding Class, and admit— Baltic and Amer. Red Pine, English Ash, American Rock Elm, Hickory. | The same as in the preceding Class, and admit— Foreign Ash, European and Amer. Grey Elm, Black Birch, and Black Walnut, Spruce Fir, White Cedar, and Hemlock. |
| as in the preceding merican White Oak, Sweet Chestnut, Hickory. | The same as in the preceding Class, and admit— Other Continental White Oak, Spanish Chestnut, Blue Gum, N. Amer. White Oak, Amer. Sweet Chestnut, Stringy Bark, Red Cedar, Pitch Pine, Larch, Hackmatack, Tamarac, Juniper, Cowdrie, Huon Pine, Baltic and Amer. Red Pine. | The same as in the preceding Class, and admit— Second-hand English Oak, African Oak, §§ East India Teak. | The same as in the preceding Class, and admit— English Ash, American Rock Elm, Hickory. | The same as in the preceding Class, and admit— Foreign Ash, European and Amer. Grey Elm, Black Birch, Bk Walnut, Spruce Fir, †† White Cedar, Yellow Pine, and Hemlock. |
| as in the preceding merican White Oak, Sweet Chestnut, Hickory. | The same as in the preceding Class, and admit— Cowdrie, Huon Pine, Baltic and Amer. Red Pine. | The same as in the preceding Class, and admit— American Rock Elm, Hickory, Second-hand English Oak, African Oak, §§ East India Teak. | The same as in the preceding Class, and admit— English Ash, Foreign Ash. | The same as in the preceding Class, and admit— European and Amer. Grey Elm, Black Birch, Black Walnut, Spruce Fir, White Cedar, Beech, and Yellow Pine. |
| as in the preceding merican White Oak, Sweet Chestnut, Hickory. | The same as in the preceding Class, and admit— Other Cont. White Oak, Span. Chestnut, Blue Gum, N. Amer. White Oak, Amer. Sweet Ches- nut, Stringy Bark, Red Cedar, Pitch Pine, Larch, Hackma- tack, Tamarac, and Juniper. | The same as in the preceding Class, and admit— Huon Pine, Second-hand English Oak, African Oak, §§ East India Teak. | The same as in the preceding Class, and admit— Baltic and Amer. Red Pine, American Rock Elm, Hickory. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, European and Amer. Grey Elm, Black Birch, White Cedar, and Yellow Pine. |
| as in the preceding merican White Oak, Sweet Chestnut, Hickory. | The same as in the preceding Class, and admit— Other Cont. White Oak, Span. Chestnut, Blue Gum, N. Amer. White Oak, Amer. Sweet Ches- nut, Stringy Bark, Red Cedar, Pitch Pine, Larch, Hackma- tack, Tamarac, and Juniper. | The same as in the preceding Class, and admit— Huon Pine, Second-hand English Oak, African Oak, §§ East India Teak. | The same as in the preceding Class, and admit— Baltic and Amer. Red Pine, American Rock Elm, Hickory. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, European and Amer. Grey Elm, Black Birch, Black Walnut, Spruce Fir, White Cedar, and Yellow Pine. |
| as in the preceding merican White Oak, Sweet Chestnut, Hickory. | The same as in the preceding Class, and admit— North Amer. White Oak, Amer. Sweet Chestnut, Stringy Bark, Red Cedar, Cowdrie, Huon Pine, and Baltic and Amer. Red Pine. | The same as in the preceding Class, and admit— Second-hand English Oak, African Oak, §§ East India Teak. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, American Rock Elm, Hickory, European and American Grey Elm. | The same as in the preceding Class, and admit— Black Birch, Black Walnut, Spruce Fir, White Cedar, Yellow Pine, Hemlock. |
| as in the preceding merican White Oak, Sweet Chestnut, Hickory. | The same as in the preceding Class, and admit— North Amer. White Oak, Amer. Sweet Chestnut, Stringy Bark, Red Cedar, Cowdrie, Huon Pine, Baltic and Amer. Red Cedar. | The same as in the preceding Class, and admit— Second-hand English Oak, African Oak, §§ East India Teak. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, American Rock Elm, Hickory, European and American Grey Elm. | The same as in the preceding Class, and admit— Black Birch, Black Walnut, Yellow Pine, Hemlock. |

* Black Birch, Beech, American Rock Elm, and Cowdrie allowed for Floors in midships, to an extent not exceeding one-half the entire length of the keel in ships of the SEVEN YEARS' grade.
 † White Cedar allowed for Third Futlocks and Top-timbers in Ships of the SEVEN YEARS' grade.
 †† In cases where second-hand Teak of approved quality is proposed to be used, application may be made to the Committee with a view to its being allowed a higher grade (not exceeding two years) than as set forth above.

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No. 2.—A TABLE of Ships, as the same will be applicable to the several Terms of Years

| PARTS OF THE OUTSIDE OF A VESSEL. | SIX YEARS. | FIVE YEARS. | FOUR YEARS. |
|---|---|--|---|
| KEEL to the 1st FUTTOCK HEADS..... | same as in the preceding Class, and admit— Spruce Fir, White Cedar, Yellow Pine. | The same as in the preceding Class. | The same as in the preceding Class, and admit— Hemlock. |
| 1st FUTTOCK HEADS..... to LIGHT WATER-MARK | same as in the preceding Class, and admit— Spruce Fir, White Cedar. | The same as in the preceding Class, and admit— Yellow Pine. | The same as in the preceding Class, and admit— Hemlock. |
| LIGHT WATER-MARK to WALES | same as in the preceding Class, and admit— American Rock Elm, Hickory. | The same as in the preceding Class, and admit— European and American Grey Elm, Spruce Fir, White Cedar, Yellow Pine. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, Black Birch, Black Walnut, Beech, Hemlock. |
| WALES..... BLACK STRAKES TOPSIDES..... and SHEER STRAKES | same as in the preceding Class | The same as in the preceding Class, and admit— American Rock Elm, Hickory, Yellow Pine. | The same as in the preceding Class, and admit— European and American Grey Elm, Black Birch, Black Walnut, Spruce Fir, White Cedar and Hemlock. |
| UPPER-DECK WATERWAYS SPRIKETING and PLANKSHEERS... | same as in the preceding Class. | The same as in the preceding Class, and admit— American Rock Elm, Hickory, ++ Yellow Pine, Second-hand English Oak, African Oak, §§ East India Teak, | The same as in the preceding Class, and admit— European and American Grey Elm, Black Birch, Black Walnut, Spruce Fir, White Cedar, Hemlock. |
| SHELVES, CLAMPS, LIMBER and BILGE STRAKES, CELLING in HOLD and BETWEEN DECKS, also SPRIKETING and WATERWAY BELOW the UPPER DECK | same as in the preceding Class, and admit— American Rock Elm, Hickory. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, Black Birch, Black Walnut, Spruce Fir, White Cedar, Beech, Yellow Pine, Second-hand English Oak, African Oak and § East India Teak. | The same as in the preceding Class, and admit— European and American Grey Elm, Hemlock. |
| RUDDER and WINDLASS MAIN PIECES. | same as in the preceding Class. | The same as in the preceding Class, and admit— Baltic and American Red Pine, English Ash, Second-hand English Oak, African Oak, §§ East India Teak. | The same as in the preceding Class, and admit— American Rock Elm, Hickory, Black Birch, Black Walnut, Spruce Fir, White Cedar, Beech. |

§ The use of Elm and of Upper Deck in Ships of the SEVEN YEARS' grade, if properly fastened, as prescribed main Keel, of one of the best quality, independently of the waterways.

the Upper Deck Beams, approved quality is proposed to be used, application may be made to the Committee

† American Rock Elm grade (not exceeding two years' than as set forth above.

grade.

‡ The materials mark

Co. 2.—A TABLE, exhibiting the different Descriptions of TIMBER, of good Quality, to be used in the Outside of Ships appointed for Ships to remain on the

| PARTS OF THE OUTSIDE OF A VESSEL. | TWELVE YEARS. | TEN YEARS. | NINE YEARS. | EIGHT YEARS. |
|---|---|---|---|--|
| KEEL to the 1st FUTTOCK HEADS..... | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Mora, Iron Bark, Mahogany of hard texture, Cuba Sabicu, Pencell Cedar, Angelly, Venetia, other Cont. White Oak, Spanish Chesnut, Blue Gum, North American White Oak, American Sweet Chesnut, Stringy Bark, Red Cedar, Pitch Pine, Larch, Hackmatack, Tamarac, Juniper, Hickory, & American Rock Elm, European and American Grey Elm, and Sileech. | The same as in the preceding Class, and admit— Cowdle, Huon Pine, English Ash, Foreign Ash, Black Birch, Black Walnut. | The same as in the preceding Class, and admit— Baltic and Amer. Red Pine. | The same as in the preceding Class. |
| 1st FUTTOCK HEADS..... to LIGHT WATER-MARK | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Mora, Iron Bark, Mahogany of hard texture, Cuba Sabicu, Pencell Cedar, Angelly, Venetia, other Cont. White Oak, Spanish Chesnut, and Blue Gum. | The same as in the preceding Class, and admit— North American White Oak, American Sweet Chesnut, Stringy Bark, Red Cedar, Pitch Pine, Larch, Hackmatack, Tamarac, Juniper. | The same as in the preceding Class, and admit— Cowdle, Huon Pine, Baltic and Amer. Red Pine. | The same as in the preceding Class, and admit— American Rock Elm, Hickory, European and American Grey Elm, Beech. |
| LIGHT WATER-MARK to WALES | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Mora, and Iron Bark. | The same as in the preceding Class, and admit— Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Angelly and Venetia. | The same as in the preceding Class, and admit— Other Continental White Oak, Spanish Chesnut, Blue Gum. | The same as in the preceding Class, and admit— North American White Oak, American Sweet Chesnut, Stringy Bark, Red Cedar, Pitch Pine, Larch, Hackmatack, Tamarac, Juniper, Cowdle, Huon Pine, Baltic and American Red Pine. |
| WALES BLACK STRAKES TOPSIDES and SHEER STRAKES | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Mora, and Iron Bark. | The same as in the preceding Class, and admit— Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Angelly and Venetia. | The same as in the preceding Class. | The same as in the preceding Class, and admit— Other Continental White Oak, Spanish Chesnut, Blue Gum, Pitch Pine, Larch, Hackmatack, Tamarac, and Juniper. |
| UPPER-DECK WATERWAYS SPIRKETTING and PLANKSHEERS... | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Mora, and Iron Bark. | The same as in the preceding Class, and admit— Mahogany of hard texture, Cuba Sabicu, Pencell Cedar, Angelly, Venetia, Pitch Pine, Larch, Hackmatack, Tamarac, Juniper, Cowdle, Huon Pine, and Baltic and American Red Pine. | The same as in the preceding Class, and admit— Other Continental White Oak, Spanish Chesnut, Blue Gum. | The same as in the preceding Class. |
| SHELVES, CLAMPS, LIMBER and BILGE STRAKES, CEILING in HOLD and RETWIX DECKS, also SPIRKETTING and WATERWAY BELOW the UPPER DECK | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Mora, Iron Bark, Mahogany of hard texture, Cuba Sabicu, Pencell Cedar, Angelly and Venetia. | The same as in the preceding Class, and admit— Other Continental White Oak, Spanish Chesnut, Blue Gum. | The same as in the preceding Class, and admit— North American White Oak, American Sweet Chesnut, Stringy Bark, Red Cedar. | The same as in the preceding Class, and admit— Pitch Pine, Larch, Hackmatack, Tamarac, Juniper, Cowdle, Huon Pine, Baltic and Amer. Red Pine. |
| RUDDER and WINDLASS MAIN PIECES. | English, African, and Live Oak, Adriatic, Italian, Spanish, Portuguese, and French Oak, East India Teak, Morung Saul, Greenheart, Mora, and Iron Bark. | The same as in the preceding Class, and admit— Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Angelly and Venetia. | The same as in the preceding Class. | The same as in the preceding Class. |

INSIDE PLANKS

* The use of Elm and Beech in Ships above the EIGHT YEARS' grade, to be restricted to a height from the lower part of the keel of one-half of the internal depth of the Ship measured, in midships, from the top of the Limber Strake to the top of the Upper Deck Beams.
† American Rock Elm allowed for Limber Strakes, Bilge Strakes, and Ceiling between them in Ships of the SEVEN YEARS' grade.
‡ The materials marked thus " under the heads of " Rudders and Windlass " allowed in Ships of 300 Tons and under only.

† Yellow in Table B.
‡ In case with a view MEM.—

used in the Outside and Inside Planking of Ships, as the same will be applicable to the several Terms of Years ships to remain on the Character A.

| EIGHT YEARS. | SEVEN YEARS. | SIX YEARS. | FIVE YEARS. | FOUR YEARS. |
|---|--|---|--|---|
| The same as in the preceding Class. | The same as in the preceding Class. | The same as in the preceding Class, and admit— Spruce Fir, White Cedar, Yellow Pine. | The same as in the preceding Class. | The same as in the preceding Class, and admit— Hemlock. |
| The same as in the preceding Class, and admit— European Rock Elm, Kory, European and American Grey Elm, Beech. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, Black Birch, Black Walnut. | The same as in the preceding Class, and admit— Spruce Fir, White Cedar. | The same as in the preceding Class, and admit— Yellow Pine. | The same as in the preceding Class, and admit— Hemlock. |
| The same as in the preceding Class, and admit— North American White Oak, American Sweet Chestnut, Stringy Bark, Red Cedar, Pitch Pine, Larch, Black-nut, Tamarac, Juniper, Cordie, Huon Pine, Baltic and American Red Pine. | The same as in the preceding Class. | The same as in the preceding Class, and admit— American Rock Elm, Hickory. | The same as in the preceding Class, and admit— European and American Grey Elm, Spruce Fir, White Cedar, Yellow Pine. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, Black Birch, Black Walnut, Beech, Hemlock. |
| The same as in the preceding Class, and admit— European Continental White Oak, Black Chestnut, Blue Gum, Pitch Pine, Larch, Black-nut, Tamarac, and Juniper. | The same as in the preceding Class, and admit— North American White Oak, American Sweet Chestnut, Stringy Bark, Red Cedar, Cordie, Huon Pine, and Baltic and Amer. Red Pine. | The same as in the preceding Class. | The same as in the preceding Class, and admit— American Rock Elm, Hickory, Yellow Pine. | The same as in the preceding Class, and admit— European and American Grey Elm, Black Birch, Spruce Fir, White Cedar and Hemlock. |
| The same as in the preceding Class. | The same as in the preceding Class, and admit— North American White Oak, American Sweet Chestnut, Stringy Bark, Red Cedar. | The same as in the preceding Class. | The same as in the preceding Class, and admit— American Rock Elm, Hickory, Yellow Pine, Second-hand English Oak, African Oak, East India Teak. | The same as in the preceding Class, and admit— European and American Grey Elm, Black Birch, Black Walnut, Spruce Fir, White Cedar, Hemlock. |

DE PLANKING.

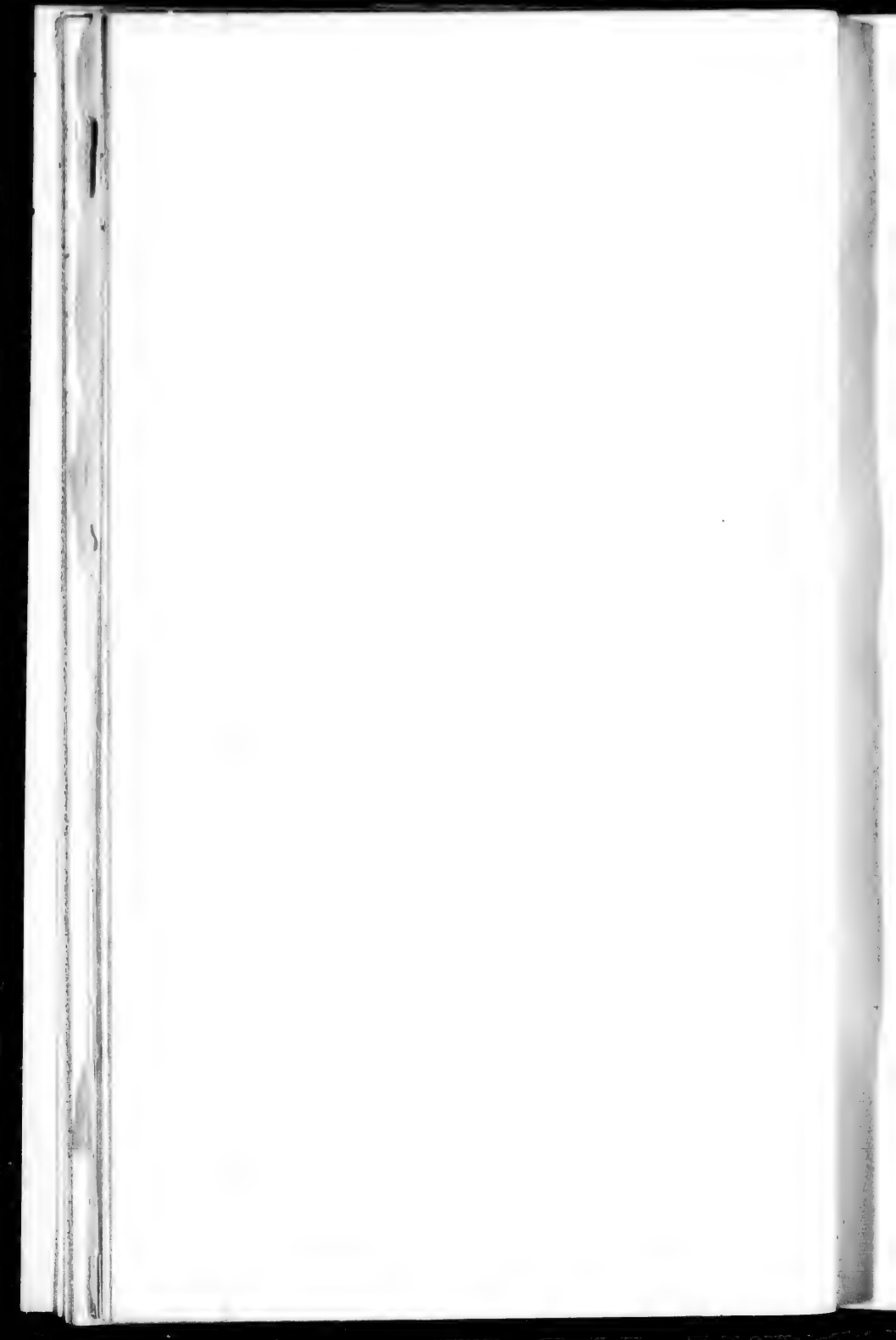
| | | | | |
|---|--|--|--|--|
| The same as in the preceding Class, and admit— Pitch Pine, Black-nut, Tamarac, Juniper, Cordie, Huon Pine, Baltic and American Red Pine. | The same as in the preceding Class. | The same as in the preceding Class, and admit— American Rock Elm, Hickory. | The same as in the preceding Class, and admit— English Ash, Foreign Ash, Black Birch, Black Walnut, Spruce Fir, White Cedar, Beech, Yellow Pine, Second-hand English Oak, African Oak and East India Teak. | The same as in the preceding Class, and admit— European and American Grey Elm, Hemlock. |
| The same as in the preceding Class. | The same as in the preceding Class, and admit— Other Continental White Oak, Spanish Chestnut, Blue Gum, North American White Oak, American Sweet Chestnut, Stringy Bark, Red Cedar, Pitch Pine, Larch, Black-nut, Tamarac, and Juniper. | The same as in the preceding Class. | The same as in the preceding Class, and admit— Baltic and American Red Pine, English Ash, Second-hand English Oak, African Oak, East India Teak. | The same as in the preceding Class, and admit— American Rock Elm, Hickory, Black Birch, Black Walnut, Spruce Fir, White Cedar, Beech. |

rt of the
top of
YEARS'

†† Yellow Pine allowed for Waterways of Upper Deck in Ships of the SEVEN YEARS' grade, if properly fastened, as prescribed in Table B, and provided the Beams are well secured, independently of the waterways.

§§ In cases where second-hand Teak of approved quality is proposed to be used, application may be made to the Committee with a view to its being allowed a higher grade (not exceeding two years) than as set forth above.

MEM.—The word "English" includes Timber the growth of the United Kingdom.



LLOYD'S REGISTER

OF

BRITISH AND FOREIGN SHIPPING.

RULES AND REGULATIONS.

1. The operations of the Societies of the two Register Books of Shipping formerly printed for the use of Merchants, Ship-Owners, and Underwriters, having ceased in the year 1834, this Society was then established for the purpose of obtaining a faithful and accurate Classification of the Mercantile Shipping of the United Kingdom, and of the Foreign Vessels trading thereto, and for the government of which the following Rules and Regulations have been from time to time adopted.

2. A Register Book to be printed annually for the use of Subscribers.

3. Each person subscribing the sum of Three Guineas per annum (or such other sum as the General Committee may fix) to be considered a Member of the Society, and entitled *for his own use* to one copy of the Register Book.

4. The subscription of Public Companies, or Public Establishments (not being engaged in Marine Insurance), to be Ten Guineas per annum.

5. The subscription of Marine Insurance Companies to be regulated by the Committee on special application, in each case, but not to be less than Ten Guineas per annum.

6. The Register Book to be periodically posted throughout the year.

7. For the convenience of subscribers not resident in London, a Supplement, containing the additions to, and corrections made in, the Register Book, to be printed, fortnightly, in such convenient form, as to admit of its transmission by Post, so that such parties may be furnished, from time to time, with the latest and most complete information.

8. The superintendence of the affairs of the Society to be under the direction of a Committee of Merchants, Ship-Owners, and Underwriters: twenty-four elected in London and eleven at the

principal outports. The Chairman for managing the affairs of Lloyd's, and the Chairman of the General Ship-Owners' Society, London; also the Chairman and Deputy Chairman of the Liverpool Committee, and the Chairman of the Liverpool Classification Committees for the time being, to be, *ex officio*, Members of the Committee.

9. Six of the Members elected in London, namely, two of each of the constituent parts of the Committee, to go out annually by rotation, but to be eligible to be re-elected. The vacancies so arising to be filled up by the election of two Underwriters and one Merchant by the Committee for managing the affairs of Lloyd's, and two Ship-Owners and one Merchant by the Committee of the General Ship-Owners' Society.

10. The Members elected at the Outports to retire at the end of every *four* years, but to be eligible for re-election.

11. The Committee to appoint from their own body, annually, a Chairman and Deputy Chairman, and also a Chairman for a Sub-Committee of Classification.

12. The Committee to appoint a Sub-Committee of Classification, to be so regulated that each Member of the General Committee may, in rotation, take his turn of duty therein throughout the year.

13. The Secretary, Clerks, and Servants of the Society, and the Surveyors, to be appointed by and be under the direction of the General Committee.

14. Special meetings to be convened by order of the Chairman, or Deputy Chairman, or on the requisition of any three Members.

15. All elections and appointments to be made by ballot.

16. No Member of the Committee to be permitted to be present on the decision of the classification of any ship of which he is the owner, or wherein he is directly or indirectly interested.

17. The Committee to be empowered to make such Bye-laws for their own government and proceedings as they may deem requisite, not being inconsistent with the original Rules and Regulations under which the Society was established; but no new Rule or Bye-law to be introduced, nor any Rule or Bye-law altered, without special notice being given for that purpose at the Meeting of the Committee next preceding that at which such Motion is intended to be made; such notice to be inserted in the summons convening the meeting. No new Rule, or alteration in any existing Rule,

materially affecting the classification of ships, to take effect until the expiration of six months from the time it shall have been determined upon.

18. All Reports of survey to be made in writing by the Surveyors according to the forms prescribed, and submitted for the consideration of the General Committee, or of the Sub-Committees of Classification ; but the character assigned by the latter to be subject to confirmation by the General Committee.

19. The reports of the Surveyors, and all documents and proceedings relating to the classification of ships, to be carefully preserved, and parties proving themselves to be interested therein, to have access to the same under the direction of the Chairman or Deputy Chairman.

Copies of the original reports (if the ships be already classed, but not otherwise), so far as relates to the dimensions, scantlings, fastenings, and materials, in cases where the correctness of the reports in these particulars is certified by the builders, are granted on application.

20. Foreign ships, and ships built in the British possessions abroad where there is not a Surveyor (*See also Section 51*), to be surveyed on their arrival at a port in the United Kingdom ; but a due regard is to be had to the circumstance of their having been exempted from the supervision while building to which all British ships are subjected, and the character to be assigned to them is to be regulated according to their intrinsic quality, and from the best information the Committee can obtain.

21. In every case in which the character assigned to a ship may be proposed, on survey, to be reduced, notice is to be given in writing to the Owner, Master, or Agent, with an intimation that if the reduction be objected to, the Committee will be ready to direct a special survey, on the Owner, Master, or Agent, agreeing to pay the expenses attending the same, provided on the said survey there shall appear sufficient ground for the proposed reduction.

22. When the Surveyors consider repairs to be requisite, they are respectfully to communicate the same in writing to the Owner, Master, or Agent, and if such repairs be not entered upon within a reasonable time, a corresponding report is to be made to the Committee for their decision thereon.

23. Parties considering the repairs suggested by the Surveyor to be unnecessary or unreasonable, may appeal to the Committee.

who will direct a special survey to be held ; but should the opinion of the Surveyor be confirmed by the Committee, then the expense of such special survey is to be paid by the party appealing.

24. The Surveyors to the Society not to be permitted (without the special sanction of the Committee), to receive any fee, gratuity, or reward whatsoever for their own use or benefit, for any service performed by them in their capacity of Surveyors to this Society, on pain of immediate dismissal.

25. The Surveyors will be directed to attend on Special Surveys of ships while building or under damage, or repair, when required by Merchants, Ship-Owners, or Underwriters ; the charge for which is to be regulated according to the nature and extent of the service performed. In all cases, the application for the assistance of the Surveyors must be made in writing addressed to the Secretary.

FUNDS.

26. The Funds to be under the authority and control of the Committee, and a statement of the Receipts and Expenditure to be annually printed for the information of the subscribers.

27. The following Fees to be charged to the Owners of ships prior to their vessels being classed and registered in the book :

I.

For Entering and Classing Ships, and for Entering and Classing Ships surveyed for Continuation, or the Character A in red, or repaired for Restoration.

| | | | | |
|--------------------|-----------------------------------|----|---|---|
| For each Ship..... | under 100 Tons..... | £1 | 0 | 0 |
| Ditto | of 100 Tons and under 200 “ | 2 | 0 | 0 |
| Ditto | 200 “ 300 “ | 3 | 0 | 0 |
| Ditto | 300 “ 400 “ | 4 | 0 | 0 |
| Ditto | 400 and upwards..... | 5 | 0 | 0 |

II.

For Registering Repairs.

| | | | | |
|--------------------|-----------------------------------|----|----|---|
| For each Ship..... | under 300 Tons..... | £0 | 10 | 0 |
| Ditto | of 300 Tons and under 500 “ | 1 | 0 | 0 |
| Ditto | 500 “ 1000 “ | 2 | 0 | 0 |
| Ditto | 1000 and upwards..... | 3 | 0 | 0 |

For Re-classing Ships (except when repaired) the Characters of which have been expunged, or change of Owners.

For each Ship.....under 200 Tons.....£0 10 0
Ditto of 200 " and above. 1 0 0

Special Surveys.

28. For ships built under the special superintendence of the Surveyors (to entitle them to the distinctive mark X) 1s. per ton.

For Surveys for damage, or for other Surveys, held at the request of the Owners, and for the Survey of Ships for Restoration, Continuation, or the character A in red, a charge (in addition to the Fee for entry) will be made, according to the nature and extent of the service performed. In cases where the caulking of ships is superintended and tested by the Surveyors, a special charge will be made, according to the tonnage of the ship. All repairs which may be required on the Surveys above referred to, must be performed under the superintendence of the Society's Surveyors.

29. Certificates of Character, of the Form No. 7, signed by the Chairman of the General Committee, or by the Chairman of the Sub-Committee of Classification, and countersigned by the Secretary, will be granted on application, the charge for which will be as follows:—

For ships under 200 Tons.....£0 2 6 each.

Ditto of 200 " and above..... 0 5 0 "

Copies of original reports, as per Section 19..... 1 1 0 "

30. Rules, each copy, 5s.

CHARACTERS.

31. The Characters to be assigned to ships to be, as nearly as possible, a correct indication of their real and intrinsic qualities,* and to be in all cases fixed (not by the Surveyors, but) by the Committee, after due consideration of the reports of the Surveyors and such other documents as may be submitted to them, and will be distinguished as follows:—

* Ships which are not built in accordance with the principles of the Society's Rules will be marked in the Register Book thus, "Expt. (B.S.)," denoting that they are built experimentally, and are classed subject to being surveyed biennially.

SHIPS A

To consist of new ships, or ships Continued, or Restored. *Vide Sections 34, 54, 55, 57.*

SHIPS A, in Red.

To consist of ships which have passed the period assigned on the original Survey, or Continuance, or Restoration; and also of ships not having had an original character, and which are found on survey of superior description, fit for the conveyance of dry and perishable goods, *to and from all parts of the world.* *Vide Section 60.*

SHIPS B

To consist of ships which are found on Survey fit for the safe conveyance of dry and perishable goods on *shorter voyages.* *Vide Section 61.*

SHIPS C

Will comprise ships which shall be found on survey fit for the conveyance of cargoes not in their nature subject to sea damage *on any voyage.* *Vide Section 64.*

SHIPS D

To consist of ships fit to carry cargoes not liable to sea damage *on shorter voyages.* *Vide Section 66.*

32. In all cases in which the application of the rules must necessarily be regulated by the ship's admeasurement, the *gross register tonnage* is to be adopted. (*See also Section 38.*)

RULES FOR CLASSIFICATION.

SHIPS A

33. Will consist of new ships and those which have not passed a prescribed age, provided they are kept in a state of complete repair and efficiency. The character A will not, however, be granted to any vessel, unless satisfactory evidence of the date of build and place where built is produced.

34. The number of years to be assigned for this Character to be determined with reference to the original construction and quality of the vessels, the materials employed, and the mode of building; and their continuance for the time so assigned to depend upon its being shown by occasional surveys (annually if practicable) that

their efficiency is duly maintained. The characters of ships A will be struck out of the Register, unless re-surveyed within a period of four years from the *date of last survey*,—or, in the case of ships exceeding the eight years' grade, within *one-half of the time* assigned. In all cases the windlass is to be examined by unhangings and stripping the wood linings within a period not exceeding one-half of that originally assigned. After the expiration of the periods prescribed (*See Section 59*), ships will be permitted to Continue in the character A, or may be Restored thereto, for a further limited period, on complying with the conditions hereinafter prescribed in Sections 54, 55, and 57.

35. New ships are to be surveyed while building, by the Surveyors to this Society, in the following three stages of their progress, or they will be liable to lose one year of the period to which they might otherwise be entitled. (*See Section 53.*) Ships intended to be built under *special survey*, must be placed under the Surveyor's inspection from their commencement, so that all parts of the timbers, deadwood, keel, stem, &c., may be examined.

First.—When the Frame is completed, timbers dubbed fair inside and outside ready to receive planking, and before any planking is wrought.

Second.—When the beams are put in, but before the Decks are laid, and with at least two strakes of the plank of the ceiling between the lower deck and the bilge unwrought, to admit of an examination of the inner surface of the plank of the bottom.

Third.—When completed, and before the plank be painted or payed.

All ships for which a higher character than Ten Year's A may be claimed, must be surveyed by an exclusive Officer of the Society, twice at least, while building; namely, at the first and at the second stages of their progress as above prescribed. Due notice must be given by the Builder or Owner of their being ready for this additional survey.

36. A full statement, agreeably to Form No. 4, of the dimensions, scantlings, &c., of all new ships, verified by the builder, is to be transmitted by the Surveyor, and to be kept as a record in the office of the Society.

RULES TO BE OBSERVED IN BUILDING SHIPS.

37. The whole of the Timber must be of good quality, of the descriptions shown in Table A as applicable to the several terms of years for which ships so constructed may respectively be appointed to remain on the character A, and be *properly seasoned*, and free from defects. The workmanship to be well executed, and *equally so for all grades*. Defects in workmanship or quality of timber will involve a reduction in class, to be determined by the Committee in each case. The frame to be well squared and free from sap; each set of timbers to be frame-bolted together throughout their entire lengths. The butts of the timbers to be close, and not to be less in thickness than one-third of the entire moulding at that place, and to be well chocked with a butt at each end of the chock. In all cases the chocks are to be of a description of wood equal to the best material required by the rules for the timbers which they unite. In all cases in which the heads and heels of the timbers shall be *square*, in vessels intended for the twelve years' grade, a dowel (to be in diameter from one-fourth to one-third of the moulding of the timber) must be introduced into the ends of such timbers in order to connect them together. In all ships an air course must be left all fore and aft, below each set of clamps, or between the clamps and spirketting of each tier of beams, and in the hold, at each end of the ship, between the keelson and hold beam clamp, to have in addition one or two tiers of air courses for one-fifth of the entire length of the ship. In the construction of top-gallant forecastles, and poops, the timbers must be of the same materials as are required by Table A for the top-timbers of the frames of ships according to the several terms of years appointed for such ships to remain on the character A, all the said timbers to extend to the planksheer. All the outside planking of top-gallant forecastles, and the sheer-strakes, plankshaers, and spirketting of top-gallant forecastles and poops must be of the materials required by Table A for the topsides of the said ship; and the shelf and clamps of poops and top-gallant forecastles may be of the same quality as those allowed in Table A for the shelf and clamp of the upper deck. All the beams of top-gallant forecastles, and the mast beams, breast beams, and transom beams of poops, to be of the materials required by Table

A for the beams of the said ships; the remainder of the beams and the water-way of the poops, and the remainder of the planking of poops and top-gallant forecastles may be of cedar, mahogany, Baltic or American red pine, pitch pine, larch, hackmatack, tamarac, or cowdie, and rock-elm for such remainder of beams only, and yellow pine or American white spruce in ships below the seven years' grade.

This Rule does not apply to raised quarter-decks, the materials of which are required to be of the same quality as those named in Table A for the main body of the ship.

38. The Scantlings of the timbers, keelson and keel, thickness of planking, &c., are not to be less than those shown in Table B.

In the inside and outside planking, waterways, planksheers, and flat of deck of raised quarter-decks, a reduction of *one-fifth* from the thickness required by the Table B for such parts in the range upper deck in ships with two decks, will be allowed.

The measurement of raised quarter decks is to be included in the gross tonnage for regulating all scantlings.

In the inside and outside planking, waterways, planksheers, and flat of deck of full poops* and top-gallant forecastles, a reduction of *one-fourth* from the thickness required by the Table B for such planks in the range of the upper deck in ships with two decks, will be allowed; and in the siding and moulding of the top timbers and beams of full poops and top-gallant forecastles, a reduction of *one-fifth* will be allowed. These reductions will not be allowed where the united lengths of poop and forecastle exceed three-fifths of the entire length of the upper deck.

In vessels having three decks or tiers of beams, where the space under the upper deck is to be used only for the accommodation of crew and passengers, or to enclose the engine openings of steam vessels, the gross tonnage below the middle or tonnage deck, is to regulate all scantlings below this deck, but the total gross tonnage is to regulate the scantlings of keelsons, and the size of main piece of rudder. The total depth of hold in spar decked ships must not exceed thirteen-sixteenths, nor be less than twelve-sixteenths of the ship's extreme breadth.

In the construction of spar decks, the timbers must be of the

* Parties desirous of making any alteration in the construction of *Poops*, with a view to diminishing the weight (but preserving the requisite strength), may submit their plans for the Committee's consideration and approval.

same materials as are required by Table A for the top timbers of the frames of ships according to the several terms of years appointed for such ships to remain on the character A. If *all* the said timbers extend to the planksheer, their siding and moulding may be reduced one-fourth at their heads; but if only the *alternate* timbers run up to the top height, then a reduction of one-fourth only will be allowed in their moulding at their heads, and in that case there must be a perfect covering board worked all round the ship at the middle deck; and in all cases the middle deck must be a perfect deck laid and caulked. All the outside planking, and the sheerstrakes, planksheers, and spirketting must be of the materials required by Table A for the topsides of the said ship; and the shelf and clamp may be of the same quality as those allowed in Table A for the shelf and clamp of the middle deck. All the beams before the foremast, and the mast beams, hatch beams, and transom beam, must be of the materials required by Table A for the beams of the said ships; and the remainder of the beams and the waterway of spar deck, and the remainder of the planking, may be of red cedar, mahogany, Baltic or American red pine, pitch pine, larch, hackmatack, tamarac, or cowdie; and in ships below the seven years' grade, the same may be of yellow pine, American white spruce, or white cedar.

In spar decks there may be a diminution of one-fourth from the dimensions, fastenings, and bolts prescribed in the tables for the upper deck of ships with two decks (except in the siding of the spar deck beams); but if the outside planking be of 12 years' wood, then a reduction of one-third may be made in the thickness from that prescribed in Table B for the main sheerstrakes of such vessels.

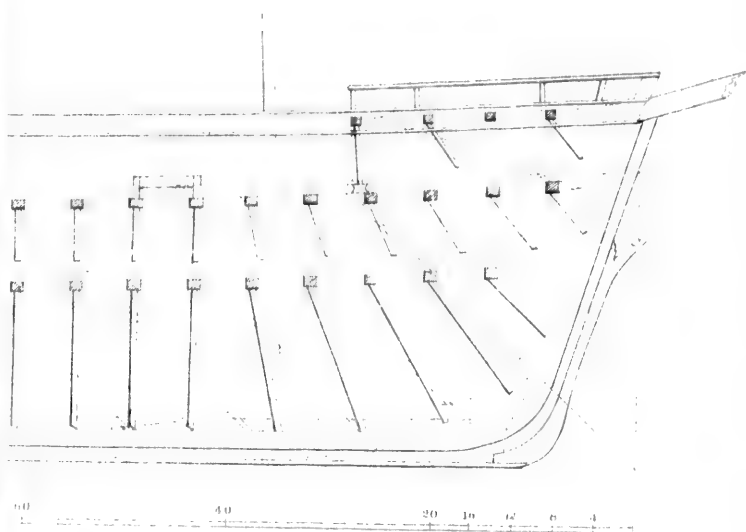
Deckhouses or other erections are allowed on spar decks, but only to the extent of one-tenth of the total superficial area of the spar deck, and are not to exceed seven feet in height. They are not to be placed nearer to either of the ends than one-fifth of the entire length of the vessel.

Vessels to which this rule applies, as regards an entire spar deck, will be noted in the Register Book thus—"Spar decked."

The *total tonnage* of the ship is to regulate the equipment, except the anchors and chains for steam vessels.—(See Sec. 81.)

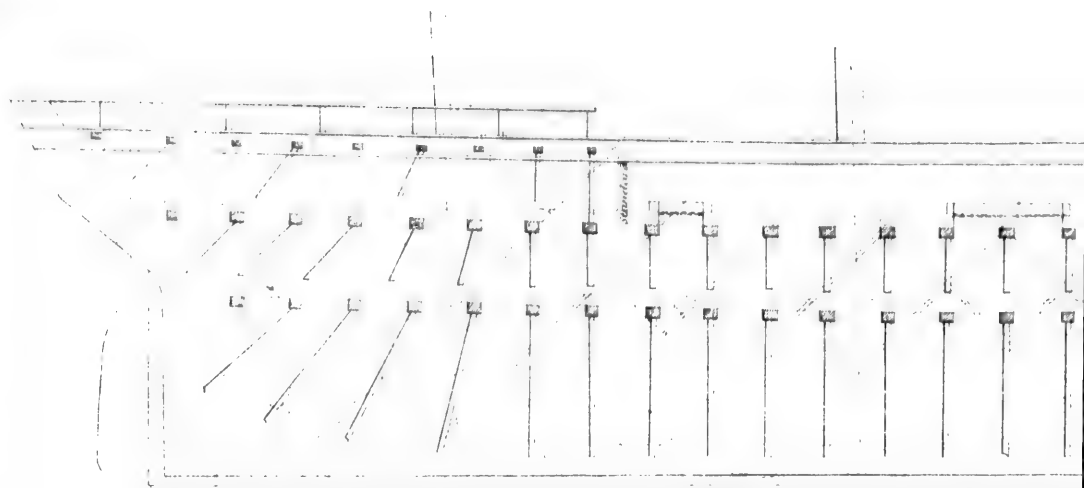
39. The intermediate dimensions for the scantling of timbers between the floor heads and the gunwale to be regulated in pro-

and Riders prescribed in the Rules.

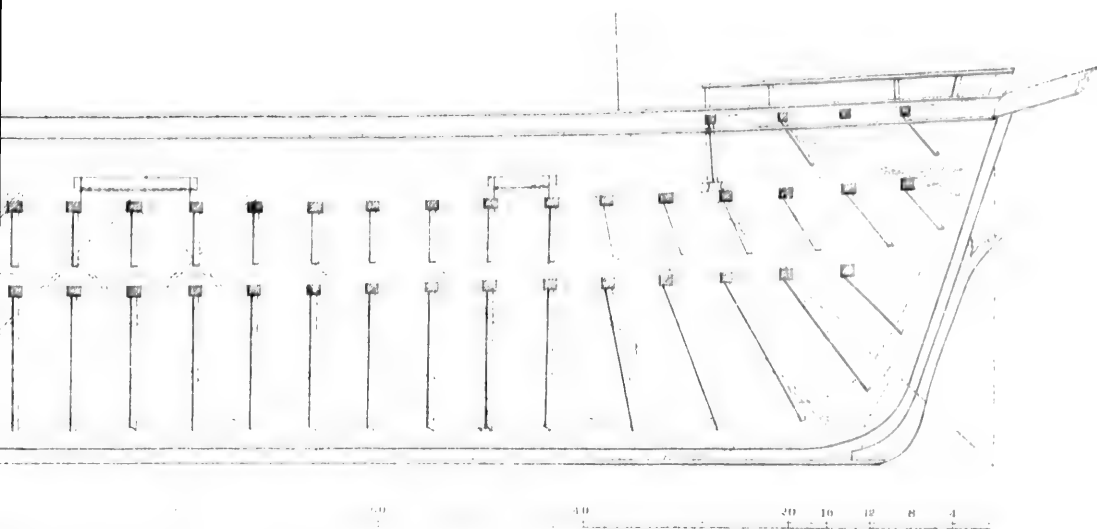


C & A CLARKE LITH

*A plan showing the direction of the Iron Plates on Frames and
Sections 30 and 6*



*on Frames and Iron Lines and Riggers prescribed in the Rules
Sections 30 and 02*



J & A CLARKE LITH

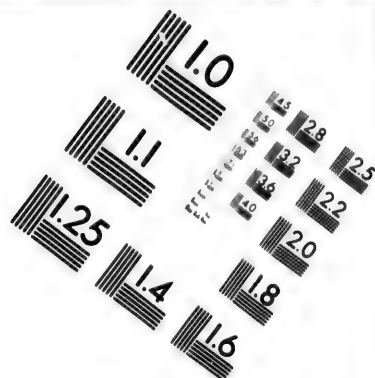
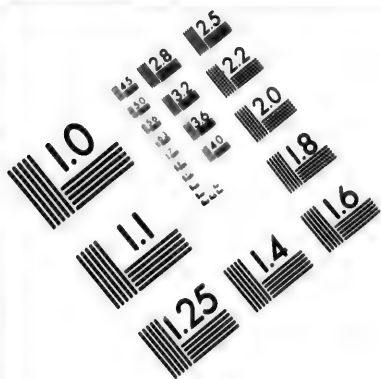
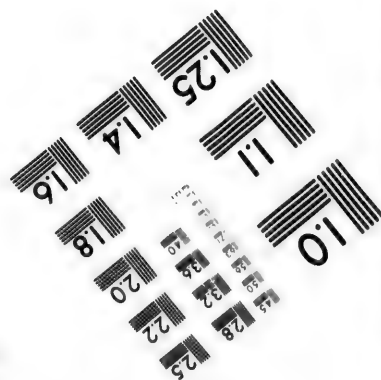
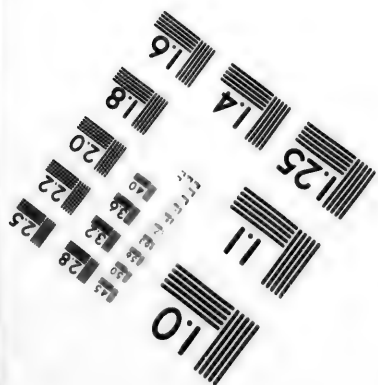
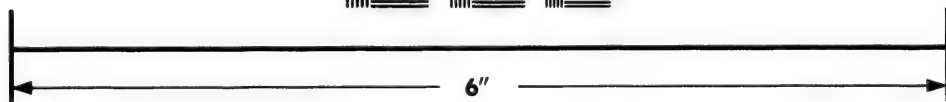
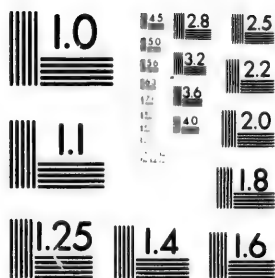


IMAGE EVALUATION TEST TARGET (MT-3)



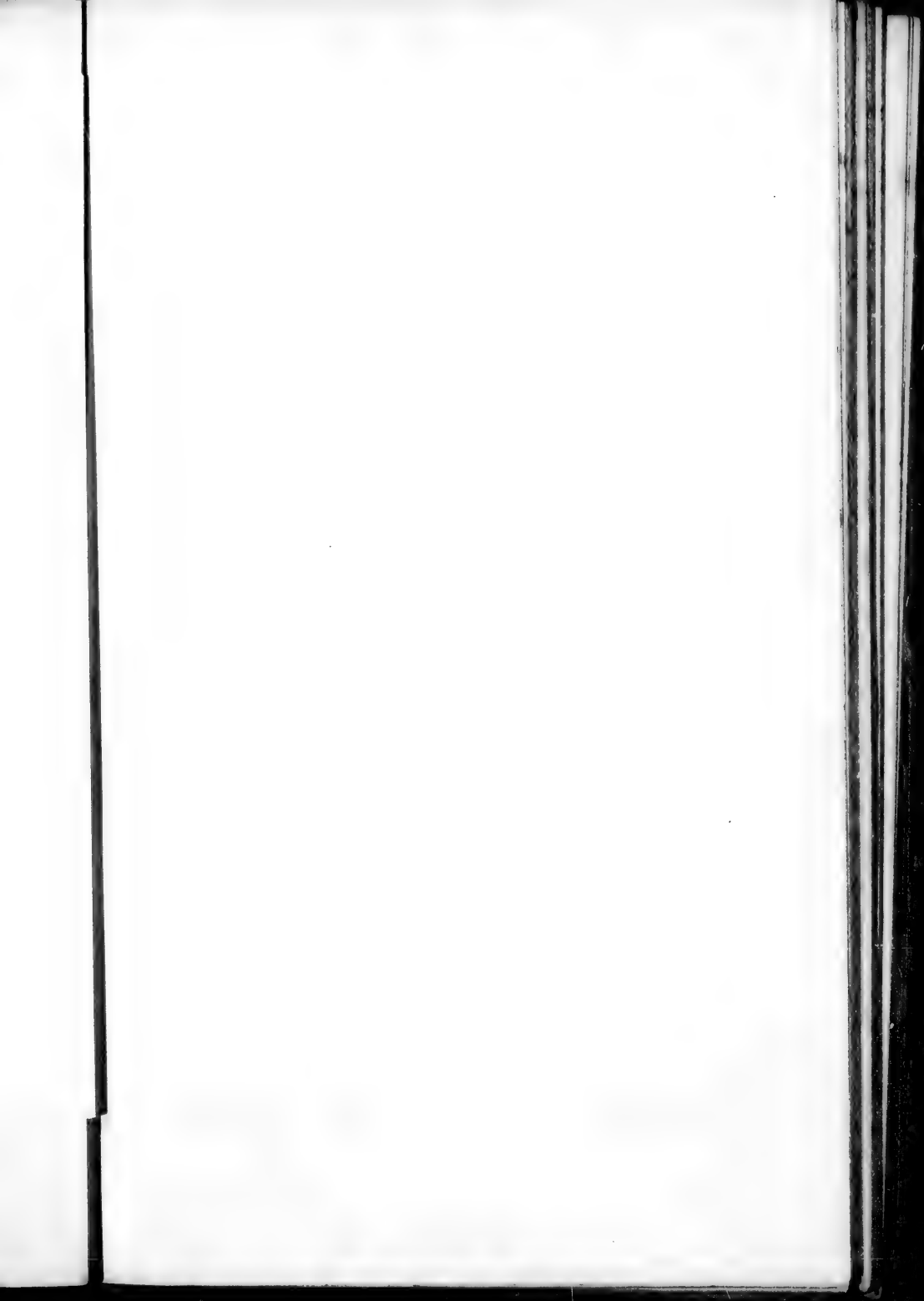
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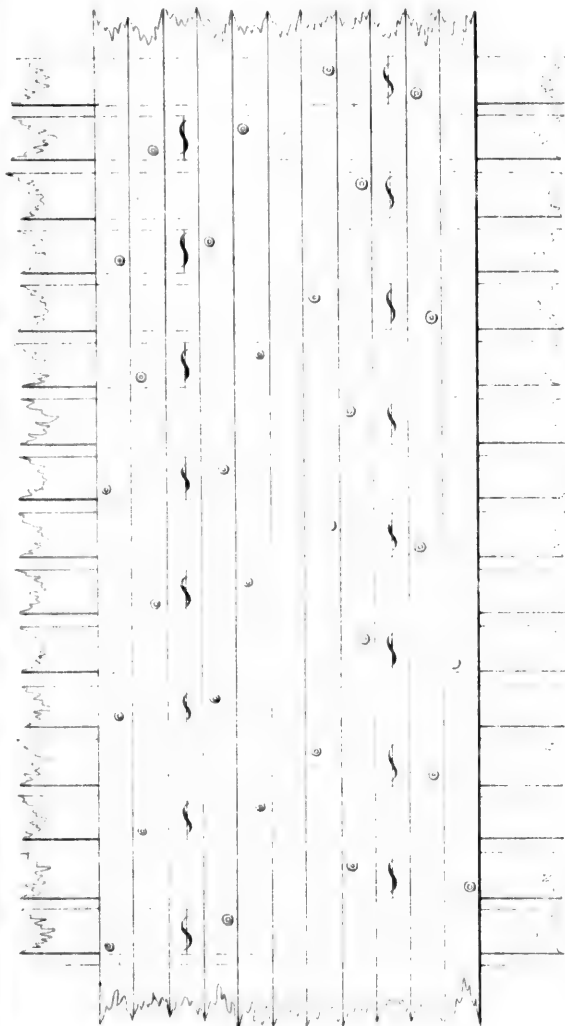
13 28 25
32 22
20

10

between the door heads and the gunwale to be regulated in pro-



SKETCH SHOWING THE ARRANGEMENT OF THROUGH BOLTS IN THICK STRAKES OVER DOUBLE FLOORS. SECTION 39



portion to the distance from the two points. Should the timber and space be increased, the siding of the timbers to be increased in proportion. Whenever ships are built with double floors, thick strakes (see Table B) must be worked inside, to extend from the lower part of the short floor-head chocks to the upper part of the long floor-head chocks, and be well bolted through and clenched, *with one bolt at the head of each long and short arm of floors, and at the heel of each first and second foothook which come upon them, from the foremast extending a distance aft equal to three-fifths of the length of the ship; in such cases, the limber strakes need not be through bolted.*

SHIPS OF GREAT LENGTH IN PROPORTION TO THEIR BREADTH OR DEPTH.

All ships, the length of which (measured from the fore part of the stem to the after part of the stern-post on the range of upper deck), shall exceed five times their extreme breadth, or eight times and under nine times their depth, shall have diagonal iron plates closely inserted either outside or inside the frame.* If placed outside, the said plates to extend from the upper side of upper tier of beams to the lower part of chocks at first futtock amidships, and to the same perpendicular height forward and aft, measured from the lower part of the keel; and if placed inside, the plates are to extend from the upper side of upper tier of beams to the lower part of chocks at *floor heads*. Whether placed outside or inside, the sizes of the plates not to be less than as follows, viz. :—

| | | | |
|---|-----------|------------|--------|
| In ships of 100 tons and under 200 tons . . . 3½ by ⅞ in. | | | |
| “ 200 | “ 400 | “ . . . 4 | by ¾ “ |
| “ 400 | “ 700 | “ . . . 4 | by ¾ “ |
| “ 700 | “ 1000 | “ . . . 4½ | by ¾ “ |
| “ 1000 | “ 1500 | “ . . . 5 | by ¾ “ |
| “ 1500 and above | 5½ by ¾ “ | | |

and to be fastened with bolts, one at each alternate timber if outside, and one at each timber if inside, not less in diameter than the sizes given for “through butt bolts” in Table D.

The number of plates to be in proportion of not less than one pair to every twelve feet of the ship's entire length taken as above,

* Parties objecting to fit the iron plates on frames as prescribed above, are at liberty to submit, through the resident Surveyor, for the Committee's consideration and approval, such compensation as will, in their opinion, render the introduction of the iron plates unnecessary.

but not to be more than eight feet asunder measured on a square ; the said plates are to be placed diagonally, at an angle of not less than 45 degrees, their lower ends pointing to the after end of the keel in the after body, and to the fore end of the keel in the fore body, four pairs crossing each other amidships.

All such ships to have shelves and waterways to each tier of beams, each equal in contents to the transverse sectional area of the beams of their respective decks at their ends ; each of the said shelves and waterways to be bolted through the outside planking at every timber, with bolts of the sizes given in Table D ; likewise the shifts of inside and outside planking not to be less than 6 feet, unless there be a strake wrought between them, and then a distance of 5 feet will be allowed.

In ships the length of which shall exceed *six* times their extreme breadth, or nine times and under ten times their depth, the number of plates must be not less than one pair to every *ten* feet of the ship's entire length taken as above, but not to be more than six feet asunder measured on a square, and to be placed diagonally as above described.* And in addition to the requirements for ships of five times their breadth in length, such ships must be fitted with a rider keelson or a pair of sister keelsons, at the option of the owner, the transverse sectional area of such rider keelson or sister keelsons each to be equal to two-thirds of that required in Table B for main keelsons. If a rider keelson be adopted, it is to be fastened with a through bolt (of the size required in Table D for keelson bolts), in every frame ; or if the owner prefers it, every intermediate bolt may be short, passing only through the main and rider keelsons.† If sister keelsons be fitted, they must be fastened with through bolts, in number not less than one in every alternate timber, and of the size required in Table D for "scarphs of keels," &c.

40. The sizes of the deck and hold Beams have been regulated so as to be determined by the length of the beams *amidships*, as shewn in Table C. The beams will be required to be of the size of the midship beam, except those at the *after end* of the ship, which may be reduced in proportion to their length. If beams of spruce

* In cases where the length of the Ship exceeds ten times the depth, the builders or owners are to submit, through the resident Surveyor, for the Committee's approval, their plans for giving the vessel the necessary strength longitudinally.

† In all cases in which a rider keelson is fitted, it must be fastened as prescribed above irrespective of the relative dimensions of the ship.

or yellow pine are used, the siding of such beams shall be one-fourth larger than is prescribed by the above Table, or be increased each way, siding and moulding, equal in area to that amount.

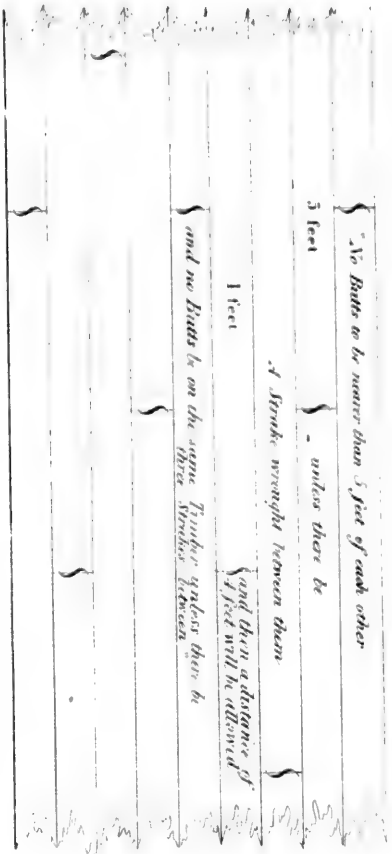
In cases where Iron beams are fitted in wood Ships the beams of each deck are to be one-eighth of an inch thicker than is required by the rules for Ships built of Iron, in consideration of the greater space between; and the depth of lower deck or hold beams is to be increased one inch. The spaces between beams of the several decks not to exceed the spaces at present allowed for wood ships, as per Rule, Section 41. Each tier of beams must have stringer plates rivetted on their ends, and tie-plates fore and aft, on each side of the hatchways, in accordance with the Rules for Iron Ships, Sec. 15, and to be of the dimensions required in Table G, or of strength equal thereto. Parties are to submit, through the resident Surveyor, their plans for attaching Iron beams to the Ship's sides, for the Committee's approval.

41. The beams of all decks to be in number and size, as hereinafter specified, and be securely fastened to the sides either with lodging-knees of iron or wood, or with a shelf-piece and waterways,* or with a shelf-piece and knees, or with some other security equal thereto; and, in addition, all ships of 150 tons and above to have vertical knees to the DECK beams; and those of 200 tons and above to have vertical knees to the HOLD beams, in number as shewn in Table E. Vessels of 18 feet, and under 15 feet hold, the spacing of the hold beams not to exceed 8 feet apart, and the deck beams 4 feet:—Vessels of 15 feet and under 18 feet hold, the spacing not to exceed 8 feet and 4 feet apart alternately, or in that proportion; the deck beams to be placed one over every hold beam, and one in all double spaces:—Vessels of 18 feet hold and above, the spacing of the beams not to exceed 4 feet 6 inches; the deck beams to be one over every hold beam. The depth in all such cases to be determined by taking the measure from the top of the limber-strake

* When the transverse sectional area of the shelf-pieces and waterways are each equal in contents to the transverse sectional area of the beams of their respective decks at their ends as given in Table C, and the beams are either dowelled or dovetailed to their shelf-pieces and waterways, and the shelves and waterways are properly shifted, scarphed, and through bolted, having also a hanging knee to the lower side of every beam end, then lodging-knees may be dispensed with, except in the mast-rooms. In Ships of 800 tons and under, where lodging-knees properly bolted are applied, the ordinary plank-clamps may be used, but the through bolting of them at alternate timbers, as per Table B, cannot be dispensed with.

(the thickness of which, for measurement, to be taken as prescribed in Table B), to the top of the upper deck beams. Ships having a depth of hold, measured from the limber-strake to the under side of the lower deck beam, above 13 feet but not exceeding 15 feet, must be secured with iron riders of the sizes, and be fastened, as shewn in Table F, and in number not less than one on every fourth floor, on each side, from the fore side of foremast to aft side of mizen-mast, to extend from the lower deck beams downwards so as to receive not less than two bolts in a substantial part of the floors ; or by orlop beams, sufficient in number and properly secured. All ships having two decks, (viz., upper and lower deck), and exceeding 24 feet in depth from the top of the limber-strake to the top of the upper deck beams, or having three decks, (viz., upper, middle, and lower deck), and exceeding 24 feet in depth from the under side of the MIDDLE DECK, to have orlop beams, the number to be in no case less than one-half the number of lower deck beams in the space between the foremast and the mizen-mast, *except in the case of flush deck ships, when a depth of 25 feet will be allowed, provided in either case the lower hold does not exceed 15 feet, measured as above from the limber-strakes to the under side of the lower deck beam.* Should a house be constructed on a flush deck ship for lodging crew or for store-room, the same not to extend aft beyond 10 feet from the mizen-mast. The application of this Rule to British North American built Ships and Fir Ships will not exempt them from the full operation of the Rule, Section 62. All dimensions, fastenings, and bolts of the middle deck in Vessels having three decks, (viz., upper, middle, and lower deck), to be the same as those prescribed in the Tables for the upper deck of ships having only two decks ; and a reduction of one-sixth from the dimensions, fastenings, and bolts, proscribed in the Tables, for the upper deck of vessels having only two (viz., upper and lower deck), will be allowed in the third or upper deck, by some called a spar deck. The middle deck to be a perfect deck laid and caulked. The united lengths of poop and forecastle not to exceed three-fifths of the entire length of the upper deck. All timbers of the frame including poop and forecastle to extend to the extreme height. Every ship exceeding 150 tons to have at least one crutch for the security of the heels of the after-timber of the frame ; one pair of pointers in addition to a knee at each end of the wing transom to connect the stern frame with the after-body of the ship ; and a transom over

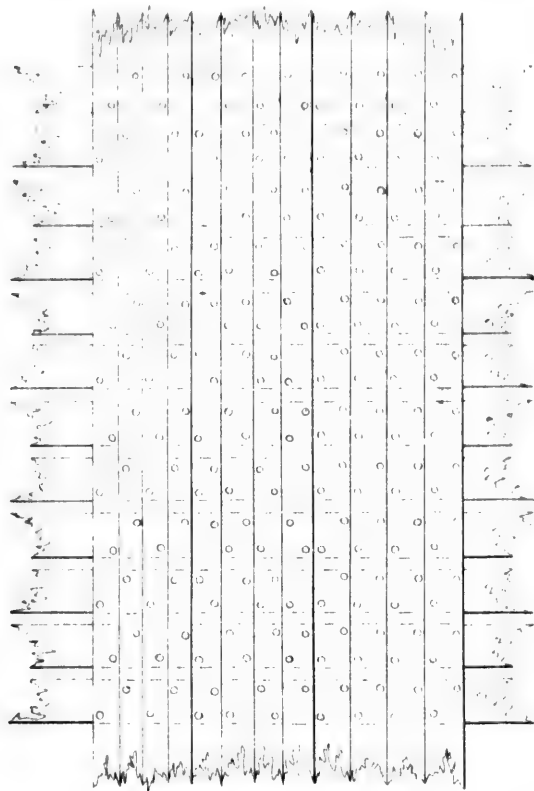
Sketch descriptive of the required shifting of Plank - Section 45



The sketch shows the principle on which the Birds should be arranged so as to avoid STEPPING which is deemed bad workmanship.

10-11-1880

Sketch showing the proper arrangement of Trunnels or Bolts Section 40



*Single Fastenings
in planks eight inches
wide & under*

*Double & Single
Fastening in planks
above 8 inches & not
above 11 inches*

*Double Fastening in
planks above 11 inches*

the heels of the stern timbers properly kneed. The heels of the cant timbers forward and aft to be stopped into the deadwood and bolted through.

42. Shifts of timber in ships of 200 tons and upwards, to be not less than one-seventh of the main breadth; and in ships under 200 tons, to be not less than one-sixth of the main breadth.

PLANK.

43. The outside planking to be of good quality, of the description prescribed in Table A, to be clear of sap and free from all defects.

44. The inside planking to be of the description shown in Table A, and free from all foxy, druxy, or decayed knots. With regard to the ceiling plank, and the efficiency of its fastening, it will be required that the planking shall be properly shifted and fastened so that there shall be at least either treenails or through bolts, or short bolts, in each plank of the ceiling *in every timber*.

45. No butts to be nearer than 5 feet to each other (*see also Section 39*), unless there be a strake wrought between them, and then a distance of 4 feet will be allowed; and no butts to be on the same timber, unless there be three strakes between, as more particularly shown in the diagram annexed (*see Plate*), but vessels under 200 tons will be exempted from the full operation of this rule; and in ships of larger tonnage a literal compliance with it will be dispensed with in cases wherein it may be satisfactorily proved that the departure from the rule is only partial, being confined to the ends of the ship, or the planking of the topside, and does not injuriously affect the ship's general strength; but such relaxation will not be sanctioned unless an accurate description of the shifting of the plank be transmitted by the Surveyors, to enable the Committee to form a proper judgment on the case.

The thickness of the plank, according to the tonnage of the ship, is not in any instance to be less than is prescribed in Table B.

The breadth of the wales in every case is to be regulated as under viz. :—

When the extreme length of the ship, measured from the fore part of the stem to the after part of the stern-post on the range of upper deck, is six times her depth of hold (and under), the wales are to be in breadth 3 inches to every foot of the depth of hold.

When the extreme length of the ship is eight times her depth of hold, the wales are to be in breadth $3\frac{1}{2}$ inches to every foot of the depth of hold.

When the extreme length of the ship is ten times her depth of hold (and above), the wales are to be in breadth 4 inches to every foot of the depth of hold.

And other intermediate dimensions in these proportions.

FASTENINGS.

46. Treenails to be of good quality, and of a description equal to the best material through which they pass; if, however, in Ships built in the British North American Colonies, or of Fir, treenails be used of materials not inferior to those comprised in line No. 2 in Table A, including Locust and all Australian and tropical hard woods of durable quality, and Beech in the bottom not higher than floor heads, a notation of "*Hard Wood Treenails*" will be inserted against the Ship's name in the Register Book. The Treenails are to be straight and circular, being either engine-turned, compressed, or planed, not grain-cut or knotty, and must be free from sap and tightly driven. In all cases in which planks above eleven inches in width shall be used, they must be double fastened; and those above eight inches in width must be treenailed double and single, except bolts intervene; and if less than that width then to be treenailed single. Not less than two-thirds of the treenails are to be driven through the inside planking, clamps, &c. Every butt in each outside plank to be fastened with *two* bolts, one of which may be in the adjoining timber, and one to be through and clenched.* The bilges to be secured with bolts so placed that from the foremast, extending a distance aft equal to three-fifths of the length of the keel, there shall, in ships under 300 tons, be at least one bolt through and clenched in each first foot-hook; and that in ships of 300 tons and upwards there shall be at least two bolts through and clenched for each set of timbers in one or other of the thick bilge strakes. All the bolts of the knees, breasthooks, crutches, riders, transoms, pointers, and keelsons, shelf pieces, *waterways*, *heels of timbers against fore and after deadwood*, and of all other material

* Where thick garboard strakes are used, they must be bolted horizontally through the keel and each other.

fastenings, are to be driven through and clenched on rings of the same metal as the bolts. The up and down bolts in the knees to beams are not required to be through the deck, but whether clenched upon the beams, or upon the deck, they must be clenched on rings of the same metal as the bolts. The two bolts, the nearest to the crowns of the pintles and braces of the rudder are also to be through and clenched, those through the braces to be in the main piece of stern post. The limber strakes to be bolted down to the floors, and one bolt in every floor, on each side, to be through and clenched.* When the heels of the first foothooks meet at the middle line on the keel under the keelson (either with full moulding or with *buted* choeks) the through bolting of the limber strakes may be dispensed with.

An additional year will be allowed to all ships of the A character, if fastened externally with treenails, and with copper or yellow metal bolts and dumps, to the exclusion of iron, from the lower part of keel up to the height of one-fifth of the depth of hold, below the upper side of the upper deck, in two or three decked ships, not being spar-decked ships, and below upper side of the main or tonnage deck in spar-decked ships, above which all fastenings of every description outside, and the whole of the inside fastenings must be properly galvanized, *except the frame bolts, and the bolts in iron straps on timbers, otherwise admitted of iron.*

And two years will be added to the A character if, in lieu of treenails above the floor heads, the whole of the planking is fastened with bolts of copper, or yellow metal to the above-named height, and above such height with properly galvanized iron bolts. All inside fastenings hitherto admitted of iron, including all frame bolts, and bolts in iron straps on timbers, or between two thicknesses of outside planking, must also be properly galvanized.

In all cases of Ships claiming extra time on the A character, the chain and preventor bolts are to be of properly galvanized iron, but the bolts in heels of timbers abutting against deadwood, forward and aft, must be of copper or yellow metal.

In all such cases of substitution, the bolts must be in number the same as is already prescribed above for treenails; the proportion of

* Watercourses are to be properly formed at underside of all floors and foothooks at the limbers on each side of middle line, so as to allow water to reach the pumps freely.

through bolts must be at least *two-thirds* ;* and all the through bolts must be of malleable metal, and clenched on rings (of the same metal) inside. The sizes of the copper or mixed metal bolts must be as under, viz. :—

| | |
|---|--------------------|
| In ships of 150 tons and under 200 tons†..... | $\frac{5}{8}$ in. |
| 200 " 350 " | $\frac{3}{4}$ in. |
| 350 " 500 " | $1\frac{3}{8}$ in. |
| 500 " 700 " | $\frac{7}{8}$ in. |
| 700 " 900 " | $1\frac{5}{8}$ in. |
| 900 and above..... | 1 in. |

and the lengths of the short bolts not less than as follows, viz. :—

| | |
|---|--|
| When used in plank of $2\frac{1}{2}$ inches, to be 7 inches long. | |
| " 3 " 8 " | |
| " 4 " 10 " | |
| " 5 " 12 " | |

and so on in proportion for plank of other thicknesses. The sizes of the bolts required in the several parts must not be less than is shown in Table D.

47. In every case where the butt and bilge bolts are not through and clenched, One Year will be deducted from the period which would otherwise be assigned in the classification of the vessel.

48. The Scantlings and dimensions for all sized vessels to be proportionately regulated, agreeably to Table B.

49. Ships surveyed while building, in which *all the materials required for a Twelve Years' ship shall have been used*, and most of the other requisites for that grade fulfilled, but which, from partial deficiencies, may not appear to be in all respects entitled to the full period, although superior to the description of a Ten Years' ship, may be marked in the book thus, 11 A ; thereby denoting that they are to remain on that grade *Eleven Years*, provided they be kept in a state of efficient repair.

50. Ships surveyed while building, in which every *alternate* set of timbers are frame-bolted together throughout their entire lengths, and the scantling and shifts of the timbers, the thickness and shifts of the planks, and size of fastenings may be the same as are required by the rules, and in which the chocks are wrought with a butt at each end, and the description of materials prescribed in Table A shall also have been used, but in which the frame is not so well

* Whenever metal fastenings are used in lieu of treenails, this proportion must be observed.

† Smaller sizes must not be used.

squared as is required for Twelve Years' ships, but which shall be *in other respects* equal thereto, shall be marked "10 A"; thereby denoting that they are to remain on that grade for *Ten Years*, provided they be kept in a state of efficient repair.

51. In all other cases, ships surveyed while building, and constructed of the materials of good quality, hereinafter shown in Table A, will be allowed the several terms of years respectively appointed, provided they be kept in a state of efficient repair. All ships, *not built under Survey*, whether in the United Kingdom or abroad, for which a character may be claimed, must be placed in dry dock or laid on blocks, in order that their bottoms may be seen and properly examined; they will also be required to have *their timbers completely exposed for examination, by a plank or listing* AS THE SURVEYOR (who must be an exclusive officer of the Society), MAY DIRECT, *being taken out, either inside or outside*, all fore and aft, *on both sides, equal to one entire strake*, at the first foothook heads, and another between decks, *and a few treenails must likewise be driven out*, so that the Surveyors, from actual inspection, may be satisfied whether or not they are of the quality and make prescribed by the Rules; and the same being thus ascertained, shall be reported to the Committee, and a character assigned. If the ship be 400 tons and upwards, the Survey must be made by two Surveyors, and their report signed accordingly.

52. Ships built under a substantial and efficient roof, kept in good repair, *which shall project at each end beyond the length, and on each side beyond the breadth, a quantity equal to half the breadth of the vessel*, shall have one year added to the period prescribed, provided they shall have been surveyed whilst building, and shall have occupied a period of not less than twelve months in their construction, *and in which no plank, except as follows, shall have been worked until the expiration of at least three months after the frame was completed, viz.:*—not more than three strakes of bilge planks, and two strakes of outside plank in the way of each tier of beams, also the clamps inside, so that the beams may be put in their places.

53. Ships built in the United Kingdom;—or in Quebec after 1851;—or St. John, New Brunswick, after 1853;—or Miramichi, and Northern Ports of New Brunswick, or in Prince Edward Island after 1855, or in Nova Scotia after 1864;—and *not surveyed while building* by the Surveyors to this Society, and all ships,

the owners or builders of which may have refused or declined to permit them to be surveyed at the several periods prescribed by the Rules, will have One Year deducted from the period which would otherwise have been assigned, in consequence of their not having been submitted to survey during their construction. In no case, however, will a higher grade than 10 A be assigned to ships built in the United Kingdom, which shall not have been surveyed while building.

CONTINUATION OF SHIPS A.

54. If, on the termination of the period of original designation, or if at any subsequent period not exceeding one-third of the number of Years assigned originally, or on Restoration, an Owner should wish to have his ship remain, or be replaced on the letter A (*vide section 59*), he is to send a written notice thereof to the Secretary, and the Committee shall then direct a Special Survey as follows, to be held by not less than *two* competent persons to be appointed by the Committee, one of them to be a Surveyor, the *exclusive officer* of the Society:—

SURVEY.—No. 1.

The ship to be either placed in dry dock or laid on blocks, so that the keel may be examined, and be scraped or *dubbed bright* from the light water-mark upwards, including the planksheers and waterways, so as to expose the surface of the plank to view.* A *listing, four inches wide, to be cut out of ceiling at each end of the hold on both sides, one-fifth the entire length of the ship between the keelson and air course under hold beam clamps. Bolts, if of iron, in number not less in any case than six on each side, in each*

* If the ship has been sheathed with wood over felt, fastened with copper or mixed metal nails, within a period of five years, and the plank from the light water-mark upwards shall, when so sheathed, have been brightened, and the condition of the bolts, planking, treenails, and caulking ascertained, and favorably reported upon by the surveyors; and provided that the sheathing which covers the binding bolts and raft ports, and a strake of sheathing all fore and aft on each side under the wales be removed, and listings of sheathing cut out at hood ends; and the planking, fastenings, and caulking so exposed, shall prove to be in good condition, then, on application to the Committee, the stripping from the light water-mark upwards may be dispensed with; but whenever the sheathing is removed, the outside planking is to be scraped or *dubbed bright*, and examined as prescribed by the above rule. If the ship has been sheathed with metal within a period of two years, and it shall appear to the Surveyors that stripping from the light water-mark upwards may be dispensed with, the case will receive due consideration on application to the Committee.

range of deck fastenings to be driven out in ships of 500 tons and under, and increased in number in proportion to the size of the ship, and one treenail to be driven out in every alternate frame or fourth timber, between the upper edge of the wales and planksheers, and one in every alternate frame or fourth timber between the upper edge of the wales and the light-water mark, and at such other parts of the bottom as the Surveyors may direct, so as to enable a judgment to be formed as to the general state of the treenails, timbers, and of the planking in the treenail holes. The hold to be cleared, and proper stages to be made, both inside and outside. All air courses and the timbers to be cleared for the examination of the timbers. The windlass to be unhung, and the wood linings stripped. The attention of the Surveyors should then be particularly directed to the state of the upper or main deck and comings, the upper and lower deck bolts, whether of iron or copper, and the outside planks through which they pass, the planksheers, waterways, and beams, so far as they can be examined; the hawse timbers, knight-heads, breast-hooks, and transoms; the floors and keelsons; the keel, rudder, and windlass; the planking outside and inside, and the treenails; and the frame and inner surface of the outside planking, where they can be seen; and the sheer and general form of the ship; the condition of the oakum and caulking also to be ascertained, and the ship to be efficiently repaired with suitable materials as hereinafter stated. Anchors, cables, and general equipments to be attended to as prescribed in Sections 71 to 76.

In British North American built Ships, where the middle line bolts are of iron, their condition is to be ascertained, and if this be not practicable, additional bolts of proper size must be driven through the keelson, floors, and keel, in each alternate frame, also through the stem, apron, and sternpost.

The Surveyors on these points shall transmit to the Committee a detailed report, accompanied by such observations as may occur to them, from inspection of the ship, or from information of the repairs she may have received. If from the report of such special survey the ship shall appear to be in a sound and efficient state, the Committee shall continue such ship on the letter A for such further period as they may think fit, not exceeding, however, one-third of the number of years which had been originally assigned, *subject to the usual annual survey.* Ships classed A for four years, will be allowed a Continuation for Two Years, provided that, in addition

to the above requisitions, the Owners shall have removed a strake in the topsides, fore and aft, on both sides ; this, however, will not exempt ships built in the British North American Colonies from the operation of the Rule, Section 6f. Ships so Continued shall be distinguished in the Register Book by the number of years for which the character is extended, being inserted separately under the number assigned on the original character, thereby denoting that the ship has been found on survey in such good and efficient order as to entitle her to be Continued for — years. The period assigned for Continuation will, upon all occasions, commence from the time the ship may have gone off the letter A, without regard to the date when the survey for this purpose may have been held.

SURVEY.—No. 2.

But should a Ship-owner at the period for Continuation as above described be willing to subject his ship to the following survey, the Committee will Continue such ship for a longer time, viz., not exceeding two-thirds beyond the term originally assigned, provided application be first made to the Committee for such survey.

And for the purpose of holding such survey, the ship must be placed in dry dock or laid on blocks upon ways, so that the keel may be examined ; *all sheathing to be entirely stripped off the bottom and elsewhere ; all the outside planking, from light water-mark upwards, including the planksheers and water-ways, to be scraped or dubbed bright ; all the air courses and the timbers to be cleared ; the timbers of the frame to be further exposed to view by the removal of planking equal to one strake fore and aft, on each side, above the wales ; a short plank in each buttock ; a strake of planking to be removed, or listing of sufficient breadth, not less than four inches, all fore and aft, on each side, at the discretion of the Surveyor, in the ceiling above the floor heads (or, if the ship-owner should prefer it, a strake of planking outside at the same height), and a reasonable number of treenails in addition to those above named, so as to ascertain the state of the lower timbers of the frame ; and in order to ascertain the condition of the beam ends, either a strake of deck next the water-ways on each side to be taken out, or an examination to be made by boring each end, at the option of the ship-owner ; iron-bolts and treenails to be driven out at the various parts as prescribed in Survey, No. 1 ; proper stages*

to be made *both inside and outside*; the windlass to be unhung, and the wood linings stripped; *and when in the state above described, the special survey to be held, as directed in Survey, No. 1, upon all the parts exposed to view; the condition of the oakum and caulking to be ascertained; the ship to be efficiently repaired with suitable materials, as hereinafter stated.*

The Surveyors on these points shall transmit to the Committee a detailed report, accompanied by such observations as may occur to them, from inspection of the ship, or from information of the repairs she may have received. If from the report of such special survey the ship shall appear to be in a sound and thoroughly efficient state, the Committee shall continue such ship on the letter A for such further period as they think fit, not exceeding, however, *two-thirds* of the number of years which had been originally assigned. This, however, will not exempt ships built in the British North American Colonies from the operation of the Rule, Section 63. Ships so Continued shall be distinguished in the Register Book by the number of years for which the character is extended, being inserted separately under the number assigned on the original character, thereby denoting that the ship has been found on survey in such good and efficient order as to entitle her to be Continued for — years. The period assigned for Continuation will, upon all occasions, commence from the time the ship may have gone off the letter A, without regard to the date when the survey for this purpose may have been held.

In cases of the repair of ships for Continuation of character under the Rules, section 54, materials of an inferior description (but not below those prescribed for the six years' grade) may be permitted to be used in those parts which must of necessity, under the operation of the Rules, section 56, be *entirely removed* on a repair for Restoration; subject, however, to the ship-owner, in every instance, making a special application to the Committee for their previous sanction.

All British North American built ships, and all Fir ships, seeking to be Continued with the character A for a period of two-thirds their original designation, must, in addition to the above requirements, be doubled diagonally from the keel to above the first strake of lower deck spirketting. The doubling to be of the thickness and be fastened as prescribed in Section 68.

RESTORATION OF SHIPS TO THE CHARACTER A.

FIRST RULE.

55. If, at any time before the expiration of two-thirds of the number of years *beyond* the period originally assigned, an owner be desirous to have his ship Restored to the A character, such Restoration (on his consenting to the special survey hereinafter described, to be held by two Surveyors, and performing the repairs found requisite) will be granted for a period not exceeding two-thirds of the time originally assigned, the same to be calculated from the date of such repairs.

Requisites for Restoration.

56. All the bolts in the range of each deck to be driven out, and the planks taken out; the upper deck waterways, and plank-sheers and spirketting, and the strake next the waterways on the lower deck in the midships, to be taken out;* the sheathing to be entirely stripped off the bottom; *all the outside planking from the light water-mark upwards, to be scraped bright*; a strake in the upper course of the bottom, between the wales and the light water-mark fore and aft, and a plank in the ceiling at the floor head, on each side, to be taken out, the limbers to be clear, and the hooks forward to be exposed; the windlass to be unhung, and the wood linings stripped; and in that state the ship to be submitted to a special survey and examination, at which the attention of the Surveyors appointed by this Society is to be particularly directed to the state

*In the cases of ships fastened with copper or yellow metal, and galvanized iron, in conformity with the Rule, Section 46, where there is no poop or fore-castle, if the whole of the plank-sheer and spirketting of upper deck and outside planking equal to one strake all fore and aft be removed in way of fastening bolts to each tier of beams, below the upper deck beams; and where there is a poop and fore-castle, then if the whole of the plank-sheer and spirketting of poop, fore-castle, and waist, and a strake of outside planking (in way of fastening bolts to upper deck beams), the entire lengths of the poop and fore-castle, and outside planking equal to one strake all fore and aft be removed in way of fastening bolts to each tier of beams below the upper deck beams; and in all cases a strake of deck next the waterway of each deck, on both sides, be removed, also the throat bolts of all knees, together with the other requisitions relating to the bottom, ceiling, &c., the further removal of fastenings, bolts, and planks, may, under the sanction of the Committee, be dispensed with, provided their condition be carefully ascertained and favorably reported upon by the Surveyors. The above relaxations, so far as they relate to the removal of plank and fastenings in the range of the lower deck, will be extended to all ships in which all the lower deck fastenings are of copper or yellow metal.

of the decks, the remaining plank of the topsides, the wales, upper courses, and treenails, and other fastenings ; also to the state of the frame, hawse timbers, and knight-heads, keelson, floors, foothooks, ceiling, and breasthooks, the rudder in all its parts and hangings. In British North American built Ships, where the middle line bolts are of iron, their condition is to be ascertained, and if this be not practicable, additional bolts of proper size must be driven through the keelson, floors, and keel, in each alternate frame, also through the stem, apron, and sternpost. If, after the above examination, the Owner should consent to take out all planks, timbers, beams, knees, waterways, fastenings, and other parts that may be found defective, or objected to, and replace them with materials of the same species, or of equal quality with those of which the ship was originally constructed, such ships to be entitled to Restoration for a period proportionate to their real condition and the extent of the repairs performed ; or if timber of an inferior description, or second-hand English or African Oak or Teak be used, then for a period not exceeding that for which such materials would have entitled a new ship to stand A according to the Tables, subject in either case to the ship being at all times thereafter kept in a state of efficient repair.

SECOND RULE.

57. If, *at any age* of a vessel, an Owner be desirous to have his ship Restored, such Restoration (on his consenting to the special survey hereinafter described, to be held by two Surveyors, and performing the repairs thereby found requisite) will be granted for so long a period as may be deemed expedient by the Committee, not exceeding, in any case, the term of eight years.

Requisites for Restoration.

58. The whole of the outside plank of the vessel to be taken off as low as the second foothook heads, and the remainder of the planking, either outside or inside, together with all the decks, to be removed, *so as to expose the timbers of the frame entirely to view*, the windlass to be unhung, and the wood linings stripped, and in that state the ship to be submitted to a special survey and examination, by the Surveyors appointed by this Society ; and if,

after such examination, all timbers, beams, knees, keelsons, transoms, breasthooks, remaining plank, inside or outside, or other parts found to be defective, be replaced with materials of the same species, or of equal quality with those of which the ship was originally constructed, and all the treenails driven out and renewed, such ship may be Restored. But if timber of an inferior description, or second-hand English or African Oak or Teak be used, then for a period not exceeding that for which such materials would have entitled a new ship to stand A according to the Tables, subject, in either case, to the ship being at all times thereafter kept in a state of efficient repair.

59. Ships which have been *Restored* shall be entitled to Continuation, subject to the same conditions of survey and examination as are prescribed for ships proposed to be Continued at the expiration of the period first assigned to them (Sec. 54); but in like manner, the term of such extended Continuance shall be limited to a period not exceeding one-third or two-thirds of the number of years for which the ships may respectively have been *Restored*, without any reference whatever to the period originally assigned to them.

At the termination of the several periods assigned to ships for remaining on the character A, they will have the word "*expired*" inserted against them; and if not surveyed prior to the reprinting of the Register Book, they will appear without any character.* But if during the *last year* of the period assigned to them, the owners of a ship shall, in consequence of her being about to proceed on a distant foreign voyage, apply to have her surveyed for continuation on the letter A, or for the character A in red, a special survey shall be held conformably to the Rules, sections 54 or 60, as the case may be: and if from the report of such special survey, the ship shall appear to be in all respects in a sound and efficient state, such as is required by those Rules, the Committee

* Ships launched between the 1st July, 1859, and the 30th June, 1863, inclusive, will retain the characters respectively assigned to them until the issuing of the Register Book for the year commencing on the 1st July next ensuing after the periods for which they have been classed shall have expired, provided they undergo the Surveys required by the Rules, and are kept in an efficient state of repair. In the case of ships launched on and after the 1st July, 1863, the period originally assigned to them on the A 1 character, will in every case date from the month in which the vessel may be launched, and will expire at the end of the corresponding month in the year at which the period assigned terminates. All ships launched previously to the 1st July, 1859, will remain under the Rules in force when they were built.

shall, from the period at which the ship's character would terminate, continue her on the letter A, or assign to her the character A in red in accordance with the Rules referred to.

SHIPS A, IN RED.

60. Ships that have passed the periods which have or might have been assigned to them for the character A originally, or on Continuation, or for Restoration, and shall be found on survey to be of a superior description, being fit for the safe conveyance of dry and perishable goods to and from all parts of the world, shall be classed A in Red, as the Second description of the First class.—(*For British North American built Ships and Fir Ships, see page 61.*)

In all cases in which the owner may claim this character, the ship must undergo a special survey by two surveyors, (to be appointed in every instance by the Committee), one of whom shall be an exclusive officer of the Society, and be subject in other respects to a compliance with the undermentioned requisitions of Survey, viz.:—

FIRST SURVEY FOR A, IN RED.

The ship to be either placed in dry dock or laid on blocks, so that the keel may be examined, and be scraped or dubbed bright from the light water-mark upwards, including the planksheers and waterways, so as to expose the surface of the plank to view.* Bolts, if of iron, in number not less in any case than six on each side, in each range of the deck fastenings to be driven out in ships of 500 tons and under, and increased in number in proportion to the size of the ship, and one treenail to be driven out in every alternate

* If the ship has been sheathed with wood over felt, fastened with copper or mixed metal nails, within a period of five years, and the plank from the light water-mark upwards shall, when so sheathed, have been brightened, and the condition of the bolts, planking, treenails, and caulking ascertained, and favourably reported upon by the surveyors; and provided that the sheathing which covers the binding bolts and raft ports, and a strake of sheathing all fore and aft on each side under the wales be removed, and listings of sheathing cut out at hood ends; and the planking, fastenings, and caulking so exposed shall prove to be in good condition, then, on application to the Committee, the stripping from the light water-mark upwards may be dispensed with; but whenever the sheathing is removed, the outside planking is to be scraped or dubbed bright, and examined as prescribed by the above rule. If the ship has been sheathed with metal within a period of two years, and it shall appear to the surveyors that stripping from the light water-mark upwards may be dispensed with, the case will receive due consideration on application to the Committee.

frame or fourth timber between the upper edge of the wales and planksheers, and one in every alternate frame or fourth timber between the upper edge of the wales and the light-water mark, and such other parts of the bottom as the surveyors may direct, so as to enable a judgment to be formed as to the general state of the treenails, timbers, and of the planking of the treenail holes. The hold to be cleared, and proper stages made both inside and outside. The windlass to be unhung, and the wood linings stripped. The attention of the surveyors shall then be particularly directed to the state of the upper or main deck and comings, the upper and lower deck bolts, and the outside planks through which they pass, the planksheers, waterways, and beams, so far as they can be examined; the hawse timbers, knightheads, breasthooks, and transoms; the floors and keelsons; the keel, rudder, and windlass; the planking outside and inside, and the treenails; and the frame and inner surface of the outside planking, where they can be seen; and the sheer and general form of the ship; the condition of the oakum and caulking also to be ascertained, and the ship to be efficiently repaired with suitable materials as hereinafter stated.

SECOND SURVEY FOR A, IN RED.

(After two-thirds the number of years beyond that assigned originally, or on Restoration, have expired.)

But when a period of two-thirds of the number of years beyond that originally assigned for a vessel's retaining the A character, or if a period of two-thirds the number of years beyond that assigned on Restoration has elapsed, an owner be desirous to have his ship *retain*, or be placed upon, this character, application must be made to the Committee in writing, who will direct the survey to be made by two surveyors, one of whom shall be an exclusive officer of the Society; and for the purpose of holding such survey, the ship must be placed in dry dock, or laid on blocks upon ways, so that the keel may be examined; all sheathing to be entirely stripped off the bottom and elsewhere; all the outside planking from light water-mark upwards, including the planksheers and waterways, to be scraped or dubbed bright; the timbers of the frame to be exposed to view by the removal of planking equal to one strake fore and aft, on each side, above the wales, either inside or outside at the dis-

cretion of the surveyor; a short plank in each buttock; a plank or listing of sufficient breadth, on each side, at the discretion of the surveyor, in the ceiling above the floor heads all fore and aft (or, if the ship-owner should prefer it, a plank outside at the same height), and a reasonable number of treenails before and abaft the same, so as to ascertain the state of the lower timbers of the frame; and in order to ascertain the condition of the beam ends, either a strake of deck next the waterways on each side to be taken out, or an examination be made by boring, at the option of the ship-owner; iron bolts and treenails to be driven out at the various parts as prescribed above;* proper stages to be made both inside and outside; the windlass to be unhung, and the wood linings stripped; and when in the state above described, the special survey to be held as above directed upon all the parts exposed to view; the condition of the oakum and caulking to be ascertained; the ship to be efficiently repaired with suitable materials, as hereinafter stated. And to entitle them to continue this character, such vessels will be required, in addition to the usual annual survey, to undergo a special re-survey, as prescribed above, within a period (from the date of the last special re-survey) not exceeding *two-thirds* of the several terms of years originally assigned to them, or earlier, if, in the judgment of the Surveyors, upon a careful examination of the ship, the same shall appear to them to be necessary.

In the repair of vessels for the above character, no materials may be used of a description inferior to those allowed in new ships for the Six Years' grade, except in the case of vessels originally classed for a shorter period than six years, when materials equal to those used in the original construction will be permitted.

All Ships built in the British North American Colonies and all Fir Ships, also all Ships, the original construction of which, by their timber materials, may not have entitled them to the A character for a longer period than five years, seeking the character A in red, must be surveyed as follows, viz.:—

If diagonally doubled, as per Sec. 68, and surveyed as required by the second clause of the rule, such ships may have the red A character for a period not exceeding that originally assigned, subject to a special re-survey at one half the period of exemption.

In cases where the owners object to double their ships, they may

* In the case of doubled ships, or ships of peculiar construction, special application may be made to the Committee.

submit them to the examination prescribed as under; and if reported to be in all respects in good and efficient condition, fit for the safe conveyance of dry and perishable cargoes to and from all parts of the world, such ships may be classed (subject to an annual survey), with the character A in red for a period not exceeding two-thirds that originally assigned to them.

SURVEY.

All the outside planking, from the height prescribed for doubling, to the lower part of choeks at floor heads, to be dubbed or scraped bright; a strake of outside planking to be removed all fore and aft on each side between the wales and light water-mark; a sufficient number of treenails to be driven out from various parts of the bottom, so as to ascertain their condition, and that of the plank and timbers through which they pass. The treenails extending from the upper part of choeks at second foothook heads to the lower part of choeks at floor heads to be renewed with through treenails of hard wood for at least half the length of the ship amidship, unless they are already of hard wood, and prove to be in good condition; and where the middle line bolts are of iron, their condition is to be ascertained, and if this be not practicable, additional bolts of proper size must be driven through the keelson, floors, and keel, in each alternate frame, also through the stem, apron, and stern post; and in all other respects the second clause of Rule for A in red is to be fully complied with.

Whenever it shall appear to the Surveyors that a vessel classed A, in Red, shall no longer be in a condition to deserve that character, notice of the proposal to reduce her shall be given in writing to the Owner, Master, or Agent, in the same manner as is prescribed by the rules, section 21, page 35.

N. B.--Ships at present classed with the AE asterisk character will be allowed to retain the same, subject to annual Survey, until the expiration of the period for which they have already been specially surveyed.

SHIPS Æ.

61. Ships that have passed the prescribed age for the A character, but have not undergone the repairs which would have entitled them to be Continued or Restored ; or having been Continued or Restored, and the additional period thus assigned expired, and also such ships as have never had an original character, which shall be found on survey fit for the conveyance of dry and perishable goods on *shorter voyages*, shall be distinguished by the diphthong Æ ; and a careful survey will be required to be made *annually*, or on the return of the ship from every foreign voyage, by one of the Surveyors to this Society, who is to state distinctly and separately the actual condition of the *upper deck fastenings, waterways, spirketting, planksheers, topsides, upper deck with its appendages, lower deck fastenings, wales, counter, plank, and treenails outside to the water's edge, rudder, windlass and capstan, beams, breasthooks, transoms, and timbers* ; but if not surveyed within twelve months, such ship having been during that time in some port in the United Kingdom, the character will be omitted until such survey be held ; or, as the case may be, she will be allowed to pass in the class E.

62. Ships built in the British North American Colonies, and all ships wherever built, the frames of which are composed of *Fir*, of 400 tons and above, shall, in order to entitle them to be classed in the Register Book of the Society, be secured in their bilges by the application of iron knee riders, or hanging knees and riders to cover the joints of the floor and foothook heads, to extend from the height of the hold beams to the floors so as to receive not less than two bolts in a substantial part of the floors ; the number of iron knees and riders to be not less than one of each to every hold or lower deck beam on each side. The knees to be connected with the riders or not, at the option or convenience of the owners ; but if not so connected, the side arms of the knees are to be of the length, and to be fastened as prescribed in Table F. The number of knees to each deck, and of riders, also their dimensions, and number of bolts, are fully explained in Table F. All ships built in the Colonies will be considered as "iron fastened" in their centre lines, unless it shall be satisfactorily shown to the contrary, either by the exposure of some of the bolts, or by a certificate to be produced from the builders.

On and after the 1st January, 1858, ships which proceed to sea *without being fastened with the iron knees and riders prescribed by the rules, will have one year deducted from the period to which they would otherwise be entitled to be classed in the Register Book.*

Ships built in the British North American Colonies, and all ships the frames of which are composed of Fir, of 600 tons and upwards, *and all ships (wherever built) the length of which (measured from the fore part of the stem to the after part of the stern-post on the range of upper deck), shall exceed five times their extreme breadth, or eight times and under nine times their depth, shall have diagonal iron plates closely inserted either outside or inside the frame. If placed outside, the said plates to extend from the upper side of upper tier of beams to the lower part of chocks at first foothook heads amidship, and to the same perpendicular height forward and aft, measured from the lower part of the keel; and if placed inside, the plates are to extend from the upper side of upper tier of beams to the lower part of chocks at floor heads. Whether placed outside or inside, the sizes of the plates not to be less than as follows, viz. :—*

| | | | | |
|--|----------------|----|----------------|-------|
| In ships of 100 tons and under 200 tons..... | $3\frac{1}{2}$ | by | $\frac{7}{16}$ | inch. |
| “ 200 “ 400 “ | 4 | by | $\frac{1}{2}$ | “ |
| “ 400 “ 700 “ | 4 | by | $\frac{5}{8}$ | “ |
| “ 700 “ 1000 “ | $4\frac{1}{2}$ | by | $\frac{3}{4}$ | “ |
| “ 1000 “ 1500 “ | 5 | by | $\frac{3}{4}$ | “ |
| “ 1500 and above..... | $5\frac{1}{2}$ | by | $\frac{7}{8}$ | “ |

and to be fastened with bolts, one at each alternate timber if outside, and one at each timber if inside, not less in diameter than the sizes given for “through butt bolts” in Table D.

The number of plates to be in proportion of not less than one pair to every 12 feet of the ship's entire length taken as above, but not to be more than eight feet asunder measured on a square; the said plates are to be placed diagonally, at an angle of not less than 45 degrees, their lower ends pointing to the after end of the keel in the after body, and to the fore end of the keel in the fore body, four pairs crossing each other amidship.

All such ships to have shelves and waterways to each tier of beams, each equal in contents to the transverse sectional area of the beams of their respective decks at their ends; each of the said shelves and waterways to be bolted through the outside planking at every timber, with bolts of the sizes given in Table D; likewise the

shifts of inside and outside planking not to be less than 6 feet, unless there be a strake wrought between them, and then a distance of 5 feet will be allowed.

In ships the length of which shall exceed *six* times their extreme breadth, or nine times and under ten times their depth, the number of plates must be not less than one pair to every *ten* feet of the ship's entire length taken as above, but not to be more than six feet asunder measured on a square, and to be placed diagonally as above described.* And in addition to the requirements for ships of five times their breadth in length, such ships must be fitted with a rider keelson, or a pair of sister keelsons, at the option of the owner,—the transverse sectional area of such rider keelson or sister keelsons each to be equal to two-thirds of that required in Table B for main keelsons. If a rider keelson be adopted, it is to be fastened with a through bolt (of the size required in Table D for keelson bolts) in every frame; or if the owner prefers it, every intermediate bolt may be short, passing through the main and rider keelsons.† If sister keelsons be fitted, they must be fastened with through bolts, in number not less than one in every alternate timber, and of the size required in Table D for "scarphs of keels," &c.

63. All British North American built ships, which have gone, or may go off the List of Ships of the A character, or which may be of an age exceeding the period for which they might have had claims to be put upon that grade (whether classed or not), shall, as from time to time they come under examination, be subjected to a careful survey, to be made by one of the Surveyors to this Society; and no further character shall be assigned them unless a survey shall be held as follows; and planking, either inside or outside, at the discretion of the Surveyors, in quantity equal to *one entire strake* fore and aft on both sides, shall be removed; to be taken out in midships immediately above the turn of the bilge, and *at such height* forward and aft as may, in their judgment, best expose the timbers of the frame to view; that a special report of the state of these timbers, and of the general state and condition of ^{the} upper

* In cases where the length of the ship exceeds ten times its depth, the builders or owners are to submit, through the resident surveyor, for the Committee's approval, their plans for giving the vessel the necessary strength longitudinally.

† In all cases in which a rider keelson is fitted, it must be fastened as prescribed above, irrespective of the relative dimensions of the ship.

deck fastenings, waterways, spirketting, planksheers, topsides, upper deck with its appendages, lower deck fastenings, wales, counter, plank and treenails outside to the water's edge, rudder, windlass and capstan, beams and breasthooks, shall be transmitted by the Surveyors to the Committee; and on the receipt of such report the character shall be assigned. If the *Æ* character be then assigned, it shall be continued (subject to an annual survey) for a period not exceeding the number of years originally assigned; at the expiration of which the character will be discontinued, unless a similar survey and examination of the frame be again submitted to.

SHIPS E.

64. Will comprise all ships which shall be found on survey fit for the conveyance of cargoes not in their nature subject to sea damage *on any voyage*.

65. Subject to occasional inspection, at least once in every two years, ships will continue in this class so long as their condition shall, in the opinion of the Committee, entitle them thereto.

SHIPS I.

66. Will comprise ships which shall be found on survey fit for the conveyance, *on shorter voyages*, (not out of Europe), of cargoes in their nature not subject to sea-damage.

67. The Bottom of every ship is to be CAULKED* once in every five years, unless wood-sheathed and felted, and then once in every seven years, except in the case of *Teak-built ships*, upon which a special survey may have been requested, and the Surveyors having ascertained, by the removal of a strake of sheathing fore and aft under the wales, and a strake at the first foothook heads, and by

* In cases where ships have been doubled with doubling of less thickness than is required by, or not fastened in accordance with, the rules, it will not be imperative that such doubling be stripped at the expiration of seven years as required for ordinary sheathing; but if, upon survey, the doubling be found in good condition, the period for its remaining on may be extended, with the sanction of the Committee, to a term not exceeding ten years, provided the doubling below the wales be copper or yellow metal fastened or treenailed.

causing listings to be cut out at the wood's ends, that such caulking is not required, the same may then be dispensed with. If any ship shall be stripped within the periods above mentioned, her bottom is to be caulked, *if necessary*.

68. In all cases in which ships may be doubled, doubling of not less than the thickness hereinafter mentioned will be required, the same to be properly wrought and fastened as follows: in every instance the doubling is to be at least single fastened either with treenails or with bolts, and a through bolt in every butt. If treenails be used, every treenail must, if practicable, be a through fastening; and if bolts be used, then one-sixth of them from the lower part of the bilge upwards must be through and clenched on the ceiling in addition to the butt bolts. In all cases of doubling, the rudder braces are to be removed.

The throat bolts of iron knees, and the bolts of iron hooks, crutches, and pointers, must be renewed through the doubling.

The thickness of the doubling for the wales and bottom, on ships

Under 400 tons to be not less than.....2 inches.

of 400 " and under 600 tons.....2½ "

of 600 " and above.....3 "

On the Topsides of ships not exceeding 300 tons, the thickness may be 1½ inches.

If the doubling be applied diagonally, it will be allowed to be of the following thicknesses, viz. :—

In ships under 500 tons.....1½ inch.

" 500 tons and under 1,000 tons.....2 "

" 1,000 tons and upwards.....2½ "

No ship hereafter doubled, shall be entitled to the character A, or A in red, unless at the time of doubling it be ascertained, in either case, that the frame is capable of securely retaining the fastenings, *by one treenail being driven out in every alternate frame or fourth timber between the upper edge of the wales and the light water-mark, and at such other parts of the bottom as the Surveyors may direct, so as to enable a judgment to be formed as to the general state of the treenails and timbers, and of the planking in the treenail holes*, or should the state of the treenails indicate defective timbers, or should the outside plank be bolt fastened, then, by cutting out listings or plank, at the discretion of the Surveyor.

Diagonal doubling on ships built in the British North American Colonies, or on ships built of fir, is to be fastened as under, viz. :—

If worked not above 11 inches broad may be single fastened with a through bolt at every butt, every *fifth* fastening to be a through bolt or a through treenail of hard-wood; the distance between these through fastenings not to exceed 4 feet 6 inches. The remaining fastenings to consist of through treenails or two long and two short dump bolts; the length of the short dumps may be half an inch less than the combined thickness of the doubling and the original outside plank, and that of the long dumps to be not less than the thickness of the doubling added to twice the thickness of the original outside plank.

Before doubling, the original fastenings in the outside planking and the rider bolts should be ascertained to be in efficient condition, or made good, but all treenails, from the lower part of the chocks at the floor heads, to the upper part of the chocks at the second futtock heads throughout the bilges for one half the length of the ship amidships, shall be *renewed* through the original inside and outside planking with hard wood treenails, unless such treenails were originally of hard wood or have been recently renewed—then application may be made to the Committee with the view of dispensing with this requirement. In all cases the throat bolts and the bolt next thereto in the iron knees and riders must be renewed through the doubling. The upper ends of the diagonal doubling to be worked against a fore and aft strake of doubling, the upper edge of which is to be *let into* the original plank sufficient to form a caulking seam, say not less than $1\frac{1}{4}$ inch. The lower ends of the diagonal doubling to be worked against two strakes of fore and aft doubling, the lower edge of the lower strake being rabbetted into the keel, and to be not less in thickness than one-and-a-half times the thickness of the doubling. All diagonal doubling to be of rock elm or of equally suitable material, and to be wrought on hair felt.

IRON-FASTENED SHIPS.

69. All ships although iron-fastened (except as hereinafter mentioned) shall be classed in the same manner as copper-fastened ships, so long as they remain unsheathed with copper, provided they are, in all other respects, constructed in accordance with the Rules; but when sheathed with copper over the iron fastenings, the words "Coppered over Iron Bolts" shall be added to the character in the

Register Book, and continued until the ship be thoroughly copper-fastened.

70. Ships built in India, although fastened with iron, shall be permitted to be copper-sheathed without any mark being placed in the Book, provided the bottom be felted or chunamed and wood-sheathed, and subjected to a careful examination of the iron-fastenings on every occasion on which the sheathing is stripped off, for which purpose some of the bolts and nails are to be taken out of the lower part of the bottom, and to be seen by the Surveyor; but no such ship shall be permitted to continue either on the A or on the A in red class for a longer period than one-half the number of years beyond the term originally assigned for her remaining on the A character, unless the bottom shall have been doubled, or the whole of the iron fastenings taken out or properly secured, and the bottom refastened with bolts, or treenails, or both including the middle line, breasthook, and crutch bolts.

EQUIPMENT.

71. All vessels are required to have their masts, spars, and rigging, the rudder, pumps, windlass or capstan, scuppers and hawse pipes, in good order, and sails in sufficient number and in good condition.

72. Every ship is to be provided with anchors, cables, &c., of approved quality, properly tested at a *public machine*,* in number and length, as set forth in the Table, No. 22, annexed.

For equipment the *total tonnage* of the ship is to be taken.

A Certificate of all Chains and Anchors having been tested, and of the strain applied to them, must be produced before the Ship is classed.

73. The length and condition of the Chain Cables are to be ascertained by removal from the lockers on every Special Survey for Classification.

74. In all cases where hempen cables are used, one-sixth more in length will be required.

BOATS.

75. All vessels under 150 tons to be provided with one good Boat; and every vessel of 150 tons and above to have a suitable number.

* See Notice No. 170 in Appendix at end of Register Book.

76. The efficient state and condition of the whole of the ship's equipment will be designated by the figure 1 ; and where the same are found insufficient in quantity, or defective in quality, by the figure 2.

SHIPS NAVIGATED BY STEAM.

✓ 77. Steam ships are to be subject to the same periodical surveys as sailing vessels, and whenever the boilers are taken out, the vessel is to be submitted to a particular and special survey, in order to ascertain her general condition. ✓

✓ 78. That with respect to the Boilers and Machinery, the Owners are required to produce to the Surveyors at the above-directed surveys, a certificate from some competent *Engineer*, describing their state and condition at those periods ; and to which certificate it is desirable there should be added a description of the particulars of the same, as far as may be practicable, in the manner and form annexed, No. 8 ; to be appended to the report of survey, and delivered to the Committee, who will thereupon insert in the Register Book the letters " M. C." denoting that the boilers and machinery have been inspected and certified to be in good order and safe working condition ; but if no certificate of their condition be furnished by the Owner or Master, then no character can be assigned for the machinery. ✓

✓ 79. HULL :—The Surveyors are directed to examine and report the scantling of timbers, plank, and fastenings, and to state where built, and by whom, in the same manner as directed for sailing vessels. ✓

80. The Surveyors are required to report the number, size, length, fastenings, and mode of arrangement of the engine and boiler *sleepers*, and the description of timber of which they are composed, and whether diagonally trussed with wood or iron, and to what extent ; the length, size, and fastenings of shelf-pieces and paddle-beams ; and whether the vessel be constructed with sponcings, and how they are formed ; and to give the length and shifting of the plank outside and inside.

✓ 81. MATERIALS AND EQUIPMENT :—The Surveyors are to examine and report the number and description of the masts, sails, anchors, cables, hawsers, warps, and boats, as directed to be done for sailing vessels ; but the anchors and cables will not be required to exceed

in weight and length those of a sailing vessel of two-thirds of the total tonnage of the steam vessel. ✓

✓ 82. The Surveyors are to be particular in examining and reporting the condition of the boats of all vessels employed in carrying passengers. ✓

FOREIGN BUILT SHIPS.

It having been deemed desirable that *Foreign Built Ships*, which have not been constructed in accordance with the Rules of the Society, should nevertheless be entered in the Register Book with a character of efficiency if their condition be such as to entitle them thereto,

NOTICE is hereby given, that in pursuance of Resolutions passed by the Committee this day, the following Regulations have been adopted for their Survey and Classification, viz.:—

“Foreign Built Ships which have not been constructed in accordance with the Rules, and have not been surveyed by the Surveyors to this Society while building, for which the owners are desirous of a character of condition or efficiency for sea-going purposes, will be surveyed for entry in the Register Book on application being made to the Committee, in writing, stating the Name of the Vessel (and if at any time she had any other name such is to be inserted in the application); likewise where and when she was built, and her length, breadth, depth, and tonnage (whether British or Foreign.)

“The Committee will then direct a special survey to be held by two Surveyors, to be appointed in every instance by the Committee, one of whom at least shall be an exclusive officer of the Society, and the ship submitted to a compliance with the undermentioned requisitions of survey, viz.:—

“In all cases the ship must be placed in dry dock or laid on blocks, so that the keel and bottom may be seen and properly examined; the hold to be cleared and proper stages to be made both inside and outside; the limbers to be cleared, bolts and treenails to be driven out at different parts of the ship, and in sufficient number to enable the Surveyors to ascertain their condition; the condition of the plank and timbers in the treenail holes also to be ascertained; the beam ends in ships of four or more years old must

be examined by boring. The Surveyors must then examine and report upon the ship, as to the state of the timbers of the frame (where examined), planking inside and outside, decks, waterways, beams, knees, keel, keelsons, stem, apron, hawse timbers, knight-heads, breasthooks, transomes, rudder, and windlass, the sheer and general form of the ship, particulars of materials and scantlings, so far as they can be ascertained, and spacing of timbers and beams, thickness and shifting of plank, mode of fastening, sizes and condition of bolts and treenails, and state of caulking in all parts of the vessel.

"Survey No. 1. If the ship is less than four years old, a listing of not less than four inches wide and equal to one-fifth of the length of the ship on each side, to be cut out below each set of clamps or shelves in such parts as the surveyors may require, sufficient to enable them to ascertain the side and condition of the frame.

"Survey No. 2. If the ship is four or more years old, she must be scraped bright from the light water-mark upwards, including the planksheers and waterways, and a listing of not less than four inches wide must be cut fore and aft below each set of clamps or shelves, and at the bilges at the discretion of the Surveyor, and a short listing outside at each buttock. This must apply to all ships of four or more years old, whether they have had the short listings previously cut or not.

"If after such examination all repairs are done to the satisfaction of the Surveyors, so as to enable them to make a favorable Report, a class of efficiency will be granted by the Committee, and entered in the Register Book, which class will be retained for twelve months only, unless it shall be made appear by the owner that the ship has not been in any port in the United Kingdom during that period; but in no case will it be continued for more than two years unless the vessel be re-surveyed as above; but upon such re-survey the openings described therein will not be required to be repeated within a period of four years."

There will be three designations of condition or character, distinguished thus :

1 F

2 F

3 F

1 F denotes ships which are found on survey to be of a superior

description, fit for the conveyance of dry and perishable goods to and from all parts of the world.

2 F denotes ships which, although not equal to the foregoing, are nevertheless found on survey to be in a good and efficient condition, and fit for the conveyance of dry and perishable goods, on shorter voyages.

3 F denotes ships which shall be found on survey fit for the conveyance of cargoes not in their nature subject to sea damage.

It is to be distinctly understood that the foregoing regulations will be confined in their application to *Foreign Built Ships*.

To entitle the ships to the figure 1, they must be supplied with stores in accordance with Table 22, attached to the Rules.

SHIPS WITH IRON FRAMES AND WOOD PLANKING.

The Committee's attention having been called to the principle of building ships with *iron* frames and *wood* planking, they have considered it right to recommend the following suggestions for adoption if the ships are intended for classification in the Register Book, viz. :

Where Iron frames are intended to be planked with but *one* thickness of wood, the space from moulding edge to moulding edge not to exceed eighteen inches ; but if the outside planking be in *two* or *more* thicknesses, *worked diagonally*, the distance may be increased.

In either case thick garboard strakes are required ; and when one thickness only is intended, the planking from garboards to the upper edge of wales is not to be less in thickness than is required in Table B for *wales* in wood ships, and thence upwards to be of the thickness required for *sheerstrakes*.

All such ships to have rivetted outside the frames a deep plate fore and aft at top height, or sheerstrake, and one fore and aft of less breadth at lower part of bilges ; also to have narrow plates diagonally extended from one to the other, rivetted to them and to the frames which they cross,—the said diagonal plates to be from six to eight feet asunder on a square.

To have a plate fore and aft on upper side of wood keel, extended up the lower part of stem and sternpost ; the said plate to be rivetted to all frames, and secured to the keel between the frames ; also to be of sufficient breadth to receive fastenings in thick garboard

strakes. The thickness of the above-named plates may be the same as is required for stringer plates on ends of beams in iron ships.

BEAMS:—Their size and distance apart to be as required for iron-plated ships, and to have stringer plates upon their ends, also fore and aft, and diagonal tie-plates rivetted upon upper side of beams, as is required for iron-plated ships.

Butts of outside planking to be placed in centre of space between two frames, and through-bolted upon a plate rivetted to the frames, the plate to be of the width of planking and not less than the thickness of the frames.

Middle-line, side, and bilge keelsons, and angle iron on stringer plates, not to be less in scantlings than are required for iron-plated ships of the same dimensions.

Floors, if of plate iron, to be of the dimensions required for iron-plated ships.

When the outside planking is in two or more thicknesses, *the inner thicknesses* (if more than one), *provided they be entirely of teak*, may be fastened to the frame with galvanized iron bolts in ships claiming, in other respects, the 14 years' grade, under the Rules, Section 46.

If the garboard strakes are in one thickness they may be of elm, but the 'thwartship bolts must be of copper or yellow metal, to entitle the ship to the advantage of such fastenings in classification.

In other respects the classing of such ships to be governed by the description of wood material which may be used for outside planking and other parts.

To entitle such ships to Classification, the plans on which they are to be built must be first submitted to the Committee for their approval.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

2, *White Lion Court, Cornhill,*
1st July, 1865.

RULES

FOR THE BUILDING AND CLASSIFICATION OF SAILING AND STEAM VESSELS BUILT OF IRON.

All vessels will be classed A so long as on careful annual and periodical *special* surveys they are found to be in a fit and efficient condition to carry dry and perishable cargoes to and from all parts of the world.

Differences of construction, as regards thickness of plating, strength, and probable durability, &c., will be indicated by the letters A B and C placed inside the letter A,—thus, A A A.

A A will denote that the vessels have been built in accordance with, or equal to, the Rules and Table G.

A will denote vessels which are considered entitled to the A character, but which have not been built in accordance with the Rules.

All vessels to be subject to occasional or annual survey when practicable.

To entitle Ships to retain their respective characters in the Register Book, the following Special Surveys must be held periodically.

SURVEY No. 1.—The vessel to be placed on blocks of sufficient height in a dry dock, or on ways; the limber boards, and ceiling equal to one strake fore and aft on both sides removed, with both surfaces of outside plating exposed.*

SURVEY No. 2.—The vessel to be placed on blocks of sufficient height in a dry dock, or on ways; the limber boards, and ceiling equal to *three* strakes fore and aft on both sides removed, with both surfaces of outside plating exposed.*

SURVEY No. 3 BY TWO SURVEYORS, ONE TO BE AN EXCLUSIVE OFFICER OF THE SOCIETY.—The vessel to be placed on blocks of sufficient height, in a dry dock, or upon ways; proper stages to be

* In cases where the inner surface of the bottom plating is coated with cement or asphalt, if a sufficient quantity of ceiling be removed to enable the coating to be carefully inspected, and tested by beating or chipping, and the coating be found sound and good, adhering satisfactorily to the iron, the removal of such coating will be dispensed with. Ships which have undergone the above examination will be noted in the Register Book thus (*s.s.No.1-65*), (*s.s.No.2-65*), (*s.s.No.3-65*); and if not submitted to such survey, will be liable to have their character suspended.

made and the hold to be cleared, the close ceiling in the hold to be removed, so that the rivets and plates of keel, and flat of bottom, may be thoroughly examined; coal bunkers of steam vessels to be cleared, the whole of the frames, stringers, hooks, floor plates, keelsons, engine and boiler bearers,* ends of beams, water-tight bulk-heads, rivets, and inner surface of the plating exposed; all oxidation to be removed by being cut or beaten off the several parts above-named, also from the outside plating, rivets, keel, stem, sternpost, and rudder, so as to completely lay bare all the surfaces of iron; the planksheers and waterways, if of wood, to be scraped bright; and when the vessel is so prepared, the Surveyors are to examine the condition and thickness of all the parts of iron above-named, also the condition of the planksheers, waterways, flat of decks and their fastenings; such parts as may be found defective or less than three-fourths of the required substance by Rule, are to be removed and replaced with proper materials, equal in substance and quality to the original construction.

Whenever the bottom plating is to be cemented, a survey is to be held prior to the cement being laid.

Every ship classed Δ must be submitted to a *special periodical survey* every *four* years:—the first survey according to No. 1; the second according to No. 2; the third according to No. 3; and afterwards according to No. 1 and No. 3 *alternately* at intervals of four years.

Every ship classed Δ_B must be submitted to a special periodical survey every *three* years, as per No. 1, 2, and 3, afterwards No. 1 and 3.

Every ship classed Δ_C must be submitted to a special periodical survey every *two* years, as per No. 1, 2, and 3, and afterwards No. 1 and 3.

* Whenever the engines and boilers are taken out for repair, the engine and boiler bearers, with the floor-plates, keelsons, rivets, etc., under them may, at the request of the Owners, be surveyed in anticipation of the above rule.

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Table G.

IRON SHIPS.—Table of Minimum Dimensions of Frames, Plating, Rivets, Keels, Keelsons,

All plates, and all beam and angle iron, used in ships intended for classification, are to be stamped legibly in two places.

| Gross Tonnage. | Keel, Stem, and Stern Post for all Grades.* | Distance of Frames from Moulding edge to Moulding edge all fore & aft for all Grades. | FRAMES. | | THICKNESS OF OUTSIDE PLATES.† | | Garboard Strakes* and Single Plate-Middle Line Keelsons standing upon floors. | | From the Garboard to the upper part of Bilge and the Sheerstrakes.* | | From upper part of bilge to a perpendicular height from upper side of keel of three-fifths the internal depth of hold, measured from the upper side of upper deck in all Ships, whether spar-deck or otherwise. | |
|---------------------|---|---|---|--|-------------------------------|-----------------|---|-----------------|---|-----------------|---|-----------------|
| | | | Dimensions of Angle Iron for all Grades. | Dimensions of Reversed Angle Iron on Frames, Bulkheads, and Box Keelsons for all Grades. | | | | | | | | |
| | | | Inches. | Inches. | Inches. | Inches. | Inches. | Inches. | Inches. | Inches. | Inches. | Inches. |
| 100 and under 200 | 6 x 1 $\frac{1}{2}$ | | 1 $\frac{5}{8}$ x 2 $\frac{1}{2}$ x 3 $\frac{1}{2}$ | 1 $\frac{5}{8}$ x 2 $\frac{1}{4}$ x 2 $\frac{1}{4}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 200 and under 300 | 6 $\frac{1}{4}$ x 2 | | 1 $\frac{5}{8}$ x 2 $\frac{1}{2}$ x 2 $\frac{1}{2}$ | 1 $\frac{5}{8}$ x 2 $\frac{1}{4}$ x 2 $\frac{1}{4}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 300 and under 400 | 6 $\frac{1}{2}$ x 2 $\frac{1}{4}$ | | 1 $\frac{5}{8}$ x 3 $\frac{1}{4}$ x 2 $\frac{3}{4}$ | 1 $\frac{5}{8}$ x 2 $\frac{1}{2}$ x 2 $\frac{1}{2}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 400 and under 500 | 6 $\frac{3}{4}$ x 2 $\frac{1}{2}$ | | 1 $\frac{5}{8}$ x 3 $\frac{1}{2}$ x 2 $\frac{3}{4}$ | 1 $\frac{5}{8}$ x 2 $\frac{3}{4}$ x 2 $\frac{1}{2}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 500 and under 600 | 7 x 2 $\frac{1}{2}$ | | 1 $\frac{5}{8}$ x 3 $\frac{3}{4}$ x 2 $\frac{3}{4}$ | 1 $\frac{5}{8}$ x 3 x 2 $\frac{1}{2}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 600 and under 700 | 7 x 2 $\frac{3}{4}$ | | 1 $\frac{5}{8}$ x 4 x 3 | 1 $\frac{5}{8}$ x 3 x 2 $\frac{3}{4}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 700 and under 800 | 7 $\frac{1}{4}$ x 2 $\frac{3}{4}$ | | 1 $\frac{5}{8}$ x 4 $\frac{1}{4}$ x 3 | 1 $\frac{5}{8}$ x 3 x 2 $\frac{3}{4}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 800 and under 900 | 7 $\frac{1}{2}$ x 3 | | 1 $\frac{5}{8}$ x 4 $\frac{1}{2}$ x 3 | 1 $\frac{5}{8}$ x 3 x 3 | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 900 and under 1000 | 8 x 3 | | 1 $\frac{5}{8}$ x 4 $\frac{3}{4}$ x 3 | 1 $\frac{5}{8}$ x 3 $\frac{1}{4}$ x 3 | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 1000 and under 1200 | 8 $\frac{1}{2}$ x 3 | | 1 $\frac{5}{8}$ x 5 x 3 | 1 $\frac{5}{8}$ x 3 $\frac{1}{2}$ x 3 | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 1200 and under 1500 | 9 x 3 | | 1 $\frac{5}{8}$ x 5 x 3 $\frac{1}{2}$ | 1 $\frac{5}{8}$ x 3 $\frac{1}{2}$ x 3 | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 1500 and under 2000 | 10 x 3 | | 1 $\frac{5}{8}$ x 5 $\frac{1}{2}$ x 3 $\frac{1}{2}$ | 1 $\frac{5}{8}$ x 4 x 3 $\frac{1}{2}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 2000 and under 2500 | 12 x 3 | | 1 $\frac{5}{8}$ x 6 x 4 | 1 $\frac{5}{8}$ x 4 $\frac{1}{2}$ x 3 $\frac{1}{2}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 2500 and under 3000 | 12 x 3 $\frac{1}{4}$ | | 1 $\frac{5}{8}$ x 6 $\frac{1}{4}$ x 4 | 1 $\frac{5}{8}$ x 4 $\frac{1}{2}$ x 3 $\frac{1}{2}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |
| 3000 | 13 x 3 $\frac{1}{2}$ | | 1 $\frac{5}{8}$ x 6 $\frac{1}{2}$ x 4 | 1 $\frac{5}{8}$ x 4 $\frac{1}{2}$ x 3 $\frac{1}{2}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ | 1 $\frac{5}{8}$ |

If single frames be adopted the space from centre to centre is not to exceed 21 inches, all fore and aft; but provided an additional frame, for half the vessel's length amidships, be fitted at opposite side of each floor plate across the keel, and extended to upper part of bilge, and rivetted through floor plates and main frames, also through the outside plating, as required for main frames, the space may be increased to 23 inches in ships under 1000 tons, and to 24 inches in ships of 1000 tons and upwards.

Table G.

Plates, Keels, Keelsons, Stems, Stern Posts, Floor Plates, Beams,† Bulkheads, Stringers, &c.
 stamped legibly in two places with the manufacturer's trade mark, or his name, and the place where made.

STEEL PLATES.†

on upper part of bilge to perpendicular height from side of keel of three-fifths the internal depth of hold, measured from the upper side of upper deck in ships, whether spar-deck or otherwise.

From three-fifths the depth of hold (measured from the upper side of upper deck in all ships, whether spar-deck or otherwise,) to lower edge of Sheerstrake.

Thickness of Stringer Plates upon Beams, Floorplates, Hooks, Crutches, and Box, or Intercoastal Keelsons for all grades.

Thickness of Plates for Bulkheads for all Grades.

Dimensions of Angle Iron on Beam Stringers or Keelsons for all Grades.

RUDDER
for all Grades.

Diameter at the Head. Diameter at the Heel.

Thickness of Wood Flat of Upper Deck

Gross Tonnage.

| | | | | Keelsons for all grades. | | the Head. the Heel. | | | | |
|---------------------------|---------------------------|---------------------------|---------------------------|---------------------------|---------------------------|---|----------------|----------------|---------------------------|---------------------------|
| Inches. $\frac{6}{16}$ | Inches. $\frac{5}{16}$ | Inches. $\frac{6}{16}$ | Inches. $\frac{5}{16}$ | Inches. $\frac{5}{16}$ | Inches. $\frac{4}{16}$ | Inches. $\frac{6}{16} \times 3 \times 3$ | Inches. 3 | Inches. 2 | Inches. $2\frac{1}{2}$ | 100 and under 200 |
| $\frac{7}{16}$ | $\frac{6}{16}$ | $\frac{6}{16}$ | $\frac{5}{16}$ | $\frac{6}{16}$ | $\frac{3}{16}$ | $\frac{6}{16} \times 3 \times 3$ | $3\frac{1}{2}$ | 2 | $2\frac{1}{2}$ | 200 and under 300 |
| $\frac{8}{16}$ | $\frac{7}{16}$ | $\frac{7}{16}$ | $\frac{6}{16}$ | $\frac{7}{16}$ | $\frac{5}{16}$ | $\frac{6}{16} \times 3\frac{1}{2} \times 3$ | $3\frac{3}{4}$ | $2\frac{1}{4}$ | 3 | 300 and under 400 |
| $\frac{9}{16}$ | $\frac{7}{16}$ | $\frac{7}{16}$ | $\frac{6}{16}$ | $\frac{7}{16}$ | $\frac{5}{16}$ | $\frac{6}{16} \times 4 \times 3$ | $4\frac{1}{4}$ | $2\frac{1}{2}$ | 3 | 400 and under 500 |
| $\frac{10}{16}$ | $\frac{8}{16}$ | $\frac{8}{16}$ | $\frac{7}{16}$ | $\frac{8}{16}$ | $\frac{6}{16}$ | $\frac{7}{16} \times 4\frac{1}{4} \times 3\frac{1}{4}$ | $4\frac{1}{2}$ | $2\frac{3}{4}$ | $3\frac{1}{2}$ | 500 and under 600 |
| $\frac{11}{16}$ | $\frac{8}{16}$ | $\frac{8}{16}$ | $\frac{7}{16}$ | $\frac{8}{16}$ | $\frac{6}{16}$ | $\frac{7}{16} \times 4\frac{1}{2} \times 3\frac{1}{2}$ | $4\frac{3}{4}$ | $2\frac{3}{4}$ | $3\frac{1}{2}$ | 600 and under 700 |
| $\frac{12}{16}$ | $\frac{9}{16}$ | $\frac{9}{16}$ | $\frac{8}{16}$ | $\frac{9}{16}$ | $\frac{6}{16}$ | $\frac{8}{16} \times 4\frac{3}{4} \times 3\frac{3}{4}$ | 5 | 3 | $3\frac{1}{2}$ | 700 and under 800 |
| $\frac{13}{16}$ | $\frac{9}{16}$ | $\frac{9}{16}$ | $\frac{8}{16}$ | $\frac{9}{16}$ | $\frac{6}{16}$ | $\frac{8}{16} \times 5 \times 4$ | $5\frac{1}{4}$ | 3 | $3\frac{1}{2}$ | 800 and under 900 |
| $\frac{14}{16}$ | $\frac{10}{16}$ | $\frac{10}{16}$ | $\frac{9}{16}$ | $\frac{10}{16}$ | $\frac{7}{16}$ | $\frac{9}{16} \times 5 \times 4\frac{1}{4}$ | $5\frac{1}{2}$ | 3 | $3\frac{1}{2}$ | 900 and under 1000 |
| $\frac{15}{16}$ | $\frac{10}{16}$ | $\frac{10}{16}$ | $\frac{9}{16}$ | $\frac{10}{16}$ | $\frac{7}{16}$ | $\frac{9}{16} \times 5 \times 4\frac{1}{2}$ | $5\frac{3}{4}$ | 3 | 4 | 1000 and under 1200 |
| $\frac{16}{16}$ | $\frac{11}{16}$ | $\frac{11}{16}$ | $\frac{10}{16}$ | $\frac{11}{16}$ | $\frac{7}{16}$ | $\frac{9}{16} \times 5\frac{1}{2} \times 4\frac{1}{2}$ | 6 | $3\frac{1}{4}$ | 4 | 1200 and under 1500 |
| $\frac{17}{16}$ | $\frac{11}{16}$ | $\frac{11}{16}$ | $\frac{10}{16}$ | $\frac{11}{16}$ | $\frac{8}{16}$ | $\frac{9}{16} \times 6 \times 5$ | $6\frac{1}{2}$ | $3\frac{1}{2}$ | 4 | 1500 and under 2000 |
| $\frac{18}{16}$ | $\frac{12}{16}$ | $\frac{12}{16}$ | $\frac{11}{16}$ | $\frac{12}{16}$ | $\frac{8}{16}$ | $\frac{10}{16} \times 6\frac{1}{2} \times 5\frac{1}{2}$ | $7\frac{1}{4}$ | $3\frac{3}{4}$ | 4 | 2000 and under 2500 |
| $\frac{19}{16}$ | $\frac{12}{16}$ | $\frac{12}{16}$ | $\frac{11}{16}$ | $\frac{12}{16}$ | $\frac{8}{16}$ | $\frac{10}{16} \times 6\frac{1}{2} \times 5\frac{1}{2}$ | $7\frac{3}{4}$ | 4 | 4 | 2500 and under 3000 |
| $\frac{20}{16}$ | $\frac{13}{16}$ | $\frac{12}{16}$ | $\frac{11}{16}$ | $\frac{12}{16}$ | $\frac{9}{16}$ | $\frac{10}{16} \times 6\frac{1}{2} \times 5\frac{1}{2}$ | 8 | $4\frac{1}{2}$ | 4 | 3000 and under 3500 |

| | | | | | | | |
|---------------------------|----------------------|---|--|--|----|----|----|
| 1500 and under 2000 | 10 x 3 | If single frames be adopted the frame, for half the vessel's length of bilges, and rivetted through the may be increased to 23 inches in | $1\frac{1}{2} \times 5\frac{1}{2} \times 3\frac{1}{2}$ | $1\frac{5}{8} \times 4 \times 3\frac{1}{2}$ | 14 | 14 | 14 |
| 2000 and under 2500 | 12 x 3 | | $1\frac{1}{2} \times 6 \times 4$ | $1\frac{5}{8} \times 4\frac{1}{2} \times 3\frac{1}{2}$ | 14 | 14 | 14 |
| 2500 and under 3000 | 12 x 3 $\frac{1}{4}$ | | $1\frac{1}{2} \times 6\frac{1}{4} \times 4$ | $1\frac{1}{2} \times 4\frac{1}{2} \times 3\frac{1}{2}$ | 14 | 14 | 14 |
| 3000 and under 3500 | 12 x 3 $\frac{1}{2}$ | | $1\frac{1}{2} \times 6\frac{1}{2} \times 4$ | $1\frac{1}{2} \times 4\frac{1}{2} \times 3\frac{1}{2}$ | 14 | 14 | 14 |

MEM.—The Scantlings given in the above Table are intended for Ships the length of which, measured from the their depth of Hold, taken from the upper part of Floors to the top of the Upper Deck Beams. For ships which ex

| RIVETS. | of an Inch. | | |
|--------------------------|-----------------------------|---------|---------|
| | Diameter of Rivets required | | |
| | for | | |
| Thickness of Plates..... | 5 16 | 6 16 | 7 16 |

* Hollow or flat keel plates (*vide* Engravings, Fig. 6, 11, and 14.) and garboard strakes, and main sheer-strakes in breadth than as follows, viz.:—In ships under 500 tons, 2 ft.; in ships 500 and under 1000 tons, 2 ft. 6 in.; in upwards, 3 ft. When hollow or flat plate keels are adopted, their thickness should not be less than one and half the strake. For Keels of other Forms, see Section 2, and Engraving, Fig. 7, 8, 9, 10, and 13.

§ FLOOR PLATES.—The floor plates to be in depth at middle line according to the following rule, viz.:—To measured from the top of keel to the top of upper or spar deck beams amidships, add the extreme breadth of vessel that sum, in inches, to be the depth of the floor plates at middle line; their thickness to be as given in Table; but vessel, for one-quarter of her length, they may be reduced in thickness one-sixteenth of an inch where the plate sixteen-enths, and two-sixteenths of an inch where the plates are ten-sixteenths and upwards. The floor plates to extend to a perpendicular height of twice the depth of floors amidships from upper side of keel at middle line, (*vide* b, Fig. 2) to be less moulded at their heads than the moulding of the frames. A floor plate to be fitted and rivetted to even worked across the middle line, except where centre through plates are adopted, so as to unite the sides of the vessel other. Watercourses are to be formed through all the floor plates on each side of middle line, so as to allow the pumps freely. (*Vide* Fig. 5 to 14.)

† PLATING.—No plates to be less in length than five spaces of frames (*vide* Fig. 2), except the fore and after hull outside plating in adjoining strakes to be nearer each other than two spaces of frames (*vide* Fig. 2). In vessels un plating may be reduced from the thickness shewn in Table, one-sixteenth of an inch forward and aft, for a distance quarter of the length of the vessel from each end, below the upper edge of main sheerstrake, down to a perpendicular upper side of keel of three-fifths the internal depth of hold, including the height of the spar deck in spar deck ships 1200 tons and upwards, a reduction of two-sixteenths will be allowed; the plates next abaft and next afore the quarter vessel to be of an intermediate or graduated thickness, between that required amidships and the reduction allowed screw propelled vessels, however, no reduction is to be made in the plating at the after end, below the lower part of

BUTT STRAPS.—All plates butts to be well fitted, and secured to the frames and to each other; the butts to be plating or otherwise, and to be united by butt straps, of not less than the same thickness as the plates breadth for riveting, as described hereafter, and to be fitted with the fibre of the iron in the same direction as the to which they are rivetted; the space between the plating and the frames to have solid filling or lining pieces, of length, and of the same breadth as the frames. It is recommended that in all cases the sheerstrake be an outside admit of the butt straps or lining pieces being extended, in one piece, from the fore-side of the frame next afore to side of the frame next abaft the butts (*vide* g, Fig. 2), or to admit of doubling the sheerstrake where it may be required (*vide* p, *vide* see foot indentations).

‡ BEAMS.—Beam plates to be in depth one-quarter of an inch for every foot in length of the midship beams, and one-sixteenth of an inch for every inch in depth of the said beams, and to be made of H iron, T bull iron, double angle irons rivetted on upper edge; the two sides of each of these angle irons to be not less in breadth the depth of beam plate, and to be in thickness one-sixteenth of an inch for every inch of the two sides of the beams may be composed of any other approved form of beam iron equal in strength. Where beams below the upper (including orlop beams) have no deck laid upon them, the angle irons on their upper edges are required to be of the angle iron of the reverse frames. All beams to be well and efficiently connected or rivetted to the frames, with butt plates; each arm of knee plates at ends of beams not to be less in length than twice and a half the depth of beams equal to the beams. The beams to be placed over each other, and pillared where practicable.

§ STRINGER AND TIE-PLATES.—All vessels to have stringer plates (of the thickness given in Table), upon top of beams. Those upon the ends of upper deck beams in vessels with two decks or tiers of beams, and on ends of

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RULES FOR THE BUILDING OF IRON SHIPS.

The scantlings given in Table G are intended for ships, the length of which, measured from the fore part of the stem to the after part of the stern-post, on the range of the upper deck, does not exceed seven times their breadth, or ten times their depth of hold, taken from the upper part of floors to the top of the upper deck beams. For ships which exceed in length ten times their depth, see Sec. 16.

1. The whole of the iron to be of good malleable quality, to be capable of bearing a longitudinal strain of twenty tons per square inch, and all plate, beam, and angle iron *to be legibly stamped in two places* with the manufacturer's trade mark, or his name and the place where made, which is also to be stated in the report of survey. The workmanship to be well executed, and submitted to the closest inspection before coating or painting: any brittle or inferior article to be rejected. (It is not intended to prevent the coating of the plates *inside* in the way of the frames.)

2. The keel, stem, stern, and propeller posts are to be either scarphed or welded together, and to be in size according to Table G; if scarphed, the length of scarphs to be eight times the thickness given in the table for keels; and the stern posts and after end of keel, for screw propelled vessels, to be double the thickness of, or twice the sectional area of, the adjoining length of keel (but the siding in no case to be less than the thickness of the keel given in Table G), and to be tapered fair into the adjoining length of keel. Where the garboard strakes are thicker than required by the rules, and extend to the bottom of the keel, the thickness of the keel may be proportionably reduced, but such reduction not to exceed one-third of the requisitions of the Rule. Where the keel and keelsons are made of several thicknesses of plates (*vide m* Fig. 1 and 2, and Fig. 7, 8, 9, 10, and 13), the plates that form the keel to be in thickness, taken together, the same as is required for a solid keel, as per Table G; and the butts of the several plates of which the keel is formed to be carefully shifted from each other, and from the butts of the garboard strakes, which in all cases must also be shifted, so as not to be opposite, or nearer to each other than two spaces of frames. For thickness and breadth of hollow or flat plate keels (*vide* Fig. 6, 11, and 14), see foot note of Table G.

3. The frames to be of the dimensions set forth in Table G ; to be in as great lengths as possible, fitted close on to the upper edge of the keel, and in all cases to extend to the gunwale ; and when butted on the keel (*vide a*, Fig. 1, and Fig. 3, 4, 5, and 12) (except when double frames, or centre through plate keels, are adopted) and wherever elsewhere butted, to have not less than four feet lengths of corresponding angle iron fitted back to back to cover and support the butts and receive the plating. If welded together, the welds to be perfect, with not less than four feet shifts.

If single frames be adopted, the space from centre to centre is not to exceed twenty-one inches all fore and aft ; but provided an additional frame, for half the vessel's length amidships, be fitted at opposite sides of each floor plate, across the keel, and extended to upper part of bilges and rivetted through floor plates and main frames, also through the outside plating as required for main frames, the space may be increased to twenty-three inches in Ships under 1,000 tons, and to twenty-four inches in Ships of 1,000 tons and upwards.

4. The floor plates to be in depth at middle line according to the following rule, viz. :—To the vessel's depth, measured from the top of keel to the top of upper or spar deck beams amidship, add the extreme breadth of the vessel ; two-fifths of that sum *in inches*, to be the depth of the floor plates at middle line ; their thickness to be as given in Table G ; but at each end of the vessel, for one quarter of her length, they may be reduced in thickness one-sixteenth of an inch where the plates are less than ten-sixteenths, and two-sixteenths of an inch where the plates are ten-sixteenths and upwards. The floor plates to extend up the bilges to a perpendicular height of twice the depth of floors amidship from upper side of keel at middle line (*vide b*, Fig. 1 and 2), and not to be less moulded at their heads than the moulding of the frames. A floor plate to be fitted and rivetted to every frame, and to be worked across the middle line (except when centre through plates are adopted), so as to unite the sides of the vessel efficiently to each other. Watercourses are to be formed through all the floor plates on each side of middle line, so as to allow water to reach the pumps freely (*vide c*, Fig. 1, and Fig. 5 to 14.)

5. Reversed angle iron on frames to be in size as per Table G. All vessels, of whatever size, to have reversed angle iron rivetted to every frame and floor plate across the middle line to the height of

upper part of bilges (*vide d*, Fig. 1), and to have double reversed angle iron in way of all keelsons and stringers in hold (*vide e*, Fig. 1 and 2); and in addition, all vessels of 300 tons and upwards to have reversed angle iron extended from bilges to the upper deck beam stringer on *alternate* frames (*vide f*, Fig. 2); and vessels of 800 tons and upwards to have reversed angle iron extended on *every* frame from bilges to above lower deck or hold beam stringer angle iron if the vessel has two decks or tiers of beams (*vide g*, Fig. 1), and to above the height of middle deck beam stringer angle iron if the vessel has three decks or tiers of beams (*vide h*, Fig. 1 and 2). The rivets for securing the reversed angle iron to the frames and floor plates to be in diameter equal to those specified in the Table for the outside plating, and not to exceed eight times their own diameter apart. Butts of reversed angle iron to be secured with butt straps.

6. The middle line keelson, if of single plate, and standing above the floor plates, to be of the same thickness as the garboard strakes, and to be two-thirds of the depth of floor plates, well fitted and rivetted thereto; and an angle iron of the size as per Table G, to be fitted on each side, both on the top and the bottom, extending all fore and aft; the lower angle irons to be rivetted to the double reversed angle irons on the top of floors (*vide i*, Fig. 1). If a box keelson be adopted, it is to be formed with a foundation plate, the plating to be of the thickness as per Table G, the depth not to be less than two-thirds of the depth of floor plates, and the breadth of the box two-thirds its depth (*vide k*, Fig. 1, and Fig. 4).

If an intercostal middle line keelson be adopted, it is to be of the same thickness as the floor plates, and rivetted to vertical angle iron on all floor plates at each end, the plates to extend from upper edge of keel to above the upper edge of floor plates, sufficiently high to be rivetted to bulb iron bars, of the same strength as the beams (*vide* Fig. 12), or to deeper bulb iron bars let down (*vide* Fig. 5 and 14), or bars of other form, but of equal strength, between double angle irons, of the dimensions given in Table G, extending all fore and aft, and the said double angle irons of keelson are to be rivetted to double angle irons on top of all floor plates.

Where flat plate keels are used, the intercostal keelson plates and centre through plates to be fitted close down on and connected to the keel by double angle irons of the dimensions given in Table G, rivetted all fore and aft to the keel and keelson (*vide* Fig. 14, 11, and 6.)

If the middle line keelson be formed of a centre through plate, extending from the lower edge of the keel to the top of the floors, it must not be less in thickness than that required in Table G for intercostal keelsons. To strengthen the floor plates transversely at their intersection at the middle line, in addition to the double vertical angle iron rivetted to their ends and to the centre plate keelson, there is to be a flat keelson plate, of the same thickness as the garboard strakes, and not less than three-fourths the breadth given in Table G, rivetted to double reverse angle irons on the upper edge of floors, and to two fore and aft angle irons on the upper edge of the centre through plate of the keelson (*vide m*, Fig. 1 and 2, and Fig. 7). But should the centre through plate keelson be extended up above the upper edge of the floors, then it is to be rivetted by two fore and aft angle irons, of the size as per Table G, to two flat plates, one on each side of the middle line, to be well rivetted to the double reverse angle irons on the upper edge of the floors (*vide* Fig. 8 and 13). In all cases the centre plate keelson to be extended to the stem and stern post, and connected thereto where practicable.

7. The bilge keelsons to be fitted and secured in an efficient manner, and to extend all fore and aft, and placed at lower turn of bilges, according to the form of the bottom (*vide n*, Fig. 1 and 2). In ships of 1,000 tons and upwards, an intercostal keelson to be fitted on each side, as far forward and aft as practicable, and to be placed about midway between the middle line keelson, and the bilge keelson, with double angle iron rivetted on the top of floor plates (*vide o*, Fig. 1 and 2). All vessels of 500 tons and upwards to have fitted between the bilge keelsons and the hold beams, at the upper part of the turn of bilge, strong angle irons, as stringers, extending all fore and aft, rivetted back to back and to the reversed irons on the frames, the size of them not to be less than those used for the middle line keelson (*vide p*, Fig. 1 and 2).

In all cases the middle line, side, and bilge keelsons, and *where practicable*, the stringers are to be carried fore and aft, without being cut off at the bulkheads, the latter being made water-tight around them; and where such parts of the ship are necessarily separated, the longitudinal strength to be efficiently maintained to the satisfaction of the Surveyor.

8. No plates to be less in length than five spaces of frames (*vide* Fig. 2), except the fore and after hoods. No butts of outside

plating in adjoining strakes, to be nearer each other than two spaces of frames (*vide* Fig. 2). In vessels under 1200 tons, the plating may be reduced from the thickness shown in Table G, one-sixteenth of an inch forward and aft, for a distance not exceeding one quarter of the length of the vessel from each end, below the upper edge of main sheerstrake, down to a perpendicular height from upper side of keel of three-fifths the internal depth of hold; and in ships of 1200 tons and upwards, a reduction of two-sixteenths will be allowed; the plates next abaft and next afore the quarter length of the vessel, to be of an intermediate or graduated thickness, between that required in midship and the reduction allowed at the ends. In screw-propelled vessels, however, no reduction is to be made in the plating at the after end, below the lower part of the rudder trunk.

All plates are to be well fitted, and secured to the frames and to each other; the butts to be closely fitted by planing or otherwise, and to be united by butt straps, of not less than the same thickness as the plates, and of sufficient breadth for rivetting, as described hereafter, and to be fitted with the fibre of the iron in the same direction as the fibre of the plates to which they are rivetted; the space between the plating and the frames to have solid filling or lining pieces, closely fitted in one length, and of the same breadth as the frames.

It is recommended that in all cases the sheerstrake be an outside strake, so as to admit of the butt straps or lining pieces being extended, in one piece, from the foreside of the frame next afore the butts to the aftside of the frame next abaft the butts (*vide* q, Fig. 2), or to admit of doubling the sheerstrake where it may be required.—*For breadth of sheerstrake see footnote in Table G.*

9. In raised quarter-decks, a reduction of one-fifth from the thickness required by the Table G for such parts in the range of the upper deck in ships with two decks will be allowed in the outside plating, beams, stringer plates upon beams, angle iron on stringer plates, and flat of deck. *The measurement of raised quarter decks is to be included in the gross tonnage for regulating all scantlings.*

In full poops and top-gallant forecastles a reduction of one-fourth from the dimensions required by the Table G for such parts in the range of the upper deck in ships with two decks will be allowed in the outside plating, beams, stringer plates upon beams, angle iron on stringer plates, and flat of deck, but in no case need the outside plating exceed six-sixteenths in thickness. These reductions will

not be allowed where the united lengths of poop and forecastle exceed three-fifths of the entire length of the upper deck. In such vessels the gross tonnage below the upper deck is to regulate all scantlings below this deck, except the scantlings of the keelsons and their number, the size of main piece of rudder, and also the requirements as to double rivetting, which are to be regulated by the gross Register tonnage, including that of the engine space in steam vessels. All frames are to extend to the stringer plates of poop and forecastle.

Where the poop or forecastle is constructed in a rounded form at the gunwale, the beams may be of plain angle iron, not less in dimensions than the sizes required in Table G for the main frames; a beam to be properly rivetted to every alternate main frame, with a scarp not less than four feet in length. The breast beams are to be double, and the rounded gunwale is to be plated and properly constructed in all respects to the satisfaction of the Surveyor.

In vessels with three decks (viz., upper, middle, and lower deck), a reduction of one-sixth from the dimensions given for such parts in the range of upper deck in ships with two decks will be allowed in the scantling of beams, flat of deck, and plating, *but not in the dimensions of sheerstrake.*

In vessels having three decks or tiers of beams, where the space under the upper deck is to be used only for the accommodation of crew and passengers, or to enclose the engine openings of steam vessels, the gross tonnage below the middle or tonnage deck, is to regulate all scantlings below this deck, but the total gross tonnage is to regulate the scantlings of the keelsons and their number, the size of main piece of rudder, and the requirements for double rivetting. The total depth of hold in spar decked ships must not exceed thirteen-sixteenths, nor be less than twelve-sixteenths of the ship's extreme breadth. In spar decks a reduction of one-fourth from the dimensions required by the Table G, for such parts in the range of the upper deck in ships with two decks, will be allowed in the dimensions of all beams and stringers, and thickness of plating, and flat of deck; but all frames are to extend to the stringer plates of spar deck.

Deckhouses or other erections are allowed on spar decks, but only to the extent of one-tenth of the total superficial area of the spar deck, and are not to exceed seven feet in height. They are not to be placed nearer to either of the ends than one-fifth of the entire length of the vessel.

Vessels to which this rule applies, as regards an entire spar deck, will be noted in the Register Book thus:—"Spar decked."

The total tonnage of the ship is to regulate the equipment, except the anchors and chains for Steam-vessels. (*See* Section 81.)

10. Beam plates to be in depth one-quarter of an inch for every foot in length of the midship beams, and to be in thickness one-sixteenth of an inch for every inch in depth of the said beams, and to be made of H iron, T bulb iron, or bulb plate with double angle irons rivetted on upper edge; the two sides of each of these angle irons to be not less in breadth than three-fourths the depth of beam plate, and to be in thickness one-sixteenth of an inch for every inch of the two sides of the angle iron; or the beams may be composed of any other approved form of beam iron, equal in strength. Where beams below the upper or middle deck (including orlop beams) have no deck laid upon them, the angle irons on their upper edges are required to be of the dimensions of the angle iron of the reverse frames. All beams to be well and efficiently connected or rivetted to the frames, with bracket ends or knee plates; each arm of knee plates at ends of beams not to be less in length than twice and half the depth of beams, and to be in thickness equal to the beams. The beams to be placed over each other, and pillared where practicable.

Upper deck beams in vessels with one or two tiers of beams, and the upper (or spar deck) and middle deck beams in vessels with three tiers of beams, to be fastened to alternate frames.

Vessels of 12 feet and under 13 feet depth of hold, or where the gross register tonnage exceeds 200 tons, shall be required to have as many hold beams as may be practicable or convenient, fastened to at least, every eighth frame. Vessels not being of a depth to require hold beams are to have a double angle iron stringer rivetted to reverse frames extending all fore and aft about midway between bilge keelson and deck beams (*vide r*, Fig. 1.)

Vessels of 13 feet depth and under 15 feet, to have hold beams fastened to every fourth frame.

Vessels of 15 feet depth and under 18 feet, to have hold or lower deck beams fastened to every second and fourth frame, alternately.

Vessels of 18 feet depth and above, to have hold or lower deck beams fastened to every alternate frame, and the same number of middle deck beams, where such are required.

All vessels having two decks (*viz.*, upper and lower deck), and

exceeding 24 feet in depth from the top of floor plates to the upper side of upper deck beams, and vessels with three decks (viz., upper, middle, and lower deck), and exceeding 24 feet in depth to the upper side of middle deck beams, and where the depth from under side of lower deck beams exceeds 15 feet, such ships to have orlop beams fastened to every sixth frame; also to have stringer plates and angle iron on their ends, all fore and aft, *equal in strength* to the requirement at Section No. 15; but, in the case of flush deck ships, a depth of 25 feet will be allowed, provided the lower hold does not exceed 16 feet in depth from the under side of lower deck beams. Should a house be constructed on such flush deck ship for lodging crew or for store-room, the same not to extend within 10 feet of the stern-post.

When the spaces between beams exceed two spaces of frames, a knee or bracket plate is to be rivetted to alternate frames and to the stringer plate at underside.

For the arrangement of beams the depth of hold is to be measured amidship from the top of the floor plates to the top of the upper deck beams in vessels with two decks, and to the top of the middle deck beams in vessels with three decks.

Where a deviation from the foregoing Rules as applying to beams takes place in way of engine-rooms or hatchways, or where no deck is intended to be laid, and the above-named spaces would materially interfere with the stowage of cargo, and where partial or entire bulkheads with horizontal stringers between them, or larger beams are substituted for ordinary beams in wider spaces, a sketch with all particulars must be submitted through the resident surveyor, for the Committee's consideration. The middle deck to be a perfect deck laid and caulked.

11. The rivets to be of the best quality, and to be in diameter as per Table G; the rivet holes to be regularly and equally spaced and carefully punched opposite each other from the faying surfaces in the laps and lining pieces or butt straps, and to be countersunk all through the outer plating (*vide* Fig. 16); the rivets not to be nearer to the butts or edges of the plating, lining pieces to butts, or of any angle iron, than a space not less than their own diameter, and not to be further apart from each other than four times their diameter, or nearer than three times their diameter, and to be spaced through the frames and outside plating, and in reversed angle iron, a distance equal to eight times their diameter apart. When rivetted up they are completely to fill the holes, and their points or outer ends

are to be round or convex (*vide* Fig. 16), and not to be below the surface of the plating through which they are rivetted. All vessels to have all edges or horizontal joints of outside plating double rivetted from the keel to the height of upper part of bilges (*vide* d, Fig. 1), all fore and aft; but vessels of 700 tons and above, intended for the highest grade, are to have all edges or horizontal joints of outside plating double rivetted *throughout* (*vide* Fig. 2.) The stem, stern post, keel, edges of garboard strakes and sheerstrakes, and butts of outside plating, and butts of floor plates, breasthooks, transoms, and plates of beams, also butts of keelsons, stringers, shelf-plates, and all longitudinal ties, to be double rivetted in all vessels. The overlaps of plating, where double rivetting is required, not to be less than five and a half times the diameter of the rivets (*vide* Fig. 16); and where single rivetting is admitted, to be not less than three and a quarter times the diameter of the rivets. If double rivetting be adopted where single rivetting is allowed by the Rules, the diameter of the rivets may be reduced one-sixteenth of an inch below that prescribed by the Rules, provided that in no case the diameter be less than five-eighths of an inch. The butts and edges of outside plating to be truly fitted, carefully caulked, and made water-tight.

12. Steamers, in addition to the engine room bulkheads, to have two water-tight bulkheads, built at a reasonable distance from the ends, to extend from the keel and outside plating to the upper deck in vessels with two decks, and to the middle deck in vessels with three decks (otherwise called "tonnage deck"); but the aftermost bulkhead will not be required to extend to this height if it be continued above the load water line, and be connected to a water-tight platform or deck of iron extending from its upper part entirely round the after part of the vessel, thus rendering the lower after body a water-tight compartment. The bulkhead is to be made water-tight where a screw shaft passes through. And in the construction of vessels propelled by machinery care must be taken that the engine and boiler bearers are properly constructed (and where they may interfere with the longitudinal strength of the vessel they must be extended a sufficient distance beyond the bulkheads of the engine and boiler space, to compensate for such interruption); and after the machinery and boilers are fitted, then as many hold or lower-deck beams are to be introduced as may be practicable; and knee or bracket plates are to be added and rivetted to the stringer-

plates, and to alternate frames which have no beams in the said space; and the vessels are to be otherwise made secure where necessary in the engine-room to the satisfaction of the Surveyors. In sailing ships the foremost or collision bulkhead only will be required. All plating of bulkheads to be of the thickness prescribed in Table G; and when fitted between two frames at each side of the vessel, to be strongly rivetted through them; or if attached only to one frame, then to have brackets or knee plates rivetted horizontally against the side plating of the vessel and to the bulkheads, foreside and afterside alternately, near the middle of the outside plates, and to be strongly rivetted thereto. Lining pieces between these frames and outside plating in way of bulkheads, are to be plates extending in one piece from the foreside of the frame afore, to the aftside of the frame abaft the bulkhead frames. The bulkheads to be supported vertically by angle irons (of the dimensions given in Table G) not exceeding two feet six inches apart; and to be efficiently connected and rivetted together and to the corresponding floors, beams of the several decks, and the frames. All such bulkheads to be caulked and made thoroughly water-tight. Where a pump is not fitted in each compartment, a sluice, cock, or valve is to be fitted at the limbers on each side of middle line, at each water-tight bulkhead, so as to allow water to be shut off, or to reach the pumps when required; the same to be worked from the deck above.

DOUBLE BOTTOMS.—To entitle a Vessel to be noted in the Register Book as having a "*Double Bottom*," the inner or second bottom must be efficiently constructed, with the plating carried forward to the fore bulkhead, as usually fitted, and to an equal distance from the after end of the ship; the plating not to be less in thickness than that given in Table G for plating of bulkheads, excepting the flange plate, which must be one-sixteenth thicker. The double bottom must be efficiently connected to the outside plating and frames of the main body of the ship. The butts and edges may be single rivetted. "Man holes" must be constructed, or provision made for the removal of a portion of the plates so as to enable the inner surface of outside plating, the frames, floors, keelsons, and rivets to be thoroughly examined, and coated when required. The upper side of the plating must be protected with wood planking as ceiling.

Should a smaller portion of the ship be constructed as above, such ship may be marked "*Part Double Bottom*," provided such portions extend to at least one-half of the length.

13. The wood ceiling or lining is not to be less than $1\frac{1}{4}$ inch, nor more than three inches in thickness in any case, and is to be so fastened to the reversed angle irons or frames that it may be easily removed for survey and painting.

14. The flat of upper deck to be fastened by screw bolts from the upper side, with nuts at the under side of the angle iron of the beams; where the planks exceed six inches in width there must be two bolts in each plank in every beam, one of which may be a short screw bolt, provided the planks do not exceed eight inches in width, in which case both bolts must be put through. The waterways, if of wood, to be fastened with screw bolts with nuts at under side of stringer plates.

15. All vessels to have stringer plates (of the thickness given in Table G) upon the ends of each tier of beams. Those upon the ends of upper deck beams in vessels with two decks or tiers of beams, and on ends of middle deck beams in vessels with three decks or tiers of beam, to be in width one inch for every seven feet of the vessel's entire length, for half her length amidship, and from thence to the ends of the vessel they may be gradually reduced to three-fourths of the width amidship—in no case, however, is the width to be less than eighteen inches amidship. The stringer plates are to be fitted home and rivetted to the outside plating at all upper decks, and at the middle deck in vessels having three decks, with angle iron of the dimensions given in Table G (*vide s*, Fig. 1); the middle deck stringer plate to have an additional angle iron extending all fore and aft inside of the frames, rivetted to the reverse angle iron on the frames, and to the stringer plate (*vide t*, Fig. 1 and 2). Stringer plates on ends of beams below the upper deck in vessels with two decks, or below middle deck in vessels with three decks, may be reduced in width to three-fourths the midship breadth above named, this breadth is to be extended all fore and aft, and to have an angle iron of the dimensions given in Table G, extending all fore and aft, rivetted to the reverse angle iron on the frames, and to the stringer plates (*vide u*, Fig. 1 and 2). In cases where no deck is laid, and the width of stringer plate on ends of hold beams is objected to, it may be reduced, provided such reduction be fully compensated for. The objectionable practice of cutting through the stringer plates for the admission of wood rough-tree stanchions will not be allowed.

All vessels to have tie-plates ranging all fore and aft upon each side of the hatchways *on each tier of beams*, and in addition thereto.

the beams of the upper and middle decks in three-decked or spar-decked ships, and of the upper deck in vessels of one or two decks, must have the tie-plates fitted from side to side diagonally (*as shewn in Fig. 15*), whenever the arrangements of the deck will admit of them; the tie-plates are to be in width once and a half the depth of beams, and of the thickness required for stringer plates, and to be well rivetted to each other, and to the beams, deck hooks, and transoms; and all butts to be properly shifted. Upon hold beams where no deck is to be laid, or where tie-plates would interfere with stowage of cargo, an angle iron of the dimensions given in Table G for angle iron on beam stringers, placed at middle line, extending fore and aft wherever practicable, and well rivetted to all beams, deck hooks, and transoms, will be admitted in lieu thereof.

All hatchways and the mast-holes of sailing ships are to be properly framed to receive half beams where required, and the latter to have mast partners at each tier of beams (except at orlop beams) the plating of which is not to be less in thickness than is required for stringer plates, and the united breadths of the plates not to be less than three times the diameter of the masts. The said plates are to be well rivetted to each other, and to the beams, and angle iron carlings; and at the decks where masts are to be wedged, an angle iron of the dimensions required for the main frames of the ship is to be properly fitted and rivetted to the plates round the mast-holes. The mast-holes of steam vessels must be properly secured to the satisfaction of the surveyors.

16. In the following cases additional longitudinal strength beyond that stated in Table G will be required, viz. :—

Ships above 10, and not exceeding 11 depths in length, to have the main sheerstrake increased in thickness one-sixteenth of an inch amidships, for three-fourths of the length of ship; or to have a doubling strake not less than nine inches broad, for the same distance amidships.

Ships above 11, and not exceeding 12 depths in length, to have the main sheerstrake increased in thickness two-sixteenths of an inch amidships, for three-fourths the length of ship, or to have a doubling strake not less than twelve inches broad, for the same distance amidships.

Ships above 12, and not exceeding 13 depths in length, to have the main sheerstrake increased in thickness two-sixteenths of an inch amidships, for three-fourths the length of ship, or to have a doubling strake not less than eighteen inches broad, for the same

distance amidships; and the stringer plate upon ends of upper deck beams, in vessels with two decks, or on ends of middle deck beams, in vessels with three decks, is to be increased two-sixteenths of an inch in thickness for half the ship's length amidships, or to be proportionately increased in width for the same distance, and to have a bulb plate of the dimensions required for beam plates, placed between and rivetted to the double angle iron keelson, at lower part of bilges (at *n*, Fig. 1), for half the length of the ship amidships.

In all the above cases, the doubling plate is not to be of less thickness than the strake next below the sheerstrake, and fitted at the upper edge of the sheerstrake.

In ships above 13, and not exceeding 14 depths in length, the main sheerstrake to be double its entire breadth for three-fourths the length of ship amidships, the doubling is not to be of less thickness than the strake next below the sheerstrake and fitted upon the edge of the same, and to extend in one or two breadths of plating to the upper edge of sheerstrake. The stringer plate on ends of beams and the bulb plate between the angle irons at bilges to be as is required in the preceding case.

In cases of ships which exceed 14 depths in length, the builders are to submit to the Committee, through the resident Surveyor, their plans for giving the vessel sufficient additional strength longitudinally. The depth for the foregoing purpose in spar-decked ships is to be taken from the under side of the "tonnage" or middle deck to the top of the floor plates.

17. The main piece of rudder to be in size according to Table G, of the best hammered iron, and the plating to be carefully stayed and rivetted.

18. Vessels intended for Classification to be surveyed as follows, viz. :—

1st. On the several parts of the frame, when in place, and before the plating is wrought.

2nd. On the plating during the progress of rivetting.

3rd. When the beams are in and fastened, and before the decks are laid.

4th. When the ship is complete, but before the plating is finally coated or cemented.

5th. And lastly, after the ship is launched and equipped.

For Equipments, see Sections 71, 72, 73, 74, 75, and 76, of Wood Ships.

SHIPS NOT BUILT UNDER SURVEY.

19. In cases of ships not surveyed while building for which a character may be required, application must be made to the Committee in writing, who will direct a special examination to be made by two Surveyors of the Society (one of whom shall be an exclusive officer), for which purpose the vessel is to be placed on high blocks in a dry dock or upon ways; the hold to be cleared and proper stages made; the rivets and plating of keel, and flat of bottom thoroughly examined; the close ceiling in the hold to be removed, and coal bunkers of steam-vessels to be cleared; the whole of the frames, stringers, hooks, floor plates, keelsons, engine and boiler bearers, ends of beams, water-tight bulkheads, rivets, and inner surface of the plating exposed to view; all oxidation to be removed by being cut or beaten off the several parts above named, also from the outside plating, rivets, keel, stem, sternpost, and rudder, so as to completely lay bare all the surfaces of iron; the planksheers and waterways, if of wood, to be scraped bright; and when the vessel is so prepared, the Surveyors are to examine the condition and thickness of all the parts of iron above-named, also the condition of the planksheer, waterways, flat of decks and their fastenings; and send a detailed report thereon, and on the dimensions and quality of the materials and workmanship to the Committee, who will then assign the vessel such character as the facts may appear to them to warrant, and define the periodical Surveys to which they shall respectively be subjected.

MEM.—The foregoing Rules have been framed for Iron Ships built with vertical frames and longitudinal plating. Parties desirous of constructing vessels varying from the rules, must submit their plans with specifications, for approval.

RULES FOR THE SURVEY OF IRON SHIPS CLASSED FOR PERIODS OF YEARS.

All vessels to be subject to occasional or annual survey when practicable, and every third year to be specially surveyed in dry dock or laid on blocks; with both surfaces of outside plating exposed;* and whenever the engines or the boilers of iron steam ships are taken out, the vessel shall be submitted to a particular and special survey.

CONTINUATION OF IRON SHIPS TO THE CHARACTER A.

20. If, on the termination of the period of original designation, or if at any subsequent period, not exceeding one-half the number of years assigned originally, or on Restoration, an owner shall wish to have his ship remain or be replaced on the letter A, he is to send a written notice thereof to the Secretary, and the Committee shall then direct a special survey, as follows, to be held by not less than two competent persons, to be appointed by the Committee, one of them to be a surveyor the exclusive servant of the Society.

SURVEY.

The vessel to be placed on high blocks, in a dry dock, or upon ways, and proper stages to be made, so that the rivets and plates of keel, and flat of bottom, may be thoroughly examined; the whole of the ceiling or lining inside to be entirely removed; coal bunkers of steam vessels to be cleared, so as to expose the whole of the frames, stringers, hooks, floor-plates, keelsons, engine and boiler bearers, ends of beams, water-tight bulkheads, rivets, and inner surface of the plating, to view; the hold to be cleared; all oxidation to be removed by being cut or beaten off the several parts above-named, also from the outside plating, rivets, keel, stem,

* As regards ships where the inner surface of the bottom plating is coated with cement or asphalt, instructions have been given to the Society's Surveyors in relaxation of this requirement. Ships which have undergone the above examination will be noted in the Register Book thus (t. s.); and if not submitted to such triennial Survey, will be liable to have their character suspended.

sternpost, and rudder, so as to completely lay bare all the surfaces of iron; the planksheers and waterways, if of wood, to be scraped bright: and when the vessel is so prepared, the Surveyors are to examine and report the condition and thickness of all the parts of iron above-named, also the condition of the planksheers, waterways, flat of decks and their fastenings; and upon the owner consenting to remove and replace with proper materials, equal in substance and quality to the original construction, such parts as may be found defective, or less than three-fourths of the required substance by Rule, such vessel, upon the repairs and efficiency being reported to the Committee, may be continued on the letter A for a term of years not exceeding one-half the number of years assigned originally, or on Restoration, subject to occasional or annual survey when practicable. The period of Continuation will, upon all occasions, commence from the time the ship may have gone off the letter A, without regard to the date when the survey for this purpose may be held.

RESTORATION OF IRON SHIPS TO THE CHARACTER A.

21. If, at any age of a vessel, an owner be desirous to have his ship Restored, such Restoration, on his application to the Committee, and consenting to the special survey hereinafter described, to be held by two Surveyors, one of whom shall be an exclusive servant of the Society, and performing the repairs thereby found requisite, will be granted for a period not exceeding two-thirds of the time originally assigned, the same to be calculated from the date of such survey.

Survey and Requisites for Restoration.

The vessel to be placed on high blocks, in a dry dock, or upon ways, and proper stages to be made, so that the rivets and plates of keel, and flat of bottom, may be thoroughly examined; the whole of the ceiling or lining inside to be entirely removed; coal-bunkers of steam vessels to be cleared, the boilers to be taken out, and also engines (unless it shall be shown by previous survey that the removal is unnecessary), so as to expose the whole of the frames, stringers, hooks, floor-plates, keelsons, engine and boiler bearers, ends of beams, water-tight bulkheads, rivets, and inner surface of

the plating, to view; the hold to be cleared; all oxidation to be removed by being cut or beaten off the several parts above-named, also from the outside plating, rivets, keel, stem, sternpost, and rudder, so as to completely lay bare all the surfaces of iron; the planksheers and waterways, if of wood, to be entirely removed, and also the flat of upper deck, except under special circumstances, to be sanctioned by the Committee in each case: and when the vessel is so prepared, the Surveyors are to examine and report the condition and thickness of all the parts of iron above-named, also the condition of the beams and their fastenings; and upon the owner consenting to remove such parts as may be found defective, or objected to, or less in thickness than hereinafter admitted for repairing such vessel, and replace them with proper materials equal in quality and substance to that required in the Table G for the nine years' grade in those originally classed 12 A, and equal in quality and substance to that required in the Table G for the six years' grade in vessels originally classed 9 A or 6 A, such vessel, upon the repairs and efficiency being reported to the Committee, may be restored to the letter A, for a term of years not exceeding two-thirds the number of years assigned originally, subject to occasional survey.

Iron ships which have been restored under the foregoing rule shall be entitled to Continuation thereon, subject to the same conditions of survey and examination as are prescribed for ships proposed to be Continued at the expiration of the period first assigned to them; but, in like manner, the term of such extended continuance to be limited to a period not exceeding one-half the number of years for which the ship may respectively have been restored, without reference to the period originally assigned to them.

22. On the expiration of the terms assigned to ships classed A, they will be liable to lapse (like ships built of wood).

23. One year will be added to the character of all ships of the A class built under a roof which shall project at each end beyond the length, and on each side beyond the breadth, a quantity equal to one-half the breadth of the vessel.

24. Vessels not surveyed while building will be classed A from year to year only, but for a period not exceeding Six Years.

IRON SHIPS ALREADY CLASSED A 1.

Iron ships built prior to the promulgation of the Rules will be allowed to remain in the Register Book classed A 1 from year to year, *subject to annual survey*, until the expiration of Six Years from their date of build, and then be examined to determine the period to which they may be entitled under the rules; and if, on such examination, it shall be found the ships are entitled to the 9 or 12 years' grade, it will be in the option of the owners either to adopt such period respectively, or continue the vessel A 1 from year to year, as above, until the expiration of the extended period; but if it shall be found that the term of years for which a vessel would have been entitled to remain on the A character has expired, she will be classed *Æ*, if entitled thereto, unless specially surveyed for Continuation or for Restoration.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

2, *White Lion Court, Cornhill,*
London, 1st July, 1865.

(No. 4.)

FORM OF THE REPORT OF ORIGINAL SURVEY.

No. — Survey held at — Date — 18— on the
— Master —

Tonnage under tonnage deck —

Ditto of poop — or spar deck —

Total tonnage —

Built at — When built — Launched —

By whom built — Owners —

Port belonging to — Destined voyage —

If Surveyed while building, Afloat, or in Dry Dock —

Length as per Section 39.

Length of Keel.

Extreme Breadth outside.

Depth of Hold.

Number of Decks —

(Depth from limber-strake to under side of lower deck beam — —)

| Feet. | Inches |
|-------|--------|
| | |
| | |
| | |

| SCANTLINGS OF TIMBER. | IN SHIP. | | | REQU'D PER RULE. | | |
|-------------------------------|----------|---------|-------|------------------|---------|-------|
| | Moulded. | | | Moulded. | | |
| | Sided. | Middle. | Ends. | Sided. | Middle. | Ends. |
| Timber and Space. | | | | | | |
| Floors. | | | | | | |
| 1st Foothooks. | | | | | | |
| 2nd Ditto. | | | | | | |
| 3rd Ditto. | | | | | | |
| Top Timbers. | | | | | | |
| Deck Beams No... { Average | | | | | | |
| Space. | | | | | | |
| Deck Beams, length amidships. | | | | | | |
| Hold Beams No... { Average | | | | | | |
| Space. | | | | | | |
| Hold Beams, length amidships. | | | | | | |
| Keel. | | | | | | |
| Scarphs of Ditto. | | | | | | |
| Keelsons. | | | | | | |
| Scarphs of Ditto. | | | | | | |

| OUTSIDE PLANK. | INCHES. | | Dimensions of Ship per Register. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|----------------------|----------|--------------------|--|----------|--------|---------------|---------|--|--|----------|--------------------|--------------------|--|--|------------------|--|--|---------------------|--|--|----------------------|--|--|------------------|--|--|--------------------|--|--|----------------------|--|--|---------------------|--|--|--------------------|--|--|
| | In Ship. | Required per Rule. | Length. | Breadth. | Depth. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Garboard Strakes.. | | | <table><tr><th>INSIDE PLANK.</th><th colspan="2">INCHES.</th></tr><tr><td></td><th>In Ship.</th><th>Required per Rule.</th></tr><tr><td>Limber Strakes....</td><td></td><td></td></tr><tr><td>Bilge Planks....</td><td></td><td></td></tr><tr><td>Ceiling in Flat....</td><td></td><td></td></tr><tr><td>Ditto Bilge to Clamp</td><td></td><td></td></tr><tr><td>Hold Beam Clamps</td><td></td><td></td></tr><tr><td>Deck Beam ditto...</td><td></td><td></td></tr><tr><td>Ceiling 'twixt Decks</td><td></td><td></td></tr><tr><td>Hold Beam Shelves..</td><td></td><td></td></tr><tr><td>Deck Beam ditto...</td><td></td><td></td></tr></table> | | | INSIDE PLANK. | INCHES. | | | In Ship. | Required per Rule. | Limber Strakes.... | | | Bilge Planks.... | | | Ceiling in Flat.... | | | Ditto Bilge to Clamp | | | Hold Beam Clamps | | | Deck Beam ditto... | | | Ceiling 'twixt Decks | | | Hold Beam Shelves.. | | | Deck Beam ditto... | | |
| INSIDE PLANK. | INCHES. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | In Ship. | Required per Rule. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Limber Strakes.... | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Bilge Planks.... | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Ceiling in Flat.... | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Ditto Bilge to Clamp | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Hold Beam Clamps | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Deck Beam ditto... | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Ceiling 'twixt Decks | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Hold Beam Shelves.. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Deck Beam ditto... | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Garboard to Bilge.. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Bilge Planks..... | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Bilge to Wales.... | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Wales | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Topsides | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Sheerstrakes | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Planksheers..... | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Waterways — | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Upper Deck | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Lower Deck | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Do. faying surface | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| against Timbers. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Upper Deck..... | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

SIZE OF BOLTS IN FASTENINGS, DISTINGUISHING WHETHER COPPER, YELLOW METAL, OR IRON; ALSO OF TREENAILS.

| | Copper or Y. M. in Ship. | Iron in Ship. | Inches required per Rule. |
|--|--------------------------------|------------------|---------------------------------|
| Heel-Knee and Deadwood abaft..... | | | |
| Scarphs of Keel, No..... | | | |
| Keelson Bolts through Keel at each floor | | | |
| Bolts through Heels of Timbers against | | | |
| Deadwood | | | |
| Transoms and Throats of Hooks..... | | | |
| Arms of Hooks | | | |
| Through Bilge and Limber Strakes.... | | | |
| Thickstuff over Double Floors..... | | | |
| Butt End Bolts | | | |
| Pintles of the Rudder..... | | | |
| Hold Beam { Waterway..... | | | |
| Bolts in { Knees..... | | | |
| { Shelf or Clamp..... | | | |
| Deck Beam { Waterway..... | | | |
| Bolts in { Knees..... | | | |
| { Shelf or Clamp..... | | | |
| Nails or Bolts in Flat of Deck..... | | | |
| Treenails..... Inches..... | | | |

TIMBERING.—The Space between the Floor Timbers and Lower Foothooks is ——— Inches. The Space between the Top Timbers is ——— Inches.

The Floors consist of ———. The First Foothooks of ———.

The Second Foothooks of ———. The Third Foothooks and Top Timbers of ———.

The Shifts of the first and second Foothooks are not less than ———

[*N. B.*—When less than prescribed by the Rule, state how many.]

The rest of the Shifts of the Frame are ———.

The frame is ——— squared from the First Foothook Heads upwards, and ——— free from sap, and from thence downwards the Frame is ———

The ——— Frames are ——— bolted together to the Gunwale.

[*N. B.*—If not, state how bolted.]

The butts of the Timbers are ——— close together; their thickness not less than ——— of the entire mouldings at that place.

The frame is ——— chocked with ——— Butt at each end of the chock. The Main piece of Rudder is ——— Of Windlass is ———.

The Keel is ———. The Main Keelson is ——— and ——— free from all defects.

The Stem and Stern Post of ———. The Transoms, Knight Heads, Hawse Timbers, and Aprons, of ——— Deadwood, of ——— and are ——— free from all defects.

The Deck and Hold Beams of ———. The Breasthooks of ———. The Knees of ———.

PLANKING OUTSIDE.—From the Keel to the Height defined in Note to Table A, or ——— to the First Foothook Heads the Plank is ———.

From the above-named height to the Light Water Mark ———.

From the Light Water Mark to the Wales ———.

The Wales and Blackstrakes are ———. The Topsides and Sheerstrakes ———.

The Spirketting and Planksheers ———.

The Waterways { Upper Deck ———.
Lower Leek ———.

The Decks ———. State of ———.

The Shifts of the Planking are not less than ——— feet ——— inches. [*N. B.—If less than prescribed by the Rule, state whether general or partial, and if partial, in what part of the Ship.*]

The Planking is wrought ——— between, and without step-butting.

PLANKING INSIDE.—The Limber-strakes and Bilge strakes are ———. The Ceiling, Lower Hold, and between Decks ———. Shelf Pieces and Clamps ———.

FASTENINGS.—To hold Beams ———. Deck Beams ———.

Number of Breasthooks ——— Pointers ——— Crutches ———.

Butt End Bolts are of ——— in the Bottom ——— Bolts in each Butt End ——— through and clenched.

Bilge and Limber Strakes ——— bolted through and clenched. Treenails of ———. How made ———.

Thickstuff over Double Floors ——— bolted through and clenched. General Quality of Workmanship ———.

We certify that the above is a correct description of the several particulars therein given.

Builder's Signature ———. *Surveyor's Signature* ———.

Her Masts, Yards, &c., are in ——— condition, and sufficient in size and length.

SAILS.—Fore Sails.....Number

Fore Top Sails.....“

Fore Topmast Stay Sails.....“

Main Sails.....“

Main Top Sails, &c.....“

| CABLES, &c. | Fathoms. | Inches. | Tested to Tons. |
|--------------------------|----------|---------|-----------------|
| Chain..... | | | |
| Hempen Stream Cable..... | | | |
| Hawser..... | | | |
| Towlines..... | | | |
| Warp..... | | | |
| All of ——— quality..... | | | |

| ANCHORS. | Number. | Weight. | Tested to Tons. |
|-------------|---------|---------|-----------------|
| Bower..... | | | |
| Stream..... | | | |
| Kedge..... | | | |

Her Standing and Running Rigging ——— sufficient in size and ——— in quality.

She has ——— Long Boat and ———. The present state of the Windlass is ———; Capstan ———; Rudder ———; Pumps ———.

General Remarks, and Statement and Date of Repairs, if any.

Order for Special Survey ———. No. ———. Date ———.

Order for Ordinary Survey ———. No. ———. Date ———.

Dates of Surveys held while building, as per Section 35. { 1st. When the frame is completed ———.
2nd. When the Beams are put in, &c., ———.
3rd. { When completed, and before the plank be painted or payed ———.

Present condition of Caulking of Bottom ———, Deck ———, and Waterways ———.

If Sheathed, Doubled, Felted, or Coppered ———. When last done ———.

I am of opinion this Vessel should be classed ———.

The amount of the Fee.....£ : : is received by me..

Special...£ : :

Certificate £ : :

Committee's Minute ———, 18—.

Character assigned ———.

(No. 6.)

FORM OF REPORT OF ANNUAL SURVEY.

No. — Survey held at — Date — 18 — on
 the — Master — Tonnage — Built at — When
 built — By whom built — Owners — Port belonging
 to — Destined Voyage — If Surveyed Afloat or in Dry
 Dock —.

Last Survey, No. — Port of — Classed —.

The present condition of the

| | | |
|---------------------------|----------------------------|---------------------------|
| Decks..... | Treenails..... | Windlass and Capstan.... |
| Waterways..... | Breathooks and Stenson.. | Pumps..... |
| Comings..... | Transoms, Pointers, and | Boats..... |
| Upper Deck Beams and | Crutches..... | Masts, Yards, &c..... |
| Fastenings..... | Timbers of the Frame at | Sails..... |
| Lower Deck Beams and | the openings..... | Anchors, No. of..... |
| Fastenings..... | Ditto at other places..... | Cables..... |
| Planksheers..... | Keelsons..... | Hawsers and Warps..... |
| Sheerstrakes..... | Clamps and Shelves..... | Standing and Running |
| Topsides..... | Ceiling..... | Rigging..... |
| Wales..... | Rudder..... | Caulking of Bottom, Deck, |
| Plank (Bottom) & Counter. | Copper, when put on..... | and Waterways..... |

General Observations and Opinion,

Committee's Minute —, 18—.

Character assigned —. Certificate (if required.)

(No. 7.)

FORM OF CERTIFICATE OF CHARACTER.

LLOYD'S REGISTER OF BRITISH AND FOREIGN SHIPPING.

ESTABLISHED 1834.

No.



London, 18

No. 2, White Lion Court, Cornhill.

THESE ARE TO CERTIFY, That the
of Master, Tons, bound to
has been surveyed at by the Surveyors to this
Society, and reported to be, on the

and that she has been CLASSED and entered in the REGISTER BOOK of
this Society with the character

Witness my hand,

Charge.

Chairman.

Secretary.

(No. 163.)

[CIRCULAR TO SURVEYORS.]

(Lloyd's Register of British and Foreign Shipping.)

SIR,—

The attention of the Committee having been called to frequent cases of insufficiency of treenails, both as regards their make and quality, and likewise as to the manner in which they are driven; I am directed to request your especial attention to this important subject, and to express the Committee's desire that you will satisfy yourself that the treenails used in your district are straight, and not grain-cut or knotty, and that they are free from sap or other defect, and that they may be made circular by engine-turning or otherwise. Also that you will see that they are tightly driven, and that the ends are sawn off and not broken. You will likewise recommend the treenails to be properly caulked with good oakum outside, in preference to wedging them as is too commonly the practice.

It is the Committee's desire also, that in all cases in which iron masts are fitted in ships reported by you, you will describe distinctly, by sketch, or otherwise, how they are constructed.

I am, Sir,

Your obedient Servant,

GEORGE B. SEYFANG,

Secretary.

No. 2, White Lion Court, Cornhill, London, E. C.,
1st June, 1864.

(No. 167.)

(*Lloyd's Register of British and Foreign Shipping.*)

SHIPS WITH IRON FRAMES AND WOOD PLANKING.

NOTICE is hereby given, that in pursuance of a Resolution passed this day by the Committee, it has been determined :—

That in Ships with Iron Frames and Planking outside in two or more thicknesses, *the inner thicknesses* (if more than one), *provided they be entirely of teak*, may be fastened to the frame with galvanized iron bolts in ships claiming, in other respects, the 14 years' grade, under the Rules, sec. 46.

If the garboard strakes are in one thickness they may be of elm, but the 'thwartship bolts must be of copper or yellow metal, to entitle the ship to the advantage of such fastenings in classification.

In other respects the classing of such ships to be governed by the description of wood material which may be used for outside planking and other parts.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

No. 2, White Lion Court, Cornhill, London, E. C.,
11th August, 1864.

(No. 168.)

(Lloyd's Register of British and Foreign Shipping.)

SHIPS CLASSED A

WHOSE CHARACTERS EXPIRE ON THE 31ST DECEMBER, 1864.

NOTICE is hereby given, that in pursuance of a Resolution passed this day by the Committee of Lloyd's Register of British and Foreign Shipping:—

“All Ships classed A for a term of years, and which term will expire at the end of the year 1864, will, on the 31st December next, have the word “*expired*” inserted against their names in the Register Book, and if not re-surveyed in the interim, they will appear without character in the reprint of the Register Book in June next.”*

By order of the Committee,

GEORGE B. SEYFANG,

*Secretary.**No. 2, White Lion Court, Cornhill, London, E. C.,**3rd November, 1864.*

* Should the vessels return to this country prior to the 30th June next, they must be submitted to survey with a view to their being classed in accordance with the Rules.

(No. 169.)

(Lloyd's Register of British and Foreign Shipping.)

SHIPS CLASSED A IN RED, AND Æ, INCLUDING THOSE
WITH THE ASTERISK.

The Rules, Section 60 and 61, requiring that ships classed A in red, or Æ (including those with the Asterisk), shall be surveyed annually, or on their return from every foreign voyage :

NOTICE is hereby given, that in accordance with the above Rules, and in pursuance of a Resolution passed this day by the Committee, the Characters of Ships Classed A in red, or Æ (including those with the Asterisk), which shall not have been surveyed since the year 1862, will be omitted in reprinting the Register Book (in June next) for the year 1865-66.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

No. 2. White Lion Court, Cornhill, London, E. C.,

3rd November, 1864.

N. B.—In the case of Ships which it shall be made to appear, by letter addressed to the Secretary, have not been in any port in the United Kingdom since 1862, the above Resolution will not be applied.

(No. 170.)

(Lloyd's Register of British and Foreign Shipping.)

ANCHORS AND CABLES.

In the year 1863, the Committee passed a Resolution to the effect that on and after the 1st July, 1864, "all Anchors and Chains supplied to Ships claiming to be classed with the figure 1 in the Register Book of this Society, must be tested up to the Admiralty proof, at a machine *under the control and superintendence of some responsible public body so as to enable it to be recognized as a public machine,*" and their attention having been called, by recent proceedings in Parliament, to the fact that several Chain and Anchor Manufactures have applied to have their private testing machines licensed by the Board of Trade, under the Chain and Anchor Testing Act, and being desirous of obviating inconveniences to parties who may be led to suppose that the Act alluded to will induce the Committee to abrogate the foregoing Resolution,—

NOTICE is hereby given, that under a deep sense of the absolute necessity of requiring that the proving of Anchors and Chains should be conducted at a Public Machine—and not by Private Individuals or Firms,—the Committee will adhere strictly to the Resolution quoted above.

The following Public Chain and Anchor Testing Machines, approved and recognized by the Committee, are now in operation, viz:

LONDON.—Lloyd's Chain and Anchor Proving House, Poplar; Superintendent, Mr. Thomas M. Gladstone, C. E.

LIVERPOOL.—Mersey Docks and Harbour Boards Chain and Anchor Testing Machines; Superintendents, Mr. W. Macdonald and Mr. James Haslam.

TYNE.—Lloyd's Tyne Public Chain and Anchor Proving House (at Low Walker); Superintendent, Mr. Robert Burroil.

SUNDERLAND.—Sunderland Public Chain and Anchor Testing House; Superintendent, Mr. John Thompson.

TIPTON.—Tipton Proving Machine, erected by the Staffordshire

Public Chain and Anchor Testing Company (Limited); Superintendent, Mr. David Logan.

NETHERTON.—Netherton Proving Machine, erected by the Staffordshire Public Chain and Anchor Testing Company (Limited); Superintendent, Mr. Samuel Brittain.

JERSEY.—Jersey Mutual Insurance Company's Machine; Superintendent, Mr. George Ennes.

LLANELLY.—Llanelly Public Chain and Anchor Testing Machine, belonging to the Harbour Commissioners, Llanelly; Superintendent, Mr. Bowen.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

No. 2, White Lion Court, Cornhill, London, E. C.,

18th May, 1865.

MEM.—In cases where Ships have been supplied with Anchors and Cable; which have been tested at a *Public Machine*, the fact will be noted in the Register Book thus, (A. & C. P.), signifying that the Anchors and Chains have been so proved.

(No. 171.)

(Lloyd's Register of British and Foreign Shipping.)

IRON SHIPS.

In pursuance of Resolutions passed by the Committee, on the 25th May, the Rules for Iron Ships have been amended as under, viz. :—

SECTION 9.—In raised quarter decks, a reduction of one-fifth from the thickness required by the Table G, for such parts in the range of the upper deck in ships with two decks will be allowed in the outside plating, beams, stringer plates upon beams, angle iron on stringer plates, and flat of deck. *The measurement of raised quarter decks is to be included in the gross tonnage for regulating all scantlings.*

In full poops and top-gallant forecastles, a reduction of one-fourth from the dimensions required by the Table G, for such parts in the

range of the upper deck in ships with two decks, will be allowed in the outside plating, beams, stringer plates upon beams, angle iron on stringer plates, and flat of deck, but in no case need the outside plating exceed six-sixteenths in thickness. These reductions will not be allowed where the united lengths of poop and forecastle exceed three-fifths of the entire length of the upper deck. In such vessels the gross tonnage below the upper deck is to regulate all scantlings below this deck, except the scantlings of the keelsons and their number, the size of main piece of rudder, and also the requirements as to double rivetting, which are to be regulated by the gross Register tonnage, including that of the engine space in steam vessels. All frames are to extend to the stringer plates of poop and forecastle.

Where the poop or forecastle is constructed in a rounded form at the gunwale, the beams may be of plain angle iron, not less in dimensions than the sizes required in Table G for the main frames ; a beam to be properly rivetted to every alternate main frame, with a scarph not less than four feet in length. The breast beams are to be double, and the rounded gunwale is to be plated and properly constructed in all respects to the satisfaction of the Surveyor.

In vessels with three decks (viz., upper, middle, and lower deck), a reduction of one-sixth from the dimensions given for such parts in the range of upper deck in ships with two decks will be allowed in the scantling of beams, flat of deck, and plating, *but not in the dimensions of sheerstrake.*

In vessels having three decks or tiers of beams, where the space under the upper deck is to be used only for the accommodation of crew and passengers, or to enclose the engine openings of steam vessels, the gross tonnage below the middle or tonnage deck, is to regulate all scantlings below this deck, but the total gross tonnage is to regulate the scantlings of the keelsons and their number, the size of main piece of rudder, and the requirements for double rivetting. The total depth of hold in spar decked ships must not exceed thirteen-sixteenths, nor be less than twelve-sixteenths of the ship's extreme breadth. In spar decks a reduction of one-fourth from the dimensions required by the Table G, for such parts in the range of the upper deck in ships with two decks, will be allowed in the dimensions of all beams and stringers, and thickness of plating, and flat of deck ; but all frames are to extend to the stringer plates of spar deck.

Deckhouses or other erections are allowed on spar decks, but only to the extent of one-tenth of the total superficial area of the spar deck, and are not to exceed seven feet in height. They are not to be placed nearer to either of the ends than one-fifth of the entire length of the vessel.

Vessels to which this rule applies, as regards an entire spar deck, will be noted in the Register Book thus:—"Spar decked."

The total tonnage of the ship is to regulate the equipment, except the anchors and chains for Steam-vessels. (*See* Section 81.)

SECTION 10. Beam plates to be in depth one-quarter of an inch for every foot in length of the midship beams, and to be in thickness one-sixteenth of an inch for every inch in depth of the said beams, and to be made of H iron, T bulb iron, or bulb plate with double angle irons rivetted on upper edge; the two sides of each of these angle irons to be not less in breadth than three-fourths the depth of beam plate, and to be in thickness one-sixteenth of an inch for every inch of the two sides of the angle iron; or the beams may be composed of any other approved form of beam iron, equal in strength. Where beams below the upper or middle deck (including orlop beams) have no deck laid upon them, the angle irons on their upper edges are required to be of the dimensions of the angle iron of the reverse frames. All beams to be well and efficiently connected or rivetted to the frames, with bracket ends or knee plates; each arm of knee plates at ends of beams not to be less in length than twice and half the depth of beams, and to be in thickness equal to the beams. The beams to be placed over each other, and pillared where practicable.

SECTION 13. The wood ceiling or lining is not to be less than one and a half inch, nor more than three inches in thickness in any case, and is to be so fastened to the reversed angle irons or frames that it may be easily removed for survey or painting.

SECTION 15. All vessels to have tie-plates ranging all fore and aft upon each side of the hatchways *on each tier of beams*, and in addition thereto the beams of the upper and middle decks, in three-decked or spar-decked ships, and of the upper deck in vessels of one or two decks, must have the tie-plates fitted from side to side, diagonally (as shown in fig. 15), wherever the arrangement of the

deck will admit of them; the tie-plates are to be in width once and a-half the depth of beams, and of the thickness required for stringer plates, and to be well rivetted to each other, and to the beams, deck hooks, and transoms; and all butts to be properly shifted. Upon hold beams where no deck is to be laid, or where tie-plates would interfere with stowage of cargo, an angle iron of the dimensions given in Table G for angle iron or beam stringers, placed at middle line, extending fore and aft wherever practicable, and well rivetted to all beams, deck hoops, and transoms, will be admitted in lieu thereof.

All hatchways and the mast-holes of sailing ships are to be properly framed to receive half beams where required, and the latter to have mast partners at each tier of beams (except at orlop beams), the plating of which is not to be less in thickness than is required for stringer plates, and the united breadths of the plates not to be less than three times the diameter of the masts. The said plates are to be well rivetted to each other, and to the beams, and angle iron carlings; and at the decks where masts are to be wedged, an angle iron of the dimensions required for the main frames of the ship is to be properly fitted and rivetted to the plates round the mast holes. The mast holes of steam vessels must be properly secured to the satisfaction of the Surveyors.

Add to Section 16, at the end—“The depth for the foregoing purpose in spar-decked ships is to be taken from the under side of the ‘tonnage’ or middle deck to the top of the floor plates.”

DOUBLE BOTTOMS.—To entitle a Vessel to be noted in the Register Book as having a “*Double Bottom*,” the inner or second bottom must be efficiently constructed, with the plating carried forward to the fore bulkhead, as usually fitted, and to an equal distance from the after end of the ship; the plating not to be less in thickness than that given in Table G for plating of bulkheads, excepting the flange plate, which must be one-sixteenth thicker. The double bottom must be efficiently connected to the outside plating and frames of the main body of the ship. The butts and edges may be single rivetted. “Man holes” must be constructed, or provision made for the removal of a portion of the plates so as to enable the inner surface of outside plating, the frames, floors, keelsons, and rivets to be thoroughly examined, and coated when required. The upper

side of the plating must be protected with wood planking as ceiling.

Should a smaller portion of the ship be constructed as above, such ship may be marked "*Part Double Bottom*," provided such portions extend to at least one-half of the length.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

No. 2, *White Lion Court*, Cornhill, London, E. C.,
25th May, 1865.

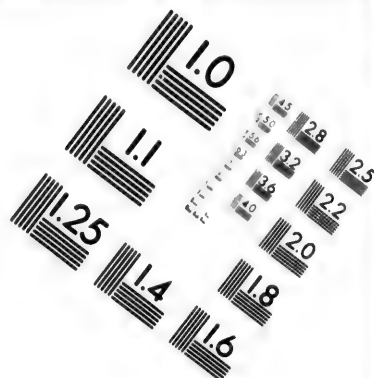
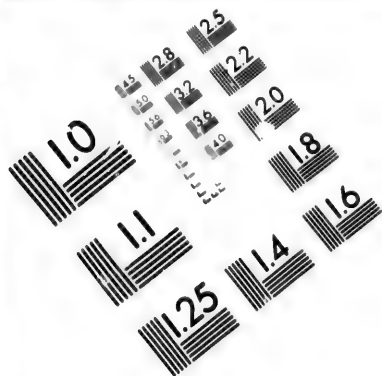
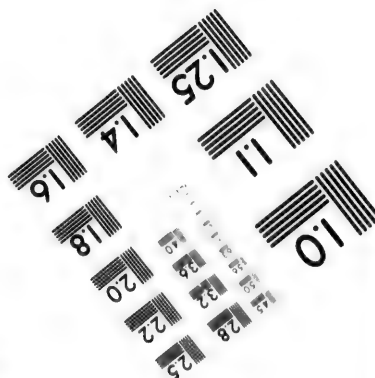
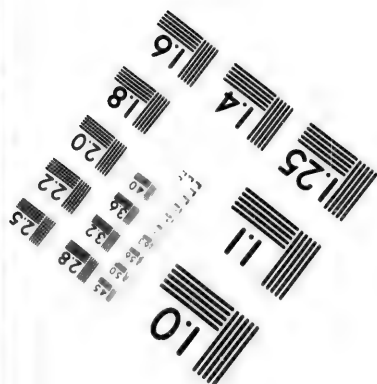
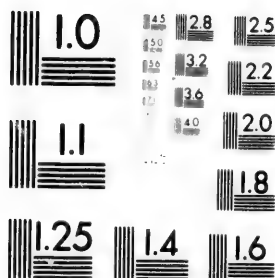


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10

ALPHABETICAL RETURN OF VESSELS
ON THE THIRTIETH DAY

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------------|---------------------|----------------|-------|----------------|
| Abbie Thomas | 49428 | Barque..... | 590 | 1864 |
| Abeona | 37732 | Schooner | 99 | 1846 |
| Abeona..... | ..... | Schooner | 29 | 1838 |
| Abie Perkins | 49005 | Schooner | 107 | 1864 |
| Abigail..... | ..... | Schooner | 58 | 1833 |
| Abigail | ... | Schooner | 110 | 1837 |
| Abigail..... | ..... | Schooner .. | 43 | 1839 |
| Abigail | 37437 | Schooner | 53 | 1842 |
| Abigail | 39005 | Schooner | 36 | 1835 |
| Abilene | 36304 | Brigantine ... | 136 | 1860 |
| Acadia | 49026 | Schooner ... | 72 | 1865 |
| Acadia | 36525 | Brigantine ... | 190 | 1861 |
| Acadia | 35004 | Schooner | 81 | 1848 |
| Acadian..... | ..... | Schooner | 45 | 1824 |
| Acadian..... | ..... | Brigantine ... | 28 | 1838 |
| Acadian..... | ..... | Schooner | 33 | 1840 |
| Acadian | 42304 | Schooner | 57 | 1862 |
| Acadian Lass | 38419 | Schooner | 24 | 1861 |
| Acadian Lass | ... | Schooner | 37 | 1830 |
| Achilles | 37639 | Schooner | 17 | 1856 |

REGISTERED IN NOVA SCOTIA,
OF SEPTEMBER, 1865.

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|--------------------------------|-------------------|
| St. Mary's Bay.... | Geo. H. & Andrew Lovitt..... | Yarmouth. |
| Port Medway..... | Charles Forest..... | Arichat. |
| Briar Island..... | Asa D. Crowell, et al..... | Halifax. |
| Parrsborough..... | James A. Hatfield, et al.... | Parrsborough. |
| New Brunswick... | William Burger..... | Halifax. |
| Yarmouth..... | John Van Morden..... | Halifax. |
| Barrington..... | James Snow, et al..... | Halifax. |
| Mahone Bay..... | George Herritt, et al..... | Halifax. |
| Barrington..... | Reuben I. Hart..... | Halifax. |
| Truro..... | Samuel Nelson, et al..... | Halifax. |
| Parrsborough..... | Charles McCabe, et al..... | Parrsborough. |
| Strait of Canso... | E. McDonald..... | Liverpool, N. S. |
| Annapolis..... | Joseph Wheelock, et al..... | Halifax. |
| Sable River..... | George Boot, et al..... | Halifax. |
| Argyle..... | Francis D'Entrement, et al.... | Halifax. |
| Barrington..... | Saml. Nickerson, et al..... | Halifax. |
| Londonderry..... | James S. Hickman, et al..... | Halifax. |
| L'Ardoise..... | Michael Sampson, et al..... | Arichat. |
| New Edenberg.... | John Domingo, et al..... | Halifax. |
| Newport..... | James H. Pratt..... | Parrsborough. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------------|------------------|----------------|-------|-------------|
| Achiever | 35806 | Schooner | 75 | 1852 |
| Acme | 38047 | Brig | 306 | 1857 |
| Acorn | 35116 | Schooner | 21 | 1853 |
| Active ..... | ..... | Schooner | 17 | 1847 |
| Active | 38342 | Schooner | 40 | 1847 |
| Active ..... | ..... | Schooner | 42 | 1845 |
| Active | 36998 | Schooner | 15 | 1860 |
| Active | 42786 | Schooner | 54 | 1861 |
| Active | 38546 | Schooner | 33 | 1830 |
| Active | 38593 | Schooner | 17 | 1855 |
| Active | 37485 | Schooner | 36 | 1832 |
| Active | 38193 | Schooner | 37 | 1853 |
| Active ..... | ..... | Schooner | 42 | 1837 |
| Actress ..... | ..... | Schooner | 59 | 1844 |
| Ada | 38222 | Barque | 815 | 1852 |
| Ada | 42096 | Schooner | 29 | 1863 |
| Ada | 41542 | Brigantine ... | 134 | 1858 |
| Ada Ann | 36496 | Schooner | 22 | 1860 |
| Adah | 52037 | Schooner | 124 | 1865 |
| Adala | 42012 | Schooner | 8 | 1859 |
| Adam Burns | 49459 | Schooner | 99 | 1864 |
| Adelaide | 37729 | Schooner ... | 113 | 1852 |
| Adelaide | 38390 | Schooner ... | 18 | 1859 |
| Adelaide | 46021 | Brigantine ... | 147 | 1862 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|------------------------------|-------------------|
| Barrington..... | J. Banks, et al..... | Halifax. |
| Beaver River.... | John Young, et al..... | Yarmouth. |
| Cornwallis..... | John Roop..... | Windsor. |
| Little Arichat.... | William Creighton..... | Arichat. |
| Rocky Bay..... | Edward Marchaud, et al..... | Arichat. |
| Newport..... | John Alexander, et al..... | Parrsborough. |
| Mahone Bay..... | James Meisner..... | Lunenburg. |
| Shelburne..... | John V. Williams, et al..... | Shelburne. |
| LaHave..... | William Burke, et al..... | Sydney, C. B. |
| Little Bras D'Or.. | James McDonald, et al..... | Sydney, C. B. |
| Argyle..... | Obid Smith..... | Yarmouth. |
| Yarmouth..... | A. C. Robbins, et al..... | Yarmouth. |
| Shelburne..... | Nehemiah Wilson, et al..... | Halifax. |
| Little Harbour.... | Richard McLearn..... | Halifax. |
| Yarmouth..... | Benjamin Murphy, et al..... | Yarmouth. |
| Westport..... | George Bayley, et al..... | Digby. |
| Walton..... | John Mosher, et al..... | Windsor. |
| Mahone Bay..... | John Hebb, Junr., et al..... | Lunenburg. |
| Clementsport.... | James Gilleatt..... | Annapolis. |
| Digby..... | Rachael Snow..... | Digby. |
| Wallace..... | Murdoch Munro, et al..... | Pugwash. |
| Little Arichat.... | Valentine Brinton..... | Arichat. |
| River Bourgeois.. | Charles Landry..... | Arichat. |
| Prince Port..... | James Douglas, et al..... | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------|------------------|----------------|-------|-------------|
| Adelaide ..... | ..... | Schooner | 70 | 1836 |
| Adelaide ..... | ..... | Schooner | 74 | 1834 |
| Adelaide | 36126 | Schooner | 27 | 1857 |
| Adelaide | 42270 | Schooner | 50 | 1856 |
| Adelia | 42009 | Schooner | 89 | 1859 |
| Adelini ..... | ..... | Schooner | 46 | 1846 |
| Adeline | 36608 | Schooner | 63 | 1859 |
| Adino | 36283 | Schooner | 40 | 1860 |
| Admiral ..... | ..... | Schooner | 72 | 1848 |
| Adonis | ... | Schooner | 73 | 1852 |
| Adonia | 37821 | Barque | 90 | 1847 |
| Adria | 46014 | Schooner | 118 | 1863 |
| Adrio | 50751 | Brigantine ... | 264 | 1864 |
| Advance | 38055 | Barque | 631 | 1857 |
| Advance | 41548 | Brigantine ... | 138 | 1858 |
| Advent | 42326 | Brigantine ... | 184 | 1862 |
| Adventure ..... | ..... | Schooner | 33 | 1824 |
| Aesial ..... | ..... | Schooner ... | 50 | 1847 |
| Afton | 39273 | Brigantine ... | 161 | 1857 |
| Africa | 35790 | Brigantine ... | 130 | 1853 |
| Agalia | 48255 | Schooner | 20 | 1861 |
| Agenora | 38635 | Schooner | 81 | 1862 |
| Agenora | 35658 | Brigantine ... | 103 | 1854 |
| Agent | 52062 | Brigantine ... | 169 | 1865 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------------|---------------------------------------|-------------------|
| Yarmouth. | John Murray. | Halifax. |
| Parrsborough. | John Davison. | Halifax. |
| LaHave. | Smith Swine. | Halifax. |
| N. Carolina, U. S. | Nehemiah K. Clements. | Halifax. |
| Wilmot. | James R. Foster, et al. | Digby. |
| Port Medway. | J. & A. Loyd. | Liverpool, N. S. |
| Parrsborough. | David Pettis, et al. | Windsor. |
| Sheet Harbor. | William Hall. | Halifax. |
| Granville. | James P. Foster, et al. | Digby. |
| Clare. | Holland E. Payson. | Digby. |
| Windsor. | Alexander Dill, et al. | Halifax. |
| River Herbert. | George Hebbard, et al. | Parrsborough. |
| Londonderry. | Archibald W. McLellan, et al. | Halifax. |
| Meteghan. | John W. Lovett, et al. | Yarmouth. |
| Cornwallis. | D. R. Eaton, et al. | Windsor. |
| Maitland. | John R. Crow, et al. | Halifax. |
| Argyle. | John Murray. | Halifax. |
| Shelburne. | Joshua Snow, et al. | Halifax. |
| P. E. Island. | John M. Watson, et al. | Halifax. |
| Newport. | John Taylor. | Halifax. |
| Sable River. | John Detman, et al. | Shelburne. |
| Main-a-Dieu, C. B. | George Dickson, et al. | Sydney, C. B. |
| Pubnico. | Edward J. Murphy, et al. | Halifax. |
| Cornwallis. | Enoch West. | Windsor. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Agile | 41972 | Schooner | 29 | 1859 |
| Agile | 36996 | Schooner | 27 | 1861 |
| Agile | 42836 | Brigantine ... | 143 | 1861 |
| Agility | 36146 | Schooner | 62 | 1858 |
| Agnes | 50752 | Barque..... | 719 | 1864 |
| Agnes Fraser | 42072 | Barque..... | 288 | 1863 |
| Agnes C. James | 38100 | Barque... .. | 283 | 1859 |
| Agnes Ross..... | ..... | Schooner | 42 | 1847 |
| Agnes Ross | 36115 | Schooner | 41 | 1847 |
| Ago | 48128 | Schooner | 26 | 1864 |
| Agra | 48438 | Barque..... | 639 | 1864 |
| Aimee | 46662 | Schooner | 108 | 1847 |
| Aimwell | 36205 | Schooner | 36 | 1859 |
| Aimeer | 42826 | Schooner | 103 | 1861 |
| A la Mode | 36990 | Schooner | 29 | 1860 |
| Albion | 39695 | Schooner | 69 | 1834 |
| Albion | 37465 | Schooner | 26 | 1842 |
| Albion | 37415 | Brig..... | 127 | 1845 |
| Albion | 34829 | Schooner | 55 | 1837 |
| Albena..... | ..... | Schooner | 27 | 1847 |
| Albert | 38238 | Schooner . . | 57 | 1845 |
| Albert | 37329 | Schooner | 68 | 1857 |
| Albert..... | ..... | Schooner | 31 | 1846 |
| Albert | 36061 | Schooner | 65 | 1846 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|--------------------------------|-------------------|
| Vogler's Cove..... | T. Vogler, et al..... | Liverpool, N. S. |
| Lunenburg..... | Edward Young, et al..... | Lunenburg. |
| Green's Creek..... | George Dart, et al..... | Windsor. |
| Port Medway..... | William H. Pye..... | Halifax. |
| Maitland..... | Archibald McCullum, et al..... | Halifax. |
| Pictou..... | William Ives..... | Pictou. |
| Tusket..... | Aaron Goudrey, et al..... | Yarmouth. |
| Shelburne..... | P. McKay, et al..... | Liverpool, N. S. |
| Shelburne..... | Franklin P. Payson, et al..... | Halifax. |
| Barrington..... | David Smith, et al..... | Halifax. |
| Windsor..... | P. M. Cunningham, et al..... | Windsor. |
| United States..... | Urbain Daucett..... | Digby. |
| Port Peswick..... | Leonard Byers..... | Halifax. |
| Cornwallis..... | William Gilleat, et al..... | Windsor. |
| LaHave..... | M. & J. Himmelman..... | Lunenburg. |
| Little Arichat..... | J. W. Morrison..... | Arichat. |
| LaHave..... | J. Pride..... | Liverpool, N. S. |
| Liverpool..... | S. C. Tupper..... | Liverpool, N. S. |
| Londonderry..... | John Alexander, et al..... | Halifax. |
| Mabou..... | William McKeen..... | Pictou. |
| Wiscasset, U. S. . . | John Carland, et al..... | Yarmouth. |
| St. Mary's Bay... . | James E. Rice, et al..... | Digby. |
| Canso..... | John Whitman..... | Halifax. |
| Londonderry..... | Samuel C. Nelson..... | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------------------|------------------|---------------|-------|-------------|
| Albert Crossby | 37640 | Barque..... | 244 | 1856 |
| Albert Davis | 49019 | Brig..... | 364 | 1865 |
| Albert McKeane | 46487 | Schooner.... | 17 | 1865 |
| Albacore | 37484 | Barque..... | 247 | 1856 |
| Alexander | 37211 | Schooner.... | 72 | 1852 |
| Alexander..... | ..... | Brigantine... | 115 | 1835 |
| Alexander | 35773 | Schooner.... | 70 | 1846 |
| Alexander..... | ..... | Schooner.... | 48 | 1837 |
| Alexander Fraser | 36474 | Schooner.... | 31 | 1859 |
| Alexander McLeod | 42230 | Schooner.... | 109 | 1862 |
| Alexandra | 38262 | Ship..... | 889 | 1863 |
| Alexandria | 42220 | Schooner.... | 35 | 1863 |
| Aleda | 34955 | Schooner.... | 59 | 1852 |
| Alert..... | ..... | Schooner.... | 14 | 1843 |
| Alert | 37400 | Schooner... | 41 | 1857 |
| Alert | 37536 | Schooner.... | 57 | 1854 |
| Alert..... | ..... | Barque..... | 116 | 1847 |
| Alert..... | ..... | Schooner.... | 62 | 1846 |
| Alert | 35952 | Schooner.... | 20 | 1846 |
| Alert | 48126 | Schooner.... | 131 | 1864 |
| Alfred Taylor | 46902 | Schooner.... | 20 | 1864 |
| Algona | 37873 | Schooner.... | 81 | 1850 |
| Alice | 37011 | Brigantine... | 109 | 1860 |
| Alice | 42830 | Brigantine... | 187 | 1861 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|----------------------------------|-------------------|
| Economy..... | Edwin Pinkham, et al..... | Parrsborough. |
| Parrsborough.... | Joshua Dewis, et al..... | Parrsborough. |
| LaHave..... | William McKeane..... | Lunenburg. |
| Liverpool, N. S. . | C. & J. Morton & Co..... | Liverpool, N. S. |
| Gilbert Cove | Peter McKay..... | Digby. |
| Windsor | John McGregor | Halifax. |
| United States..... | James B. Oxley..... | Halifax. |
| Truro..... | Charles Pricker..... | Halifax. |
| Lunenburg..... | Lewis Hurtle..... | Lunenburg. |
| Annapolis..... | John M. Foster, et al..... | Annapolis. |
| Pleasant Cove.... | Abel C. Robbins | Yarmouth. |
| Port Medway..... | D. Condon, et al..... | Liverpool, N. S. |
| Parrsborough.... | John K. Elderkin, et al..... | Parrsborough. |
| Pugwash | Angus Beaton | Pictou. |
| Guysborough..... | Abraham M. Whitman..... | Guysborough. |
| LaHave | Philip Ross, et al..... | Lunenburg. |
| Barrington | Thomas Coffin, junr., et al..... | Halifax. |
| Parrsborough.... | Charles Stuart..... | Halifax. |
| Little River. | Thomas Duncan..... | Halifax. |
| Londonderry | George Peppard, et al..... | Halifax. |
| Margaree | Henry Taylor, et al..... | Port Hawkesbury. |
| Londonderry | John Vance..... | Parrsborough. |
| Barrington | John Punton..... | Shelburne. |
| Maitland | William H. Hamilton, et al..... | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------------------|------------------|----------------|-------|-------------|
| Alice | 35999 | Schooner | 22 | 1852 |
| Alice | 36004 | Schooner | 27 | 1856 |
| Alice | 36352 | Brigantine ... | 111 | 1861 |
| Alice Franklin | 38033 | Brig..... | 814 | 1857 |
| Alice Maud Stone | 42228 | Schooner | 38 | 1861 |
| Alice Mills | 42234 | Schooner | 48 | 1862 |
| Alice M. Hopper | 38650 | Schooner | 39 | 1864 |
| Alice V. Goodhue | 38149 | Brigantine ... | 151 | 1851 |
| Allandale | 37661 | Brigantine ... | 147 | 1860 |
| Allegro | 35729 | Schooner | 50 | 1854 |
| Alliance | 37349 | Schooner | 93 | 1858 |
| Alliance | 36176 | Schooner | 39 | 1858 |
| Alma | 37802 | Brigantine ... | 140 | 1856 |
| Alma | 34832 | Schooner | 65 | 1857 |
| Alma | 36132 | Barque | 302 | 1854 |
| Alma | 36487 | Schooner | 65 | 1860 |
| Alma | 48034 | Schooner | 28 | 1863 |
| Alma | 37335 | Schooner | 30 | 1844 |
| Alma | 37231 | Schooner | 95 | 1855 |
| Alma | 37245 | Schooner | 85 | 1855 |
| Alma | 37366 | Schooner | 48 | 1854 |
| Alma | 1027 | Barque | 503 | 1854 |
| Alma Jane | 50763 | Brigantine ... | 126 | 1864 |
| Almira | 37464 | Schooner | 61 | 1832 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|-----------------------------------|-------------------|
| Bay of Islands.... | John Gullum, et al | Halifax. |
| Clam Harbor.... | William Palmer..... | Halifax. |
| Shelburne | Thomas C. Kinnear, et al..... | Halifax. |
| Yarmouth..... | E. M. Moody, et al..... | Yarmouth. |
| Wilmot..... | James E. Cropley, et al..... | Annapolis. |
| Annapolis..... | John Mills, et al..... | Annapolis |
| Fouchie, C. B.... | Josiah Hooper..... | Sydney, C. B. |
| Salmon River.. | A. C. Robbins, et al..... | Yarmouth. |
| Economy | James P. Soley, et al..... | Parrsborough. |
| New Dublin..... | John Oxner, et al..... | Halifax. |
| Hillsborough | William Law, et al..... | Yarmouth. |
| Petite Reviero.... | William S. Drew, et al. | Halifax. |
| Ship Harbor..... | Thomas Embrio, et al..... | Pictou. |
| Bay Verte | Joseph Jones | Pictou. |
| Liverpool | L. A. Sponagle | Liverpool, N. S. |
| Lunenburg..... | Ephraim Lohnes, et al..... | Lunenburg. |
| Clements | William Franklin Potter, et al... | Annapolis. |
| Maine, U. S..... | J. H. Hatfield, et al..... | Yarmouth. |
| Hillsborough | John Proctor, et al. | Digby. |
| Granville | Thomas Hall, et al | Digby. |
| Guysborough | Reuben I. Hart, et al | Halifax. |
| Campton, N. B.... | John Lithgow | Halifax. |
| Shelburne | William Muir, et al..... | Shelburne. |
| Maine, U. S..... | Obid Smith | Yarmouth. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------------------|---------------------|----------------|-------|----------------|
| Aloe | 37637 | Schooner | 73 | 1856 |
| Alonzo | 4 79 | Barque | 591 | 1863 |
| Alonzo ..... | ..... | Schooner | 45 | 1847 |
| Alpha | 37865 | Brigantine ... | 129 | 1855 |
| Alpha | 49015 | Brigantine ... | 142 | 1865 |
| Alton | 37401 | Schooner | 133 | 1857 |
| Althea | 38197 | Brigantine ... | 92 | 1862 |
| Aluarer | 46489 | Schooner | 42 | 1865 |
| Alvarado | 46382 | Brigantine ... | 177 | 1863 |
| Amazon | 37671 | Brigantine ... | 198 | 1861 |
| Amazon | 37375 | Schooner | 94 | 1855 |
| Amaranth | 37233 | Schooner | 99 | 1854 |
| Amathist ..... | ..... | Schooner | 71 | 1840 |
| Amanda Jane | 42370 | Brigantine ... | 182 | 1863 |
| Amanda | 50783 | Schooner | 28 | 1865 |
| Amelia ..... | ..... | Schooner | 15 | 1851 |
| Amelia | 38406 | Schooner | 51 | 1860 |
| Amelia | 36797 | Schooner | 63 | 1859 |
| Amelia | 38452 | Schooner | 114 | 1863 |
| Amelia | 41655 | Brig | 236 | 1859 |
| Amelia | 48265 | Brigantine... | 255 | 1864 |
| Amelia | 42364 | Schooner | 35 | 1863 |
| Amelia | 53556 | Schooner | 105 | 1865 |
| Amelia & Ellen | 35948 | Schooner | 11 | 1855 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------------|------------------------------------|-------------------|
| Minudie | Gilbert Seaman | Parrsborough. |
| River John | Peter Crerar | Halifax. |
| Clements | Alfred Winchester | Digby. |
| Horton | Gaspar Drillio, et al. | Windsor. |
| Maccan | Jeptha Elderkin, et al. | Parrsborough. |
| Little River | Elisha William Kundle | Guysborough. |
| Shelburne | S. Messenger, et al | Yarmouth. |
| Lallave | Leonard Rothenhiser | Lunenburg. |
| Cornwallis | William Simpson, et al | Windsor. |
| Parrsborough | Joshua Denis, et al. | Parrsborough. |
| Guysborough | James A. Shaw | Halifax. |
| Digby | Moses Shaw, et al | Digby. |
| Parrsborough | William Glenn, et al. | Halifax. |
| Noel | Osmond O'Brien, et al. | Halifax. |
| Petite Riviere | Peter Bell | Halifax. |
| Pomquet | Peter De'Carteret, et al | Arichat. |
| Grand Ruisseau | John Lucas, et al | Arichat. |
| P. Edward Island | Abraham Givroir | Arichat. |
| Arichat | Peter LeBlanc | Arichat. |
| River John | Charles McLennan | Pictou. |
| Shelburne | L. M. M. Willett, et al. | Shelburne. |
| Pope's Harbor | David Cruchey | Arichat. |
| Amherst | Elisha Embree, et al | Halifax. |
| Chester | Michael Flinn | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------|------------------|---------------|-------|-------------|
| America | 49081 | Brigantine... | 206 | 1864 |
| America..... | ..... | Brigantine... | 80 | 1852 |
| America | 42126 | Brigantine... | 231 | 1862 |
| Amiel | 35679 | Schooner | 41 | 1853 |
| Amiel Corkum | 46476 | Schooner | 46 | 1864 |
| Amie Ada | 52014 | Barque..... | 280 | 1865 |
| Amie Maria | 41732 | Schooner | 59 | 1853 |
| Amy M. | 46371 | Schooner | 74 | 1863 |
| Andrew Sprague | 42846 | Schooner | 72 | 1846 |
| Angelique..... | ..... | Schooner | 48 | 1836 |
| Angelique..... | ..... | Schooner | 27 | 1846 |
| Angelique | 36053 | Schooner | 31 | 1841 |
| Angelique | 38359 | Schooner | 23 | 1857 |
| Angelique | 38360 | Schooner | 16 | 1857 |
| Angelique.. .. | ..... | Schooner | 26 | 1820 |
| Angelique..... | ..... | Schooner ... | 35 | 1828 |
| Angelique..... | ..... | Schooner | 23 | 1837 |
| Angel Dugas..... | ..... | Schooner | 15 | 1851 |
| Angola | 49007 | Schooner | 122 | 1864 |
| Anglo Saxon..... | ..... | Brig..... | 167 | 1864 |
| Anglo American..... | .. .. | Brigantine... | 131 | 1848 |
| Annabella..... | ..... | Schooner | 22 | 1842 |
| Annabella | 38287 | Schooner | 69 | 1845 |
| Annabella... .. | ..... | Schooner | 20 | 1854 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|---------------------------------|-------------------|
| Merigomish | John T. Ives, et al..... | Pictou. |
| Cornwallis | William Harrington | Windsor. |
| New Brunswick... | William M. Harrington..... | Hali. |
| LaHave..... | L. & I. Gerhart..... | Lunenburg. |
| LaHave..... | Caleb Corkum, et al | Lunenburg. |
| Clyde River..... | James Sutherland, et al..... | Shelburne. |
| Canada | Felig Landry | Arichat. |
| Advocate | Asahel Bligh, et al | Windsor. |
| Five Islands | William Wadman, et al..... | Halifax. |
| D'Escouse | Henry Landry..... | Arichat. |
| Margaree | Aubert LeBlanc, et al..... | Arichat. |
| Harbor Bouche... | Richard McLearn..... | Arichat. |
| Grand Ruisseau... | Simon LeBlanc..... | Arichat. |
| L'Ardoise..... | Mara Pate..... | Arichat. |
| Harbour Bouche.. | Michael Drew, et al | Halifax. |
| Harbour Bouche.. | Charles Levanger, et al..... | Halifax. |
| New Brunswick... | Thomas O'Quinn..... | Halifax. |
| River Bourgeois .. | Placid Dugas..... | Arichat. |
| Parrsborough..... | John E. Southengreen, et al.... | Parrsborough. |
| Kempt | Edwin Tomlinson, et al..... | Halifax. |
| Petite..... | Abel Tomlinson, et al | Halifax. |
| Tracadie | Roderick McKenzie, et al..... | Arichat. |
| Grand Ruisseau... | James LeLaushein | Arichat. |
| Barrington..... | Asa D. Crowell..... | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|---------------|-------|-------------|
| Anna Conda | 37672 | Schooner | 57 | 1861 |
| Anna Francis | 46655 | Barque..... | 297 | 1863 |
| Anna Lenora | 37306 | Schooner | 73 | 1856 |
| Anna Maria | 49012 | Schooner | 99 | 1856 |
| Anna Miller | 37319 | Schooner | 93 | 1856 |
| Anna | 41774 | Schooner | 30 | 1843 |
| Anna | 52032 | Schooner | 88 | 1865 |
| Ann | 37698 | Schooner | 95 | 1851 |
| Ann | 35781 | Schooner | 76 | 1861 |
| Ann | 37633 | Schooner | 14 | 1855 |
| Ann..... | ..... | Schooner | 15 | 1823 |
| Ann..... | ..... | Schooner | 47 | 1812 |
| Ann..... | ..... | Schooner | 13 | 1823 |
| Ann..... | ..... | Schooner | 39 | 1814 |
| Ann..... | ..... | Schooner | 20 | 1822 |
| Ann | 42362 | Barque..... | 389 | 1863 |
| Ann & Alice | 37982 | Barque..... | 309 | 1862 |
| Ann Augusta | 38027 | Barque..... | 645 | 1856 |
| Ann Eliza..... | ..... | Schooner | 77 | 1848 |
| Ann Eliza | 29374 | Ship..... | 717 | 1859 |
| Ann Isabella..... | ..... | Schooner | 58 | 1833 |
| Ann Leahy | 50767 | Schooner | 83 | 1861 |
| Ann Lovitt | 38093 | Brigantine... | 135 | 1859 |
| Ann Maria | 41772 | Schooner | 31 | 1858 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|---------------------------------|-------------------|
| Parrsborough..... | Edmond J. Ward, et al. | Parrsborough. |
| Sandy Cove..... | Jones Morehouse, et al. | Digby. |
| Gilbert Cove..... | Henry S. Mallitt..... | Digby. |
| Cherry Field, U. S. | William H. Mitchell..... | Parrsborough. |
| Annapolis..... | George Ogilvie, et al. | Digby. |
| New Dublin..... | John Henry Publicover | Halifax. |
| Wilmot..... | James E. Chapman, et al. | Annapolis. |
| Arichat | Peter Grimes..... | Arichat. |
| P. E. Island..... | Victor LaVasche..... | Arichat. |
| Parrsborough..... | Frederick Barts | Parrsborough. |
| Chester | Andrew Walker | Halifax. |
| Falmouth..... | William Young, et al. | Halifax. |
| Petite..... | Benjamin Redding..... | Halifax. |
| Barrington..... | John Stoddart, et al. | Halifax. |
| Lake Porter..... | William Green..... | Halifax. |
| Maitland | Archibald McCullum, et al. | Halifax. |
| Church Point..... | John S. Hatfield, et al. | Yarmouth. |
| Meteghan..... | John W. Lovett, et al. | Yarmouth. |
| Parrsborough... | Thomas Fairweather, et al. | Parrsborough. |
| Hantsport | Ezra Churchill, et al. | Windsor. |
| Trout Cove..... | David Butterford, et al. | Halifax. |
| Marblehead, U. S. | William Leahey..... | Halifax. |
| Meteghan..... | B. Burrell, et al. | Yarmouth. |
| LaHave. | Philip Skenkel, et al. | Lunenburg. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|----------------|------------------|-------------|
| Ann McKeane | 37531 | Brig | 140 [•] | 1854 |
| Annie | 49443 | Barque | 480 | 1865 |
| Annie | 38381 | Schooner | 25 | 1858 |
| Annie | 37741 | Barque | 249 | 1854 |
| Annie | 36832 | Schooner | 86 | 1860 |
| Annie ..... | ..... | Schooner | 30 | 1836 |
| Annie | 46300 | Brigantine ... | 267 | 1864 |
| Annie | 36832 | Schooner ... | 86 | 1860 |
| Annie Collins | 46037 | Brigantine ... | 168 | 1862 |
| Annie Gildert | 37905 | Brigantine ... | 112 | 1851 |
| Annie Grieve | 38654 | Schooner | 111 | 1861 |
| Annie Laurie | 41804 | Schooner | 37 | 1858 |
| Annie Laurie | 42235 | Schooner | 90 | 1863 |
| Annie Laurie | 36066 | Schooner | 24 | 1857 |
| Annie Lavinia | 49413 | Schooner | 81 | 1864 |
| Annie Leahy | 50767 | Schooner | 83 | 1851 |
| Annie Mussenden | 37009 | Schooner | 57 | 1860 |
| Annie M. | 42687 | Brigantine.... | 99 | 1862 |
| Annie M. Young | 48084 | Brig | 345 | 1864 |
| Annie Maria | 38592 | Schooner | 63 | 1857 |
| Annette | 35779 | Brigantine.... | 114 | 1854 |
| Answer ..... | ..... | Schooner | 80 | 1837 |
| Antelope ..... | ..... | Schooner | 36 | 1847 |
| Antelope | 37967 | Schooner | 50 | 1854 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|--------------------------------|-------------------|
| New Dublin..... | William B. Hamilton..... | Halifax. |
| Green Cove... .. | Aaron Goudrey..... | Yarmouth. |
| River Bourgeois .. | Donald Boyd..... | Arichat. |
| New Glasgow..... | William Elliot..... | Pictou. |
| De Sable, P. E. I.. | Chas. T. Irving..... | Pictou. |
| Weymouth | Thomas Small..... | Halifax. |
| Liverpool | M. McLearn, et al..... | Liverpool. |
| P. E. Island..... | Charles T. Irving..... | Pictou. |
| Windsor..... | P. M. Cunningham, et al..... | Windsor. |
| Diligent River.... | Joseph Cochran, et al..... | Windsor. |
| Dunbarton, Scotl'd. | Alexander McKay..... | Sydney, C. B. |
| Annapolis..... | Gilbert Sanders, et al..... | Annapolis |
| Cornwallis | Daniel B. Parker, et al..... | Annapolis. |
| Shelburne..... | George Wilson, et al..... | Halifax. |
| Tusket..... | L. A. Hatfield, et al..... | Yarmouth.. |
| Marblehead..... | William Leahy..... | Halifax. |
| Shelburne..... | Charles Mussenden..... | Shelburne.. |
| St. John..... | John D. Cummins..... | Halifax. |
| Yarmouth..... | John Young & L. E. Baker..... | Yarmouth. |
| North Sydney..... | James E. Muggah..... | Sydney, C. B. |
| Dartmouth | Benjamin W. Salter, et al..... | Halifax. |
| Port Medway..... | Charles Aikins, et al..... | Halifax. |
| Shelburne..... | W. Chadsey, et al..... | Liverpool, N. S. |
| Argyle..... | William M. Bond..... | Yarmouth. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------|------------------|----------------|-------|-------------|
| Antelope | 38196 | Schooner | 66 | 1862 |
| Antelope ..... | ..... | Schooner | 61 | 1851 |
| Antelope | 36195 | Schooner | 25 | 1845 |
| Antelope | 35876 | Schooner | 59 | 1854 |
| Antares | 36036 | Schooner | 41 | 1847 |
| Antecello | 48140 | Brigantine... | 271 | 1864 |
| A. P. L. | 38404 | Brigantine ... | 168 | 1860 |
| Apollo | 33407 | Schooner | 82 | 1853 |
| Apt | 37610 | Schooner | 32 | 1858 |
| Arab | 35889 | Brigantine ... | 113 | 1855 |
| Arabia | ... | Schooner | 54 | 1853 |
| Arabian | 38464 | Brigantine ... | 152 | 1865 |
| Arabella | 36485 | Schooner | 26 | 1859 |
| Arabecca | 41796 | Schooner | 11 | 1858 |
| Arbutus | 38335 | Schooner | 46 | 1852 |
| Arbutus | 38046 | Brig | 277 | 1857 |
| Arcola | 37294 | Schooner | 10 | 1854 |
| Arctic | 35669 | Brigantine ... | 91 | 1850 |
| Arctic | 50787 | Schooner | 29 | 1865 |
| Argyle | 42273 | Schooner | 80 | 1847 |
| Argyle | 36893 | Brigantine ... | 57 | 1860 |
| Argyle ..... | ..... | Schooner | 28 | 1848 |
| Argo | 38263 | Barque | 595 | 1863 |
| Argo | 37731 | Brigantine ... | 150 | 1851 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|-----------------------------------|-------------------|
| Tusket Wedge.... | John W. Lovett, et al..... | Yarmouth. |
| Digby..... | James Crowley, et al..... | Digby. |
| LaHave..... | John Smith..... | Halifax. |
| Lower Village, Col. | James Crowe, et al..... | Halifax. |
| Cape Breton.. . | Donald Murray..... | Halifax. |
| Londonderry..... | William E. McRobbert, et al. | Halifax. |
| Arichat..... | Allen Landry, et al..... | Arichat. |
| L'Ardoise, C. B. . | John McDonald..... | Sydney, C. B. |
| LaHave..... | J. & F. Lohnes..... | Lunenburg. |
| Bedford Basin . . . | John Taylor..... | Halifax. |
| Sable River..... | J. Swain, et al..... | Liverpool, N. S. |
| Arichat..... | Simon Renhard, et al..... | Arichat. |
| Lunenburg..... | Casper Meisner, et al..... | Lunenburg. |
| Tidnish..... | John McEachern..... | Pugwash. |
| Essex Uset..... | Samuel Laurence..... | Arichat. |
| Yarmouth..... | B. Rogers, et al..... | Yarmouth. |
| Westport. | Frederick W. Nuggles..... | Digby. |
| Shelburne..... | Edward Vigneau, et al..... | Halifax. |
| Indian Harbor.... | Isaac Willett..... | Halifax. |
| N. Carleton, U. S. | Nehemiah K. Clements..... | Halifax. |
| Malignant Cove. . | G. M. Mack..... | Liverpool, N. S. |
| P. E. Island..... | Henry Shelmut..... | Halifax. |
| Yarmouth. | Aaron Goudrey..... | Yarmouth. |
| Maitland..... | William McDougal, et al..... | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Argo | 37510 | Schooner | 51 | 1848 |
| Argo | 46481 | Brigantine ... | 165 | 1864 |
| Ariadne | 43078 | Barque. | 375 | 1863 |
| Ariadne..... | ..... | Schooner | 30 | 1836 |
| Ariel | 36573 | Schooner | 44 | 1860 |
| Ariel | 36564 | Schooner | 33 | 1851 |
| Ariel | 37225 | Schooner | 95 | 1855 |
| Ariomede | 46381 | Brigantine ... | 204 | 1863 |
| Arion..... | ..... | Schooner | 48 | 1827 |
| Ariomede | 46381 | Brigantine ... | 204 | 1863 |
| Arion ..... | ..... | Schooner | 48 | 1827 |
| Ark | 38443 | Schooner | 69 | 1863 |
| Armada | 37614 | Schooner | 44 | 1858 |
| A. R. McKenzie | 33005 | Schooner | 42 | 1860 |
| Armide | 37261 | Schooner | 47 | 1829 |
| Armagh | 37880 | Schooner | 70 | 1847 |
| Armstrong | 46394 | Ship..... | 816 | 1863 |
| Arno | 36060 | Schooner ... | 51 | 1854 |
| Arno | 36508 | Schooner | 23 | 1860 |
| Arouse | 36507 | Schooner | 48 | 1860 |
| Arrow | 46478 | Schooner | 27 | 1864 |
| Arrow | 49030 | Schooner | 77 | 1865 |
| Arrival | 36165 | Schooner | 28 | 1850 |
| Arrabel | 41919 | Schooner | 36 | 1858 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|-----------------------------------|-------------------|
| Barrington | Samuel Smith. | Halifax. |
| Malhene Bay..... | George Duncan, et al. | Lunenburg. |
| Tatamagouche.... | Edward Kent, et al. | Pictou. |
| Clements | Nelson Stronach, et al. | Halifax. |
| Sherbrooke | James Dickson. | Guysborough. |
| Lalhave | Lewis Hurtle. | Lunenburg. |
| Clementsport..... | Isaiah S. Potter, et al. | Digby. |
| Maitland | David Crow, et al. | Windsor. |
| Granville | Robert Boid. | Halifax. |
| Maitland | David Crow, et al. | Windsor. |
| Granville | Robert Boid. | Halifax. |
| Grand Ruisseau .. | Peter Ferguson, et al. | Arichat. |
| Port Medway | J. Ritcey, et al. | Lunenburg. |
| Sable River | Alexander McKenzie | Shelburne. |
| Cape Negro..... | Holland E. Payson | Digby. |
| Quaco, N. B..... | John Conlon. | Windsor. |
| Cornwallis | John H. Clarke, et al. | Windsor. |
| Horton | William Lawson. | Halifax. |
| Petite Riviere ... | J. C. Perry. | Liverpool, N. S. |
| Petite Riviere ... | R. & S. Smith | Lunenburg. |
| Malhene Bay | Reuben Vienot, et al. | Lunenburg. |
| Parrsboro..... | Alexander P. Bradley, et al. | Parrsborough. |
| Newfoundland | Nicholas Murphy. | Halifax. |
| Port Medway..... | Nathaniel Butt. | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Arthur | 37609 | Schooner | 27 | 1858 |
| A. Seaman | 49002 | Schooner | 85 | 1864 |
| Asia | 46379 | Brigantine ... | 227 | 1863 |
| Aspen | 50790 | Brigantine ... | 235 | 1865 |
| Assistance ..... | ..... | Schooner | 43 | 1838 |
| Atalanta | 36529 | Schooner | 104 | 1862 |
| Atalanta | 36512 | Schooner | 86 | 1860 |
| Atalanta | 37434 | Schooner | 55 | 1845 |
| Atalia | 41771 | Schooner | 25 | 1858 |
| Athol ..... | ..... | Brigantine.... | 234 | 1853 |
| Athlete | 49478 | Schooner | 174 | 1864 |
| Atlantic | 38443 | Schooner ... | 88 | 1863 |
| Atlantic | 36144 | Schooner | 37 | 1837 |
| Atlantic | 48463 | Schooner | 166 | 1864 |
| Atlas ..... | ..... | Brigantine ... | 135 | 1857 |
| Attie Dinkee | 49410 | Brig | 307 | 1864 |
| Augusta | 37902 | Schooner | 21 | 1857 |
| Augusta Parker | 35723 | Schooner | 44 | unk'wn |
| Augustus Kelly | 38266 | Barque | 304 | 1863 |
| Aura | 48465 | Brig | 256 | 1864 |
| Aurora | 42515 | Brigantine ... | 227 | 1863 |
| Aurora | 34824 | Schooner | 33 | 1848 |
| Aurora | 46038 | Brigantine ... | 237 | 1862 |
| Aurora ..... | ..... | Schooner | 56 | 1826 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|-----------------------------------|-------------------|
| Lunenburg. | C. & J. Schwartz..... | Lunenburg. |
| River Hebert. | George Hebbard, et al..... | Parrsborough. |
| Burncoat. | William Faulkner, et al..... | Windsor. |
| Maitland..... | Joseph Miller, et al..... | Halifax. |
| Margaree..... | M. Le Blanc..... | Arichat. |
| Strait of Canso ... | Archibald B. Skinner, et al. | Port Hawkesbury. |
| Liverpool. | S. Delory..... | Liverpool, N. S. |
| Shelburne..... | William H. Swain, et al..... | Halifax. |
| La Have. | John Ritecy..... | Lunenburg. |
| Pictou..... | John Crerar, et al. | Pictou. |
| Digby..... | Churchill & Taylor, et al..... | Digby. |
| Martinique..... | D. Lauglois..... | Arichat. |
| Argyle. | James W. Lennox..... | Halifax. |
| Hantsport..... | Joseph Lockhart, et al..... | Windsor. |
| Pugwash. | J. H. Reynolds..... | Pictou. |
| Meteghan..... | Thomas Kellam, et al..... | Yarmouth. |
| Cornwallis..... | Edward Russell Bishop..... | Windsor. |
| United States [Am] | Peter McPhee..... | Halifax. |
| Shelburne..... | William Rogers, et al..... | Yarmouth. |
| Cornwallis..... | William McKenzie, et al..... | Windsor. |
| La Have. | Andrew Gon, et al..... | Lunenburg. |
| Little Harbor.... | Franklin E. Payson..... | Digby. |
| Maitland..... | John McDougal, et al..... | Windsor. |
| Falmouth..... | Constant Wilson, et al..... | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------------------|------------------|----------------|-------|-------------|
| Aurora | 39343 | Schooner | 47 | 1849 |
| Australia..... | ..... | Brig | 272 | 1852 |
| Austos | 48267 | Brigantine ... | 132 | 1864 |
| Automaton | 37682 | Schooner | 62 | 1862 |
| Autumn | 42842 | Brigantine ... | 132 | 1861 |
| Ava | 42409 | Schooner | 62 | 1862 |
| Ava | 37834 | Barque..... | 336 | 1855 |
| A. V. Major | 46094 | Schooner | 89 | 1863 |
| Avon | 36451 | Schooner | 46 | 1860 |
| Avon | 37878 | Barque..... | 649 | 1856 |
| Avosetta | 35917 | Brig | 144 | 1855 |
| A. W. Cohoon | 53758 | Barque | 292 | 1865 |
| A. W. Singleton | 38223 | Barque..... | 561 | 1862 |
| A. & E. Lovitt | 38223 | Ship | 872 | 1862 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|--------------------------------|-------------------|
| Shelburne..... | Andrew Lovitt, et al..... | Halifax. |
| River John..... | John Mockler.. .. | Pictou. |
| Shelburne..... | William D. Crowell, et al..... | Shelburne. |
| Apple River.... | Adam McCullough, et al..... | Parrsborough. |
| Noel..... | John Niel, et al... .. | Windsor. |
| Wallace..... | Richard Scotts..... | Pugwash. |
| Newport..... | Nicholas Mosher, et al..... | Windsor. |
| Shelburne..... | Samuel Perry, et al..... | Shelburne. |
| Barrington..... | Bartlett F. Kenny, et al..... | Shelburne. |
| Newport..... | George Munroe, et al..... | Windsor. |
| Bedford Basin.... | James W. Harris, et al..... | Halifax. |
| Port Medway.... | Edward Cohoon, et al..... | Liverpool. |
| Tusket..... | R. Guest, et al..... | Yarmouth. |
| Belle Vue Cove... .. | Andrew Lovett, et al..... | Yarmouth. |

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| Where built. | Registered Owner. | Port of Registry. |
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| Where built. | Registered Owner. | Port of Registry. |
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| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------|------------------|----------------|-------|-------------|
| Banter | | Schooner | 19 | 1839 |
| Banner | 36452 | Schooner | 35 | 1860 |
| Banshee ..... | ..... | Schooner | 24 | 1851 |
| Banquet | 35709 | Schooner | 30 | 1855 |
| Barbara | ... | Schooner | 31 | 1832 |
| Barbara | 38371 | Schooner | 108 | 1857 |
| Barbara Ann ..... | ..... | Schooner | 14 | 1839 |
| Bar | 35928 | Schooner | 26 | 1851 |
| Baronet | 37530 | Schooner | 48 | 1853 |
| Barrington | 36142 | Schooner | 42 | 1858 |
| Bassalieu | 41621 | Brigantine ... | 120 | 1857 |
| Beauty | 37506 | Schooner | 28 | 1857 |
| Beauty | 35984 | Schooner | 123 | 1856 |
| Beatrice | 49481 | Brigantine ... | 157 | 1864 |
| Beaver | 38048 | Barque | 426 | 1857 |
| Bee ..... | ..... | Schooner | 11 | 1839 |
| Bee ..... | ..... | Schooner | 9 | 1841 |
| Bee | 37361 | Schooner | 36 | 1848 |
| Belgrade | 38934 | Schooner | 85 | 1853 |
| Belle | 37238 | Schooner | 106 | 1855 |
| Belle ..... | ..... | Brig. | 158 | 1853 |
| Belle | 35974 | Schooner | 46 | 1856 |
| Belle | 36201 | Schooner | 21 | 1858 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|------------------------------|-------------------|
| New Dublin..... | B. Eisenhaur..... | Lunenburg. |
| Sable River..... | Robert Abbott..... | Shelburne. |
| Bras D'Or, C. B... | Richard Brown, et al..... | Sydney, C. B. |
| Isaac's Harbour... | Simon Griffin..... | Halifax. |
| Granville..... | Mayhew Beckwith..... | Halifax. |
| Arichat..... | Peter LeBlanc, et al..... | Arichat. |
| Clements..... | Longshaw Clarke..... | Halifax. |
| Cape Breton..... | Reuben I. Hart, et al..... | Halifax. |
| Lunenburg..... | Nicholas Ernst, et al..... | Lunenburg. |
| Barrington..... | Warren Doan, et al..... | Halifax. |
| Antigonish..... | James LeBlanc..... | Arichat. |
| Shelburne..... | David Dickson..... | Halifax. |
| LaHave..... | James G. A. Creighton..... | Halifax. |
| Bear River..... | John Harlow..... | Digby. |
| Beaver River..... | N. Atley, et al..... | Yarmouth. |
| Yarmouth..... | Jabez Landers, et al..... | Halifax. |
| Londonderry.... | James M. Spencer, et al..... | Halifax. |
| Guysborough..... | Augustus F. Cunningham..... | Halifax. |
| Grand Ruisseau... | Edward LeBlanc, et al..... | Arichat. |
| Granville..... | Samuel Morrice..... | Digby. |
| Cornwallis..... | Benjamin Wier, et al..... | Halifax. |
| Broad Cove, Lunb'g | Martin Teel, et al..... | Halifax. |
| Port Peswick..... | Thomas Young..... | Halifax. |

| Vessel's Name. | Official Number. | Rdg. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Belle | 48116 | Schooner | 18 | 1863 |
| Belle Paulet | 37713 | Schooner | 108 | 1853 |
| Belle Isle | 35755 | Schooner | 79 | 1854 |
| Bellerophon..... | ..... | Schooner | 9 | 1839 |
| Bellingham | 50788 | Sloop..... | 69 | 1856 |
| Bellona | 37898 | Schooner | 63 | 1845 |
| Belvidere | 42828 | Brig..... | 233 | 1861 |
| Belvidere..... | ..... | Schooner | 24 | 1822 |
| Bella Young | 36500 | Schooner | 34 | 1860 |
| Ben Nevis | 41392 | Brigantine ... | 233 | 1857 |
| Benevolence | 49438 | Schooner | 22 | 1850 |
| Benjamin Killam | 49441 | Schooner | 54 | 1865 |
| Bercaldine | 34970 | Barque..... | 395 | 1854 |
| Bernice | 37547 | Schooner | 66 | 1848 |
| Best..... | ..... | Schooner | 13 | 1828 |
| Bessie | 42306 | Schooner | 104 | 1862 |
| Bessie Harris | 48452 | Barque..... | 589 | 1864 |
| Bessie Bent | 24221 | Brigantine ... | 112 | 1854 |
| Bessie Yorke | 49009 | Brigantine ... | 131 | 1864 |
| Betsey..... | ..... | Schooner | 55 | 1800 |
| Betsey..... | ..... | Schooner | 17 | 1806 |
| Betsey..... | ..... | Schooner | 37 | 1819 |
| Betsey..... | ..... | Schooner | 38 | 1816 |
| Betsey..... | ..... | Schooner | 19 | 1827 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|------------------------------------|-------------------|
| Chezzetcook | Peter Conrod | Halifax. |
| Grand Ruisseau . . . | Edward LeBlanc, et al. | Arichat. |
| St. Mary's | Daniel Cronan | Halifax. |
| Hall's Cove | Moses Stafford | Halifax. |
| Chelsea, U. S. | James F. Phelan | Halifax. |
| Sedgwick, U. S. . . . | William H. Church | Windsor. |
| Walton | David H. Morris, et al. | Windsor. |
| Lunenburg | James Richardson | Halifax. |
| Lunenburg | Caleb Heisler, et al. | Lunenburg. |
| Pictou | Benjamin Forest, et al. | Arichat. |
| Argyle | Louis D'Entrement, et al. | Yarmouth. |
| Argyle | Warren Goodwin, et al. | Yarmouth. |
| Liverpool | J. Campbell | Liverpool, N. S. |
| Barrington | William J. Bigelow, et al. | Halifax. |
| Economy | Samuel Green, et al. | Halifax. |
| Pompquet | Hyacinth Martell | Halifax. |
| Cornwallis | William H. Harris, et al. | Windsor. |
| Wallace | James B. Oxley | Halifax. |
| Parrsborough | Daniel York, et al. | Parrsborough. |
| LaHave | James DeWolf | Halifax. |
| Yarmouth | Samuel Stanwood | Halifax. |
| LaHave | George Smith, et al. | Halifax. |
| Lunenburg | Michael Shirskel, et al. | Halifax. |
| Chezzetcook | Samuel Ashmore | Halifax. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|----------------------------------|------------------|----------------|-------|-------------|
| Betsey..... | ..... | Schooner | 25 | 1814 |
| Betsey..... | ..... | Schooner | 54 | 1833 |
| Betsey..... | ..... | Schooner | 53 | 1839 |
| Betsey..... | ..... | Schooner | 32 | 1825 |
| Betsey..... | ..... | Schooner | 21 | 1845 |
| Betsey Bridge | 35634 | Schooner | 77 | 1845 |
| Betsey & Poly..... | ..... | Schooner | 39 | 1780 |
| Betsheba & Jane..... | ..... | Schooner | 41 | 1817 |
| Beverley | 49474 | Schooner | 72 | 1864 |
| Beverley | 35923 | Schooner | 61 | 1854 |
| Bidwell | 38225 | Barque. . . . | 493 | 1862 |
| Billow | 38438 | Schooner | 122 | 1864 |
| Billow..... | ..... | Brigantine ... | 132 | 1831 |
| Billow..... | ..... | Schooner | 44 | 1827 |
| Billow.. | ..... | Schooner | 22 | 1838 |
| Billow..... | ..... | Schooner | 36 | 1839 |
| Bishop McKinnon | 37718 | Brigantine ... | 155 | 1853 |
| Blanche | 41801 | Schooner | 102 | 1858 |
| Blanche | 52011 | Brigantine ... | 220 | 1865 |
| Blair Athol | 48041 | Barque. | 425 | 1864 |
| Black Hawk | 37206 | Schooner | 16 | 1846 |
| Bloomer..... | ..... | Brigantine ... | 143 | 1852 |
| Bloomer | 38411 | Schooner | 42 | 1853 |
| Bloomer | 37316 | Brigantine... | 104 | 1856 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|-----------------------------------|-------------------|
| Sambro. | James McDonald, et al. | Halifax. |
| St. Mary's. | John Lindsay. | Halifax. |
| Pictou. | James Graham, et al. | Halifax. |
| Blandford. | George Young. | Halifax. |
| Lunenburg. | James A. Moren. | Halifax. |
| L'Ardoise. | Plasiade Boudroit. | Arichat. |
| Rhode Island. | Henry McDaniel. | Halifax. |
| Barrington. | Jessie Dexter Swaine, et al. | Halifax. |
| Westport. | Edward A. Jones, et al. | Digby. |
| United States. | Thomas Blansh. | Halifax. |
| Yarmouth. | Young & Baker. | Yarmouth. |
| Barrington. | J. R. Smith, Junr. | Yarmouth. |
| Newport. | Ezra Churchill, et al. | Windsor. |
| Lunenburg. | Peter Heckman. | Halifax. |
| Canso. | Joseph David, Junr., et al. | Halifax. |
| Guysborough. | John Moore. | Halifax. |
| Little Arichat. | Charles Boudroit, et al. | Arichat. |
| Granville. | Arthur W. Corbell. | Annapolis. |
| Jordan River. | Lewis P. Churchill, et al. | Shelburne. |
| Granville. | John Johnson, et al. | Annapolis. |
| Yarmouth. | David Amberman. | Digby. |
| River John. | Charles McLennan, et al. | Pictou. |
| Strait of Canso. | J. G. McKeen. | Arichat. |
| Gilbert's Cove. | Thomas Killam. | Digby. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------------|------------------|----------------|-------|-------------|
| Bloomer | 37309 | Schooner | 78 | 1852 |
| Bloomer | 35711 | Schooner | 39 | 1852 |
| Bloomer | 38658 | Schooner | 43 | 1865 |
| Bloomer Lass ..... | ..... | Brigantine.... | 124 | 1851 |
| Blossom | 37715 | Schooner | 26 | unk'wn |
| Blossom | .. . | Schooner | 33 | 1840 |
| Blossom ..... | ..... | Schooner | 18 | 1844 |
| Blossom ..... | ..... | Schooner | 49 | 1834 |
| Blomidon | 48441 | Barque. | 563 | 1864 |
| Blonde | 48129 | Brigantine.... | 158 | 1864 |
| Blue Bell | 35996 | Schooner | 25 | 1856 |
| Blue Jacket | 43041 | Schooner ... | 94 | 1862 |
| Blue Nose | 35747 | Schooner | 52 | 1850 |
| Blue Nose | 38136 | Brig..... | 263 | 1860 |
| Blue Wave | 46012 | Brigantine.... | 144 | 1863 |
| Blue Wave | 35042 | Schooner | 42 | 1853 |
| B. O. DeWolf | 51774 | Barque. | 269 | 1865 |
| Bonnie Kate | 37768 | Schooner | 17 | 1855 |
| Boomerang | 49112 | Barque. | 379 | 1865 |
| Bonita | 35702 | Schooner | 38 | 1854 |
| Bonita | 37583 | Schooner | 22 | 1857 |
| Bonita | 35834 | Schooner | 59 | 1852 |
| Bonetta | 52052 | Schooner | 116 | 1865 |
| Boreas | 36889 | Brigantine.... | 156 | 1859 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|---------------------------------|-------------------|
| Granville | Robert Bent, et al. | Digby. |
| LaHave..... | Thomas Shaw..... | Halifax. |
| North Sydney.... | Samuel Jenkrose..... | Sydney, C. B. |
| New Glasgow.... | Thomas Graham..... | Pictou. |
| Foreign Built.... | John Livingston, Senr..... | Pictou. |
| Granville | Robert Graves..... | Halifax. |
| Seal Island..... | Edmund Crowell, et al. | Halifax. |
| Cornwallis..... | Stephen Stoddart, et al..... | Halifax. |
| Cornwallis..... | D. R. Eaton, et al..... | Windsor. |
| Maitland..... | William C. Barbrick, et al..... | Halifax. |
| Strait of Canso.... | David Gramby..... | Arischat. |
| Pictou..... | James Foote..... | Pictou. |
| Mahone Bay..... | James Moody, et al..... | Yarmouth. |
| Weymouth..... | Samuel Killam, et al..... | Yarmouth. |
| Parrsborough..... | J. P. Pellow..... | Parrsborough. |
| Cornwallis..... | George S. Brown, et al..... | Yarmouth. |
| Port Matoon..... | B. O. DeWolf, et al..... | Liverpool, N. S. |
| Sheet Harbor.... | Samuel Embree..... | Pictou. |
| New Glasgow.... | Peter Crichton..... | Pictou. |
| LaHave | J. Atkins, et al..... | Liverpool, N. S. |
| Lunenburg..... | Robert G. Noble, et al..... | Halifax. |
| Barrington..... | Charles DeLong..... | Halifax. |
| Pembroke..... | Samuel Clark, et al..... | Windsor. |
| P. E. Island..... | William Black, et al..... | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|---------------|-------|-------------|
| Boston | 35851 | Brigantine... | 96 | 1853 |
| Bounding Billow | 46396 | Brig | 234 | 1863 |
| Boundary ..... | ..... | Schooner | 140 | 1837 |
| Boxer | 48480 | Schooner | 8 | 1860 |
| Branch | 48451 | Brigantine... | 195 | 1864 |
| Branch ..... | ..... | Schooner | 15 | 1845 |
| Brave | 41142 | Schooner | 21 | 1856 |
| Bravo | 36446 | Schooner | 85 | 1859 |
| Bravo | 46471 | Schooner | 56 | 1864 |
| Bradorian | 38622 | Schooner | 17 | 1861 |
| Brazilian | 48433 | Barque | 396 | 1864 |
| Bravely ..... | ..... | Schooner | 35 | 1818 |
| Breeze | 42849 | Brigantine... | 227 | 1862 |
| Breeze ..... | ..... | Schooner | 16 | 1844 |
| Bridget | 38331 | Brigantine... | 112 | 1848 |
| Brinda | 42385 | Ship | 958 | 1863 |
| Bright Star | 36386 | Schooner | 18 | 1859 |
| Bridget Ann | 36531 | Schooner | 28 | 1863 |
| Brisk | 42210 | Schooner | 18 | 1850 |
| Brisk | 37002 | Schooner | 66 | 1860 |
| Brisk | 36082 | Brigantine... | 101 | 1857 |
| Britannia | 38437 | Schooner | 138 | 1864 |
| Britannia | 41788 | Schooner | 16 | 1859 |
| Britannia . . . . . | ..... | Schooner | 20 | 1815 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|-------------------------------|-------------------|
| Port Medway..... | George C. Harvey..... | Halifax. |
| Parrsborough..... | Samuel Strong, et al | Windsor. |
| Kempt..... | James Clark, et al | Halifax. |
| Newport | Benjamin Masters..... | Windsor. |
| Cornwallis..... | Ebenzer Bigelow, et al..... | Windsor. |
| Strait of Canso.... | Samuel Embree..... | Arichat. |
| Pugwash..... | William Orchard..... | Pictou. |
| Shelburne..... | Lewis P. Churchill..... | Shelburne. |
| Mahone Bay..... | John O'Brien | Halifax. |
| Little Bras d'Or, CB | Pascal LeBlanc..... | Sydney, C. B. |
| Walton..... | John Mosher, et al..... | Windsor. |
| Cape Breton..... | John Mullins..... | Halifax. |
| Five Mile River... | William McDougall, et al..... | Windsor. |
| LaHave | David Kiely | Halifax. |
| Strait of Canso.... | David Le Blanc, Jr..... | Arichat. |
| Maitland..... | Robert Bonk, Jr., et al | Halifax. |
| River Bourgeois... | Urbain Sampson, et al..... | Arichat. |
| Strait of Canso.... | James McKay, et al..... | Port Hawkesbury. |
| Ragged Island.... | J. McDonald, Jr..... | Liverpool, N. S. |
| Shelburne..... | Howard Locke..... | Shelburne. |
| Barrington | Isaac Nickerson, et al..... | Halifax. |
| Little Arichat.... | Charles King | Arichat. |
| LaHave | E. & J. Mullock.. | Lunenburg. |
| LaHave..... | Henry Henrity, et al.... | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------|------------------|---------------|-------|-------------|
| Britannia ..... | ..... | Schooner | 32 | 1829 |
| Britannia..... | ..... | Schooner | 41 | 1825 |
| Britannia ..... | ..... | Schooner | 11 | 1835 |
| Britannia ..... | ..... | Brig | 107 | 1838 |
| Britannia ..... | ..... | Schooner | 22 | 1834 |
| Britannia ..... | ..... | Schooner | 76 | 1840 |
| Britannia ..... | ..... | Schooner | 54 | 1841 |
| Britannia ..... | ..... | Schooner | 25 | 1836 |
| British Pearl ... | 37063 | Schooner | 77 | 1863 |
| British Crown | 36993 | Schooner | 70 | 1861 |
| British Crown..... | ..... | Schooner | 53 | 1842 |
| British Eagle | 35748 | Schooner | 44 | 1855 |
| British Hero | 34797 | Schooner | 40 | 1843 |
| British Lass | 41783 | Schooner | 29 | 1859 |
| British Lass | 36123 | Schooner | 24 | 1858 |
| British Lion | 46362 | Ship | 1279 | 1863 |
| British Pride | 50754 | Schooner | 39 | 1864 |
| British Queen | 37619 | Schooner | 20 | 1857 |
| British Queen | 37903 | Schooner ... | 61 | 1842 |
| British Queen | 42834 | Ship..... | 1195 | 1861 |
| British Queen | 35613 | Schooner | 54 | 1844 |
| British Queen | 42504 | Schooner | 27 | 1843 |
| British Queen | 52033 | Brig | 113 | 1865 |
| British Tar | 41775 | Schooner | 49 | 1853 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|-----------------------------------|-------------------|
| Mahone Bay..... | Henry Hawbolt..... | Halifax. |
| Grand Passage ... | James Clarke..... | Halifax. |
| LaHave | Valentine Shaffleburg, et al..... | Halifax. |
| Yarmouth..... | John Cann, et al..... | Halifax. |
| Lunenburg..... | Edward Dunn..... | Halifax. |
| Pugwash | William W. Bent, et al..... | Halifax. |
| Guysborough..... | Stephen Binney, et al..... | Halifax. |
| LaHave | Augh McEachern..... | Halifax. |
| Co. Guysborough.. | Joseph William Hadley..... | Guysborough. |
| Lunenburg..... | Philip Henritey, et al..... | Lunenburg. |
| Hall's Harbor..... | John Mosher, et al..... | Windsor. |
| New Dublin..... | Abraham Romkey, et al..... | Halifax. |
| Bay Verte | William Spence..... | Halifax. |
| LaHave | James Smith, et al..... | Lunenburg. |
| Shelburne..... | Joshua Snow..... | Halifax. |
| Windsor | Bennet Smith, et al..... | Windsor. |
| Torbay..... | Joseph Fougere..... | Halifax. |
| LaHave | Frederick Lohnes..... | Lunenburg. |
| Parrsborough..... | Samuel Clare, et al..... | Windsor. |
| Windsor..... | Bennet Smith, et al..... | Windsor. |
| Liscomb Harbor.. | John H. Murphy..... | Halifax. |
| Mahone Bay..... | Peter Ross..... | Halifax. |
| Granville | Peter Farnsworth, et al..... | Annapolis. |
| Broad Cove..... | William Evans, et al..... | Lunenburg. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Britain ..... | ..... | Schooner | 61 | 1846 |
| Brilliant ... .. | ..... | Brig | 99 | 1839 |
| Brilliant | 35608 | Brig..... | 190 | 1855 |
| Brilliant Star | 37601 | Schooner | 39 | 1857 |
| Brilliant Star | 35977 | Schooner | ... | 1856 |
| Bride | 49433 | Brigantine ... | 178 | 1864 |
| Bride | 34846 | Schooner | 36 | 1844 |
| Bride ..... | ..... | Schooner | 22 | 1844 |
| Brigand | 42795 | Schooner | 66 | 1862 |
| Brill | 37332 | Brig | 234 | 1857 |
| Brisk | 46484 | Schooner | 32 | 1865 |
| Brothers | 35742 | Schooner | 36 | 1847 |
| Brothers | 37712 | Brigantine ... | 108 | 1851 |
| Brothers ..... | ..... | Schooner | 47 | 1849 |
| Brothers | 41659 | Schooner | 39 | 1859 |
| Brothers | 37555 | Schooner | 33 | 1843 |
| Brothers ..... | ..... | Schooner | 22 | 1825 |
| Brothers | 39018 | Schooner | 43 | 1828 |
| Brothers | 35869 | Schooner | 26 | 1855 |
| Brothers | .. . | Schooner | 21 | 1855 |
| Brookville | 37635 | Schooner | 69 | 1856 |
| Brooklyn | 46289 | Brigantine ... | 138 | 1863 |
| B. Rogers .. | 49402 | Barque. | 576 | 1864 |
| Brunswick . | ..... | Schooner | 30 | 1831 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|------------------------------------|-------------------|
| New Brunswick . . . | Amos Seaman | Halifax. |
| Londonderry | Jacob B. Curry, et al. | Halifax. |
| St. Mary's | Isaac Nickerson, et al. | Halifax. |
| LaHave | J. & W Levesconte. | Arichat. |
| Broad Cove. | Stephen Smith, et al. | Halifax. |
| Shelburne. | James A. Starret, et al. | Yarmouth. |
| Granville | Reid McNeil. | Digby. |
| LaHave | Peter Bell. | Halifax. |
| Shelburne. | Lewis P. Churchill, et al. | Shelburne. |
| St. Mary's Bay. . . . | George McCrowell, et al. | Digby. |
| Mahone Bay. | Reuben Vienot, et al. | Lunenburg. |
| Wallace | John Hubert. | Arichat. |
| P. E. Island. | Abraham Gerroir. | Arichat. |
| P. E. Island. | James Curry | Pictou. |
| Bailey's Brook. . . . | John Curan | Pictou. |
| LaHave | A. Corkum, et al. | Lunenburg. |
| Clyde River. | Benjamin Perry. | Halifax. |
| Petite Riviere | James Cousins. | Halifax. |
| Mira Bay, C. B. . . . | William Kelly | Halifax. |
| St. Mary's | Joseph Bragg. | Halifax. |
| Parrsborough. | Charles Hatfield, et al. | Parrsborough. |
| Liverpool, N. S. . . . | L. Gardner, et al. | Liverpool, N. S. |
| Tusket | B. Rogers, et al. | Yarmouth. |
| New Brunswick . . . | Charles Tucker | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------|------------------|----------------|-------|-------------|
| Brunswick..... | 39088 | Schooner | 24 | 1850 |
| B. Smith..... | 42115 | Brigantine.... | 196 | 1861 |
| Buffalo..... | ..... | Brig..... | 112 | 1841 |
| Burmah..... | 22291 | Ship | 788 | 1853 |
| Busy Bee..... | ..... | Schooner | 16 | 1858 |
| Busy..... | ..... | Schooner | 32 | 1827 |
| Buskar..... | ..... | Schooner | 76 | 1846 |
| Butterfly..... | ..... | Schooner | 29 | 1831 |
| Butterfly..... | ..... | Schooner | 15 | 1844 |
| B. Wier..... | 41917 | Schooner | 32 | 1858 |
| Byron..... | ..... | Schooner | 52 | 1829 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|------------------------------|-------------------|
| New Brunswick... | Thomas Limmerton | Halifax. |
| Walton..... | John Mosher, et al,..... | Halifax. |
| Five Islands..... | Samuel Nelson..... | Halifax. |
| Hantsport..... | Ezra Churchill, et al. | Windsor. |
| Strait of Canso ... | Adolphus Emphree, Jr..... | Port Hawkesbury. |
| Shelburne..... | George Hewett..... | Halifax. |
| Port Medway.... | William Foster, Jr. | Halifax. |
| Canso | Edward Langley..... | Halifax. |
| Spry Harbor ... | John Gerrald. | Halifax. |
| Port Medway.... | J. Rose | Liverpool, N. S. |
| Liverpool, N. S. ... | Richard Prentes, et al. | Halifax. |

| Where built. | Registered Owner. | Port of Registry. |
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REGISTRY OF NOVA SCOTIA SHIPPING.

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When
built.

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
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| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|--------------------------|------------------|----------------|-------|----------------|
| Cabot | 36096 | Barque | 597 | 1861 |
| C. A. Jones | 42838 | Barque | 350 | 1861 |
| Cairo | 53561 | Brigantine ... | 316 | 1865 |
| Caledonia | 36892 | Schooner | 98 | 1860 |
| Caledonia | 34907 | Schooner | 40 | 1845 |
| California | 42325 | Schooner | 135 | 1849 |
| California ..... | ..... | Schooner | 72 | 1850 |
| Camilla ..... | ..... | Schooner | 57 | 1851 |
| Camilla ..... | ..... | Brig | 166 | 1839 |
| Cambridge ..... | ..... | Brig | 111 | 1843 |
| Campages | 35957 | Schooner | 47 | 1844 |
| Candour | 36628 | Schooner | 116 | 1861 |
| Canning | 48417 | Brigantine... | 229 | 1861 |
| Candidate | 36051 | Schooner | 20 | 1840 |
| Capella | 42798 | Schooner | 30 | 1862 |
| Caroline | 38285 | Brigantine... | 136 | 1850 |
| Caroline | 38348 | Barque | 179 | 1856 |
| Caroline | 37480 | Schooner | 25 | Prize, 1814 |
| Caroline ..... | ..... | Schooner | 31 | 1845 |
| Caroline | 37390 | Schooner | 18 | 1856 |
| Caroline | 37562 | Schooner | 30 | 1852 |
| Caroline | 37293 | Brigantine ... | 117 | 1847 |
| Caroline ..... | ..... | Schooner | 22 | 1834 |

| When built. | Where built. | Registered Owner. | Port of Registry. |
|----------------|-----------------------|------------------------------------|-------------------|
| 1861 | New Glasgow..... | James W. Carmichael..... | Pictou. |
| 1861 | Cornwallis | Charles Rufus Burgess, et al. | Windsor. |
| 1865 | Londonderry..... | John Geddes, Jr., et al. | Halifax. |
| 1860 | Antigonishe..... | Daniel Fraser, et al..... | Pictou. |
| 1845 | Londonderry | Duncan Urquhart, et al..... | Halifax. |
| 1849 | Sable River..... | Theodore Nickerson, et al..... | Halifax. |
| 1850 | St. Mary's | James Byrns, et al..... | Halifax. |
| 1851 | Little Loraine, C. B. | John Kennedy..... | Sydney, C. B. |
| 1839 | Yarmouth | George Allan, et al | Halifax. |
| 1843 | Kempt..... | James McNeill Parker | Halifax. |
| 1844 | Indian Harbor.... | James Cohoon | Halifax. |
| 1861 | Mira..... | James McDonald, et al | Sydney, C. B. |
| 1861 | Cornwallis..... | John Northup, et al..... | Windsor. |
| 1840 | Lunenburg | John Monk..... | Halifax. |
| 1862 | Barrington | Isaac Bayne, et al..... | Shelburne. |
| 1850 | Bras d'Or Lake... | Thomas LeNoir. | Arichat. |
| 1856 | Arichat | Isaac LeVesconte, et al | Arichat. |
| Prize, 1814 | Not known..... | D. Campbell | Liverpool, N. S. |
| 1815 | Liverpool | J. McKenzie | Liverpool, N. S. |
| 856 | Cape Canso | George Whitman..... | Guysborough. |
| 852 | La Have..... | Philip Ernst, et al..... | Lunenburg. |
| 847 | Digby | Charles Morehouse, et al..... | Digby. |
| 834 | Lunenburg..... | John E. Fairbanks, et al..... | Halifax. |

| Vessel's Name. | Official Number. | Rlg. | Tons. | When built. |
|----------------------------|---------------------|----------------|-------|----------------|
| Caroline..... | ..... | Schooner | 44 | 1846 |
| Caroline | 36121 | Schooner | 26 | 1843 |
| Caroline | 36278 | Schooner | 45 | 1834 |
| Caroline | 42360 | Schooner | 41 | 1863 |
| Carrie | 43059 | Schooner | 33 | 1862 |
| Carrie | 36270 | Schooner | 40 | 1859 |
| Carrie Wright | 49406 | Barque..... | 5-10 | 1864 |
| Carib..... | ..... | Schooner | 31 | 1851 |
| Carleton..... | ..... | Brigantine... | 83 | 1830 |
| Cassandra..... | ..... | Schooner | 46 | 1828 |
| Cassie Gray | 48101 | Brigantine ... | 176 | 1864 |
| Catherine | 38439 | Schooner ... | 20 | 1864 |
| Catherine..... | ..... | Schooner | 6 | 1840 |
| Catherine | 36918 | Schooner | 197 | 1861 |
| Catherine..... | ..... | Schooner | 21 | 1853 |
| Catherine | 36249 | Schooner | 90 | 1853 |
| Catherine | 37296 | Schooner | 34 | 1856 |
| Catherine | 42027 | Schooner ... | 3 | 1860 |
| Catherine | 42088 | Schooner | 101 | 1862 |
| Catherine..... | ..... | Schooner | 36 | 1823 |
| Catherine..... | ..... | Schooner | 15 | 1827 |
| Catherine..... | ..... | Brig..... | 145 | 1828 |
| Catherine..... | ..... | Schooner | 39 | 1833 |
| Catherine..... | ..... | Schooner | 45 | 1830 |

When
built.

1846

1843

1834

1863

1862

1859

1864

1851

1830

1828

1864

1864

1840

1861

1853

1853

1856

1860

1862

823

827

828

833

830

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|-------------------------------------|-------------------|
| Mahone Bay. | Frederick Ernst, et al. | Halifax. |
| Chester. | William Munroe | Halifax. |
| Argyle | Walter Larkin | Halifax. |
| Harbor au Bouche. | Joseph Brow | Halifax. |
| Tednish | Abraham A. Stevens, et al. | Pugwash. |
| Cape Canso. | Donald Scott | Halifax. |
| Hillsburg | S. E. Flint, et al | Yarmouth. |
| Shelburne. | J. Snow | Liverpool, N. S. |
| Shelburne | William B. Hamilton, et al. | Halifax. |
| Patridge Island . . . | John Young, et al | Halifax. |
| Moose River. | James A. Moren. | Halifax. |
| L'Ardoise. | A. Martell, et al. | Arichat. |
| Merigomish | William Manning, et al. | Pictou. |
| Pomquet. | Christopher McDonald. | Pictou. |
| Mahone Bay | James Hewett. | Lunenburg. |
| L'Ardoise, C. B. . . . | Hector McDonald | Sydney, C. B. |
| Clare | John V. LeBlanc. | Digby. |
| Clare | Joseph White. | Digby. |
| Gilbert's Cove | Robert M. Kelly | Yarmouth. |
| Lunenburg. | Frederick Zwicker | Halifax. |
| Lunenburg. | Maurice Power, et al. | Halifax. |
| Cape Breton. | Haliburton Grant, et al. | Halifax. |
| Tatamagouche | William Campbell, et al. | Halifax. |
| Indian Harbour. . . . | James Gillent. | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------|------------------|----------------|-------|-------------|
| Catherine ..... | ..... | Schooner | 18 | 1816 |
| Catherine ..... | ..... | Schooner | 11 | 1842 |
| Catherine | 36254 | Schooner | 28 | 1846 |
| Catherine Ann ..... | ..... | Schooner | 23 | 1851 |
| Catherine & Elizabeth .. | 38932 | Schooner | 45 | 1832 |
| Catherine Glenn ..... | ..... | Ship | 1327 | 1852 |
| Catherine Isabella ..... | ..... | Schooner | 99 | 1844 |
| Catherine Jane | 43067 | Barque | 373 | 1863 |
| Catherine Jane | ... | Schooner | 36 | 1850 |
| Catherine Morris | 49025 | Brigantine ... | 138 | 1865 |
| Catherine McGill ..... | ..... | Schooner | 54 | 1832 |
| Catherine White | 49101 | Barque | 298 | 1816 |
| Catalina | 36916 | Schooner | 22 | 1861 |
| C. C. Horton | 49430 | Ship | 775 | 1864 |
| Cecelia Ann | 36184 | Schooner | 11 | 1829 |
| Celecity | 36473 | Schooner | 67 | 1859 |
| Celeste | 42383 | Schooner | 68 | 1849 |
| Celia | 35990 | Barque | 264 | 1856 |
| Centenary | 38174 | Barque | 406 | 1861 |
| Centurion ..... | ..... | Schooner | 29 | 1843 |
| C. F. Eaton | 42839 | Brigantine ... | 296 | 1861 |
| Charlotte | 38898 | Schooner | 46 | 1847 |
| Charlotte | 37722 | Brigantine ... | 172 | 1851 |
| Charlotte ..... | ..... | Schooner | 11 | 1851 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|-------------------------------------|-------------------|
| Argyle | Christopher Shoales, Senr., et al.. | Halifax. |
| P. E. Island. . . . | John Christian, et al | Halifax. |
| Spry Harbor. | William Garrett. | Halifax. |
| Plaister Cove. | Peter D. Carteret, et al | Arichat. |
| Harbor Bank. | Alexander LeBlanc. | Arichat. |
| New Glasgow | George McKenzie. | Pictou, |
| Wallace | David McKay, et al. | Pictou. |
| New Glasgow | John Lowray, et al | Pictou. |
| Strait of Canso ... | Patrick Power. | Halifax. |
| Parrsborough. | Eratus Morris, et al. | Parrsborough. |
| Parrsborough. | Michael Michenen. | Halifax. |
| Newbury, U. S. . . . | Catherine White. | Pictou. |
| Cape George. | Neil McIsaac. | Pictou. |
| Yarmouth. | Dennis Horton, et al. | Yarmouth. |
| LaHave | William A. Brill. | Halifax. |
| East Medway. | H. Greaser, et al. | Lunenburg. |
| Hopewell | Matthew M. Bennett, et al. | Halifax. |
| Port Elgin, N. B. . | William B. Hamilton. | Halifax. |
| Yarmouth. | Young & Baker. | Yarmouth. |
| Mahone Bay. | Frederick Ernst. | Halifax. |
| Cornwallis. | Ebenzer H. Curry, et al. | Windsor. |
| Grand Ruisseau. . . | W. Palmer. | Arichat. |
| Pictou. | Benjamin Forest. | Arichat. |
| Little Arichat. | John Ferguson. | Arichat. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------------|------------------|----------------|-------|-------------|
| Charlotte | 38391 | Schooner | 27 | 1859 |
| Charlotte | 38427 | Schooner | 118 | 1862 |
| Charlotte..... | ..... | Brigantine ... | 242 | 1853 |
| Charlotte..... | ..... | Brigantine... | 119 | 1848 |
| Charlotte | 42010 | Schooner | 71 | 1859 |
| Charlotte..... | ..... | Schooner | 82 | 1832 |
| Charlotte..... | ..... | Schooner | 58 | 1819 |
| Charlotte..... | ..... | Schooner | 29 | 1815 |
| Charlotte..... | ..... | Schooner ... | 19 | 1818 |
| Charlotte..... | ..... | Schooner | 15 | 1815 |
| Charlotte Arnold | 46090 | Schooner | 72 | 1863 |
| Charlotte Geddie | 50800 | Barque..... | 497 | 1865 |
| Champion | 41629 | Brigantine ... | 291 | 1862 |
| Champion | 38895 | Schooner | 28 | 1847 |
| Charles | 38594 | Brigantine ... | 142 | 1857 |
| Charles | 36597 | Brigantine ... | 93 | 1854 |
| Charles..... | ..... | Schooner | 9 | 1839 |
| Charles..... | ..... | Brigantine ... | 114 | 1839 |
| Charles | 36048 | Schooner | 15 | 1838 |
| Charles | 36133 | Schooner .. | 27 | 1858 |
| Charles Albert | 48261 | Brigantine... | 200 | 1864 |
| Charles Adams | 37913 | Brigantine... | 136 | 1857 |
| Charles & Edward | 42420 | Barque..... | | 1864 |
| Charles J. Baker | 38252 | Ship | 552 | 1853 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|--------------------------------|-------------------|
| River Bougeois.... | Fobian Faugere..... | Arichat. |
| River John..... | Simon LeBlanc, et al..... | Arichat. |
| Clements | Joseph Heathcock..... | Pictou. |
| Weymouth | James Merritt..... | Digby. |
| Kempt..... | Colin Campbell, Junr..... | Digby. |
| Arichat..... | Milton Tomlinson..... | Windsor. |
| Unknown | William Kerr, et al..... | Halifax. |
| Grand Ruisseau... | John Bell..... | Halifax. |
| LaHave | Philip Greaves..... | Halifax. |
| Clare..... | Gilbert Van Embury, et al..... | Halifax. |
| Sable River..... | Lewis P. Churchill, et al..... | Shelburne. |
| New Glasgow.... | James William Carmichael..... | Halifax. |
| River John | Isidore Martell..... | Pictou. |
| Antigonish | James McPherson..... | Halifax. |
| Baddeck | Charles J. Campbell..... | Sydney, C. B. |
| Cornwallis..... | Absalom Smith..... | Windsor. |
| Clare..... | Gabriel Sournier..... | Halifax. |
| Truro | Isaac Ketchum..... | Halifax. |
| Clare..... | Amaziah Smith..... | Halifax. |
| Sheet Harbor.... | William Geddes..... | Halifax. |
| Shelburne..... | William Muir, et al..... | Shelburne. |
| Maitland | Christian Peterson, et al..... | Windsor. |
| Pugwash | Charles B. DeWolf, et al..... | Pugwash. |
| Yarmouth..... | Young & Baker | Yarmouth. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------|------------------|----------------|-------|-------------|
| Charles C. Foster..... | 37267 | Schooner | 61 | 1850 |
| Charles Gunn..... | 38219 | Barque..... | 352 | 1852 |
| Charles D. Horton..... | 37667 | Schooner | 107 | 1860 |
| Charles Henry..... | 35988 | Schooner | 33 | 1856 |
| Charles Phinney..... | 49011 | Schooner | 103 | 1864 |
| Charles Robert..... | 36127 | Schooner | 19 | 1855 |
| Charles Smith..... | 49020 | Brigantine ... | 123 | 1865 |
| Charles Tupper..... | 11816 | Schooner | 81 | 1865 |
| Charles Thomas..... | ..... | Schooner | 95 | 1833 |
| Challenge..... | ..... | Schooner | 29 | 1850 |
| Challenge..... | 36101 | Schooner | 91 | 1857 |
| Chance..... | ..... | Schooner | 46 | 1833 |
| Chance..... | ..... | Schooner | 23 | 1829 |
| Chance..... | ..... | Schooner | 42 | 1831 |
| Chance..... | ..... | Schooner | 9 | 1839 |
| Chance..... | ..... | Schooner | 19 | 1835 |
| Chancellor..... | 52013 | Barque..... | 541 | 1865 |
| Chanticleer..... | 36332 | Brig..... | 194 | 1859 |
| Chamois..... | 36302 | Schooner | 88 | 1860 |
| C. H. Dyer..... | 35065 | Schooner | 101 | 1852 |
| C. H. Sampson..... | 42300 | Brigantine ... | 103 | 1851 |
| Cherokee..... | 40001 | Brigantine ... | 83 | 1847 |
| Cherub..... | 38206 | Schooner | 26 | 1839 |
| Cherub..... | ... . | Schooner | 41 | 1847 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|-------------------------------|-------------------|
| Granville | Juen Wooste..... | Digby. |
| Salmon River..... | A. C. Robbins, et al..... | Yarmouth. |
| Parrsborough..... | Timothy Kelly, et al..... | Parrsborough. |
| Bay of Islands.... | Samuel Pyke, et al..... | Halifax. |
| Parrsborough..... | Robert Boyd, et al..... | Parrsborough. |
| Terence Bay..... | Edward J. Lordly.. .. | Halifax. |
| Parrsborough..... | Charles Smith, et al..... | Parrsborough. |
| Whycocomagh.... | Lauchlin McDougall..... | Baddeck. |
| New Brunswick... | James Elderkin..... | Halifax. |
| LaHave..... | James Bell..... | Lunenburg. |
| Sherbrooke..... | John Porteous, Junr..... | Halifax. |
| Lunenburg..... | T. S. Harding..... | Windsor. |
| Digby..... | Andrew Coggins..... | Halifax. |
| Clements | James Gate, Junr., et al..... | Halifax. |
| Clare..... | Jonis Robichau, et al..... | Halifax. |
| LaHave..... | Henry Pye, et al..... | Halifax. |
| Clyde River..... | James D. Coffin, et al..... | Shelburne. |
| LaHave..... | George H. Starr, et al..... | Halifax. |
| Shelburne..... | James A. Moren..... | Halifax. |
| Parrsborough..... | Robert Boyd..... | Parrsborough. |
| Bath, U. S..... | Thomas Winnett..... | Halifax. |
| United States..... | D. & J. Dunlop..... | Liverpool, N. S. |
| United States..... | N. Moses..... | Yarmouth. |
| Canso..... | James W. Bears, et al..... | Halifax. |

| Vessel's Name. | Official Number. | Rlg. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Cherub..... | ..... | Schooner | 27 | 1845 |
| Cherub..... | ..... | Schooner | 11 | 1849 |
| Chebucto | 42337 | Schooner | 38 | 1844 |
| Chebucto | 36323 | Brigantine ... | 113 | 1861 |
| Chester | 42333 | Schooner | 87 | 1862 |
| Chillian Walch | 49114 | Brigantine.... | 299 | 1865 |
| Chilion | 46479 | Brigantine ... | 133 | 1864 |
| Chilo | 50771 | Brigantine.... | 207 | 1864 |
| China | 37885 | Ship | 830 | 1855 |
| Chispa..... | .... | Schooner ... | 27 | 1846 |
| Chief | 42365 | Brig | 299 | 1863 |
| Chieftain | 50758 | Brigantine ... | 126 | 1864 |
| Chieftain | 35750 | Schooner | 87 | 1853 |
| Chronicle..... | ..... | Schooner | 63 | 1847 |
| Christine | 46906 | Barque | 383 | 1864 |
| Christiana | 39082 | Schooner | 32 | 1855 |
| Christy Ann ..... | ..... | Schooner | 20 | 1841 |
| Cinderella ..... | ..... | Schooner | 14 | 1846 |
| Clara | 50799 | Brigantine ... | 144 | 1865 |
| Clara Jane | 49100 | Schooner | 68 | 1864 |
| Clara Jenkins | 42090 | Schooner | 97 | 1862 |
| Clara | 36163 | Schooner | 84 | 1858 |
| Clare ..... | ..... | Schooner | 53 | 1829 |
| Clarissa Ann | 48457 | Schooner | 101 | 1864 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|-----------------------------|-------------------|
| Mahone Bay..... | Henry Boutilier..... | Halifax. |
| Londonderry..... | William Stuart, et al..... | Halifax. |
| LaHave..... | Ann Drummond..... | Halifax. |
| Richmond..... | John E. Cummins..... | Halifax. |
| Chester..... | Nathan Eisenhaur..... | Halifax. |
| Sheet Harbor..... | Charles T. Irving..... | Pictou. |
| Lunenburg..... | Lewis Anderson, et al..... | Lunenburg. |
| St. Ann's, C. B.... | Robert Boak, Junr..... | Halifax. |
| Windsor..... | Bennett Smith, et al..... | Windsor. |
| Halifax..... | John McDougall..... | Halifax. |
| Truro..... | John Taylor, et al..... | Halifax. |
| Sheet Harbor..... | Jacob B. Conrod..... | Halifax. |
| Sheet Harbor..... | John Anderson, et al..... | Halifax. |
| Parrsborough..... | Charles Eaton, et al..... | Windsor. |
| Strait of Canso... | James Forrestall..... | Port Hawkesbury. |
| P. E. Island..... | William Pryor..... | Halifax. |
| P. E. Island..... | William Weeks, et al..... | Arichat. |
| Holland Harbor... | Samuel Allan, et al..... | Halifax. |
| Truro..... | Robert Smith, et al..... | Halifax. |
| Tatamagouche.... | Archibald Campbell..... | Pictou. |
| St. Mary's Bay... | John Rice..... | Digby. |
| Pope's Harbor.... | John D. Cummins, et al..... | Halifax. |
| Clare..... | James A. Cook, et al..... | Halifax. |
| Cornwallis..... | John Arnold, Junr..... | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------------|------------------|----------------|-------|-------------|
| Clear | 35274 | Schooner | 31 | 1852 |
| Clear the Track | 36088 | Schooner | 12 | 1857 |
| Cleary | 42298 | Schooner | 35 | 1847 |
| Clan | 41915 | Schooner | 20 | 1858 |
| Cleo | 42351 | Brigantine... | 257 | 1863 |
| Cleta | 48131 | Brigantine... | 117 | 1864 |
| Clipper | 38367 | Schooner | 21 | 1857 |
| Clifton | 41541 | Schooner | 116 | 1858 |
| Clio | 42334 | Brigantine ... | 118 | 1862 |
| Clotile | 38378 | Schooner | 25 | 1858 |
| Clyde | 38134 | Ship | 719 | 1860 |
| Clyde | 46670 | Schooner | 110 | 1864 |
| Clyde | 42102 | Brigantine ... | 116 | 1860 |
| Clyde | ... | Schooner | 54 | 1840 |
| Clyde ..... | .... | Schooner | 50 | 1842 |
| Cobequid ..... | .... | Brig | 239 | 1852 |
| Collenton | 38296 | Schooner | 79 | 1848 |
| Colonel James Scott | 36908 | Barque | 384 | 1851 |
| Colin Campbell, Senr. | 36102 | Schooner | 43 | 1839 |
| Colin Campbell | 7493 | Brig | 169 | 1852 |
| Colombe | 38588 | Schooner | 11 | 1856 |
| Columbine | 37916 | Schooner | 68 | 1846 |
| Columbine ..... | .... | Schooner | 66 | 1832 |
| Coleraine ..... | .... | Schooner | 64 | 1850 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------------|-----------------------------------|-------------------|
| LaHave | John Conrod. | Lunenburg. |
| Jedore | Peter Webber, et al. | Halifax. |
| Mahone Bay. | Richard Harris. | Halifax. |
| Shelburne | W. McLean, et al. | Liverpool, N. S. |
| Londonderry. | John M. Blaikie, et al. | Halifax. |
| Richmond | Lovett Willett, et al. | Halifax. |
| Little Arichat | Abraham Forest. | Arichat. |
| Old Barns, (Col.) | Matthew C. Norris, et al. | Windsor. |
| Halifax | Benjamin Willett, et al. | Halifax. |
| Cheficamp | P. Robins & Co. | Arichat. |
| Clare | A. C. Robbins, et al. | Yarmouth. |
| Granville | Andros Bohakir, et al. | Digby. |
| Maitland | Charles A. Borey, et al. | Windsor. |
| Parrsborough. | James Dill, et al. | Halifax. |
| Hillsburgh. | Barlow McCall. | Halifax. |
| Truro | George Reading, et al. | Parrsborough. |
| Little Arichat | Archibald McDonald. | Arichat. |
| River John | Daniel Ward Job. | Pictou. |
| United States. | S. Morton | Liverpool, N. S. |
| Weymouth | Colin Campbell, Senr | Digby. |
| Miquelon. | Robert McCoy | Sydney, C. B. |
| Parrsborough. | Isaac Relfe, et al. | Windsor. |
| Shelburne | Robert Littlewood. | Halifax. |
| Cornwallis. | William Cochran, et al. | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|------------------|---------------|-------|-------------|
| Collector ..... | .... | Schooner | 76 | 1826 |
| Collector ..... | ..... | Schooner | 57 | 1832 |
| Collector ..... | ..... | Schooner | 21 | 1851 |
| Collingwood ..... | ..... | Schooner | 32 | 1847 |
| Commerce ..... | ..... | Schooner | 33 | 1841 |
| Commerce ..... | ..... | Brig..... | 150 | 1849 |
| Commerce (Steamer)..... | 48119 | Schooner | 278 | Unk'n |
| Comet | 37656 | Schooner | 97 | 1858 |
| Comet | 34751 | Schooner | 53 | 1843 |
| Comet | 36257 | Schooner | 30 | 1839 |
| Comet | 36350 | Schooner | 62 | 1861 |
| Commodore | 37574 | Schooner | 40 | 1856 |
| Combine..... | ..... | Schooner | 52 | 1836 |
| Combine | 37379 | Schooner | 48 | 1840 |
| Consort | 38454 | Schooner | 56 | 1863 |
| Conservative | 36533 | Schooner | 21 | 1863 |
| Conservative | 35614 | Schooner | 63 | 1842 |
| Constitution | 37472 | Brigantine... | 82 | 1854 |
| Consolation ..... | ..... | Schooner | 28 | 1814 |
| Continental | 46031 | Brigantine... | 216 | 1862 |
| Conquerall | 42414 | Schooner | 147 | 1853 |
| Conquest | 38211 | Brig | 191 | 1862 |
| Conquest | 37920 | Brigantine... | 114 | 1858 |
| Conquest | 35782 | Brigantine... | 129 | 1854 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|-------------------------------|-------------------|
| Barrington | John Kendrick, et al. | Halifax. |
| Argyle | Nathaniel Jenkins. | Halifax. |
| LaHave | John Hays | Halifax. |
| Sheet Harbor. | William Watt. | Halifax. |
| Margaree | Firman Cormier. | Arichat. |
| Cornwallis. | Elisha Card, et al. | Windsor. |
| Unknown | James F. Phelan. | Halifax. |
| Parrsborough | John T. Wylde, et al. | Halifax. |
| Granville | James Roy. | Digby. |
| Green Harbor | Charles H. Boucher. | Halifax. |
| Necum Teuch. | William Laug, et al. | Halifax. |
| LaHave | James Sarty, et al. | Lunenburg. |
| Ragged Island. . . | James Giffin, et al. | Halifax. |
| St. Mary's | David Reid. | Halifax. |
| Little Arichat. | Edward Le June, et al. | Arichat. |
| Margaree | Henry Taylor, et al. | Port Hawkesbury. |
| Jedore | John Henry Myers, et al. | Halifax. |
| Yarmouth. | Ryerson & Co | Yarmouth. |
| Yarmouth. | Joshua Ronkins. | Halifax. |
| Cornwallis | John Northup, et al. | Windsor. |
| LaHave | John McKeen, et al. | Lunenburg. |
| Tusket | Geo. S. Brown, et al. | Yarmouth. |
| Aylesford | James Mismiss, et al. | Windsor. |
| Barrington | George H. Starr, et al. | Halifax. |

| Vessel's Name. | O/ Num. | Rig. | Tons. | When built. |
|-------------------------|------------|----------------|-------|----------------|
| Concord..... | ..... | Schooner | 82 | 1828 |
| Concord..... | ..... | Brig | 72 | 1838 |
| Concord..... | ..... | Schooner | 39 | 1830 |
| Concord | 36326 | Schooner | 31 | 1845 |
| Condor | 36301 | Schooner | 81 | 1860 |
| Convoy | 35881 | Schooner | 15 | 1845 |
| Coquette | 41807 | Sloop..... | 8 | 1859 |
| Coquette | 35955 | Schooner | 22 | Unk'n |
| Cora | 46301 | Brigantine... | 308 | 1864 |
| Coral | 36148 | Schooner | 34 | 1860 |
| Coralla | 38657 | Schooner | 113 | 1865 |
| Cora Linn | 38022 | Barque..... | 392 | 1856 |
| Cordelia..... | ..... | Schooner | 27 | 1847 |
| Cordelia | 35704 | Schooner | 55 | 1854 |
| Cordelia | 48448 | Ship | 881 | 1864 |
| Cordelia..... | ..... | Schooner | 27 | 1846 |
| Cordelia | 36210 | Schooner | 19 | 1837 |
| Cordelia | .. . | Schooner | 16 | 1852 |
| Cordelia | 35959 | Schooner | 15 | 1856 |
| Cornicopia | 46901 | Schooner | 107 | 1863 |
| Coronella | 42003 | Schooner | 36 | 1858 |
| Coronella | 42845 | Brigantine ... | 141 | 1862 |
| Cornwallis | 46034 | Barque..... | 611 | 1862 |
| Cornwallis..... | ..... | Barque | 298 | 1848 |

| Where built. | Registered Owner. | Port of Registry. |
|-------------------|--------------------------------|-------------------|
| Shelburne..... | George H. Denstadt, et al..... | Halifax. |
| Newport..... | John Liswell, et al..... | Halifax. |
| Barrington..... | Obediah Wilson, et al..... | Halifax. |
| Sheet Harbor..... | William Hare..... | Halifax. |
| LaHave..... | Lewis Romkey..... | Halifax. |
| Cape Negro..... | Charles U. Wright, et al..... | Halifax. |
| Clements..... | Richard Clem, et al..... | Annapolis. |
| Unknown..... | Charles W. Wright..... | Halifax. |
| Liverpool..... | L. A. Sponagle, et al..... | Liverpool. |
| Jordan River..... | John B. Harding..... | Shelburne. |
| North Sydney..... | John Lewis Ingraham..... | Sydney, C. B. |
| Beaver River..... | N. Moses, et al..... | Yarmouth. |
| La Have..... | C. Corkum..... | Lunenburg. |
| Port Medway..... | Geo. Romkey, et al..... | Lunenburg. |
| Windsor..... | T. S. Harding, et al..... | Windsor. |
| La Have..... | Thomas Kiely..... | Halifax. |
| Lunenburg..... | Israel Wilson..... | Halifax. |
| Lunenburg..... | John Neforth..... | Halifax. |
| Lunenburg..... | John Boutilier..... | Halifax. |
| Maria Joseph..... | Angus Grant, et al..... | Port Hawkesbury. |
| Clements..... | Geo. B. Salis, et al..... | Digby. |
| Walton..... | John Mosher, et al..... | Windsor. |
| Cornwallis..... | Daniel Huntley, et al..... | Windsor. |
| Cornwallis..... | Samuel Chipman..... | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|------------------|----------------|-------|-------------|
| Cornelian | 35972 | Schooner | 28 | 1856 |
| Corira | 46390 | Brigantine ... | 209 | 1863 |
| Cotnain | 48456 | Schooner | 99 | 1864 |
| Countess Dalhousie.... | ..... | Schooner | 55 | 1833 |
| County of Pictou | 49116 | Ship | 683 | 1865 |
| Cragrowine | 46370 | Barque | 434 | 1863 |
| Crescent | 48032 | Brigantine ... | 254 | 1863 |
| Creole ..... | ..... | Schooner | 63 | 1850 |
| Crown..... | ..... | Schooner | 51 | 1835 |
| Crusade | 40009 | Schooner | 84 | 1853 |
| Cruiser | 37527 | Schooner | 40 | 1846 |
| Cruiser..... | ..... | Schooner | 36 | 1847 |
| Cuba | 48443 | Brig | 272 | 1864 |
| Cumberland | 49023 | Barque | 554 | 1865 |
| Cumberland | 42361 | Barque | 328 | 1863 |
| Curlew ..... | ..... | Schooner | 42 | 1839 |
| Curlew | 35815 | Schooner | 50 | 1840 |
| Curlew | 35912 | Schooner | 43 | 1852 |
| Curlew | 38926 | Schooner | 55 | 1851 |
| Cutter | 42800 | Schooner | 27 | 1862 |
| Cupid | 46006 | Brigantine ... | 292 | 1863 |
| C. W. Lyle | 41781 | Schooner | 31 | 1859 |
| C. Wood | 46368 | Schooner | 91 | 1863 |
| C. W. Wright | 33437 | Schooner | 66 | 1855 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|-------------------------------------|-------------------|
| La Have | Gasher Wenzel, et al | Halifax. |
| Newport | Terence Cochran, et al | Windsor. |
| Cornwallis | John W. Borden, et al | Windsor. |
| Shelburne | John L. Darrow | Halifax. |
| New Glasgow | George McKenzie. | Pictou. |
| Maitland | Alexander Donaldson | Windsor. |
| Granville | Samuel Hall, et al | Annapolis. |
| Sable River | Lewis P. Churchill, et al | Halifax. |
| Kempt | John Brown, et al. | Halifax. |
| Sable River | L. P. Churchill | Liverpool, N. S. |
| Chester | James Dauphinee, et al. | Halifax. |
| Mahone Bay | William Crooks | Halifax. |
| Cornwallis | Elijah West, et al | Windsor. |
| Parrsborough | Daniel Cox, et al. | Parrsborough. |
| Napan | Jonathan McCully, et al | Halifax. |
| Argyle | James R. Kenny, et al | Halifax. |
| Ship Harbor | John Hall, et al. | Halifax. |
| Shelburne | Isaac Nickerson, et al | Halifax. |
| Tatamagouche | Isaac Langelle, et al | Pictou. |
| Shelburne | Benjamin P. King | Shelburne. |
| Parrsborough | Patrick Blake | Parrsborough. |
| Lunenburg | J. & J. Sinclair | Lunenburg. |
| Advocate | Daniel B. Parker, et al. | Windsor. |
| St. Mary's | David Dizon | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------|------------------|----------------|-------|-------------|
| Cygnets | 37624 | Schooner | 30 | 1858 |
| Cygnets | 42229 | Brigantine ... | 194 | 1862 |
| Cypress | 42831 | Brigantine ... | 213 | 1861 |
| Cyrene | 42382 | Barque..... | 473 | 1863 |
| Cyruss | | Schooner | 15 | 1852 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
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| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
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| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------|------------------|---------------|-------|-------------|
| Daisy..... | 38259 | Brig | 337 | 1863 |
| Daisy..... | 46667 | Schooner | 11 | 1864 |
| Dancing Star..... | ..... | Schooner | 32 | 1853 |
| Daniel Yorke..... | 37659 | Schooner | 116 | 1859 |
| Daniel O'Connell..... | ..... | Schooner | 52 | 1839 |
| Daniel Gillaspie..... | 46010 | Schooner | 137 | 1863 |
| Daniel Dana..... | ..... | Schooner | 26 | 1827 |
| Daniel P. King..... | 36074 | Schooner | 49 | 1848 |
| Danish Princess..... | 38237 | Barque..... | 562 | 1863 |
| Dapper..... | ..... | Schooner | 22 | 1829 |
| Daring..... | 36989 | Schooner | 46 | 1852 |
| Daring..... | 36129 | Schooner | 26 | 1847 |
| Daring..... | 35896 | Schooner | 80 | 1845 |
| Daring..... | 42312 | Schooner | 106 | 1862 |
| Dart..... | 38638 | Schooner ... | 20 | 1863 |
| Dart..... | ..... | Schooner | 81 | 1851 |
| Dart..... | ..... | Schooner | 30 | 1835 |
| Dart..... | 38534 | Schooner | 69 | 1852 |
| Dart..... | 37528 | Schooner | 43 | 1854 |
| Dart..... | 36200 | Schooner | 8 | 1859 |
| Darling..... | 36218 | Schooner ... | 27 | 1859 |
| Dasher..... | 38205 | Schooner | 52 | 1862 |
| Dasher..... | 37326 | Schooner ... | 89 | 1857 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|----------------------------------|-------------------|
| Granville | George Ryerson | Yarmouth. |
| Gilbert's Cove | James W. Murphy | Digby. |
| Londonderry | Robert Jackson | Parrsborough. |
| Parrsborough..... | William Phinney, et al | Parrsborough. |
| Noel | Mark Spearing..... | Windsor. |
| Parrsborough..... | P. M. Cunningham, et al..... | Windsor. |
| Yarmouth. | John E. Scott | Halifax. |
| United States..... | John J. Woodin | Halifax. |
| St. John, N. B.... | W. Moody | Yarmouth. |
| Tatamagouche ... | Alexander Campbell, et al | Halifax. |
| Lunenburg. | Geo. Slauenwhite, et al..... | Lunenburg. |
| Cape Negro | George Lafoly..... | Halifax. |
| Halifax..... | Peter McPhee | Halifax. |
| Antigonishe.... | William Graham | Halifax. |
| North Sydney, C. B. | John A. Moore | Sydney, C. B. |
| Weymouth | Henry Grant..... | Digby. |
| Newport | Nelson Cardenbrook | Halifax. |
| Yarmouth..... | John Duffus, et al | Halifax. |
| Lunenburg | William Rose..... | Halifax. |
| Jedore | Charles E. Wiswell..... | Halifax. |
| Chester..... | Robert G. Noble, et al..... | Halifax. |
| Tusket Wedge.... | J. K. Ryerson | Yarmouth. |
| Granville | William Crosscup, et al..... | Digby. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Dasher | 35808 | Brigantine.... | 76 | 1845 |
| Dashaway | 36339 | Schooner | 78 | 1861 |
| Dancing Feather | 39159 | Schooner | 75 | 1854 |
| Dauntless..... | ..... | Schooner | 26 | 1838 |
| David Cameron | 43069 | Barque. | 577 | 1863 |
| David Henry | 38363 | Schooner | 21 | 1857 |
| David Higgins..... | ..... | Schooner | 74 | 1828 |
| David McNutt | 48436 | Barque. | 502 | 1864 |
| David..... | ..... | Schooner ... | 29 | 1846 |
| Day Spring | 43090 | Brigantine ... | 115 | 1863 |
| Day Spring | 48118 | Schooner | 19 | 1864 |
| Day Spring | 48440 | Brigantine.... | 175 | 1864 |
| Day Star | 46399 | Schooner | 45 | 1864 |
| Daylight..... | ..... | Schooner | 22 | 1814 |
| Daylight | 51779 | Schooner | 71 | 1865 |
| Dearborn | 38641 | Schooner | 132 | 1861 |
| Debel | 42289 | Schooner | 42 | 1861 |
| Debounaire | 37854 | Schooner | 95 | 1846 |
| Defiance | 37565 | Schooner | 35 | 1845 |
| Defiance | 37877 | Brigantine ... | 113 | 1856 |
| Defiance..... | ..... | Schooner | 21 | 1828 |
| Defiance..... | ..... | Schooner | 33 | 1830 |
| Defiance..... | ..... | Schooner | 43 | 1818 |
| Defiance..... | ..... | Schooner | 23 | 1836 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------------|-----------------------------------|-------------------|
| Yarmouth | Frederick McDonald | Halifax. |
| Sherbrooke | James L. Bremner | Halifax. |
| P. E. Island | Joseph Provost, et al. | Arichat. |
| Sable River | Philip Carton, et al | Halifax. |
| River John | Charles McLennan, et al. | Pictou. |
| Strait of Canso | Robert Noble & Sons | Arichat. |
| Truro | John Sanderson, et al. | Halifax. |
| Bedford | Nelson Chambers, et al. | Windsor. |
| Harbor au Bouche | Alexander Carpenter | Halifax. |
| New Glasgow | James Bayne | Pictou. |
| Jeddore | John Mitchell, et al. | Halifax. |
| Noel | Adam J. O'Brien, et al. | Windsor. |
| Horton | Otis Rathburn, et al. | Windsor. |
| Cape Breton | William McLeod | Halifax. |
| Port Mabou | John Campbell | Liverpool. |
| United States | J. D. Decelle | Sydney, C. B. |
| Mahone Bay | Martin Westhaver | Halifax. |
| Cornwallis | Robert Hunter, et al. | Windsor. |
| Lunenburg | Benjamin Rickard, et al. | Lunenburg. |
| Maitland | Robert Esdale, et al. | Windsor. |
| Shelburne | James Ryan, et al. | Halifax. |
| Magdalen Islands | Hallaway Hays | Halifax. |
| New Brunswick | John H. McEachran, et al. | Halifax. |
| La Have | Christian Smith, et al. | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------|------------------|----------------|-------|-------------|
| Defiance..... | ..... | Schooner | 26 | 1842 |
| Defiance..... | ..... | Schooner | 73 | 1846 |
| Defiance | 39032 | Schooner | 59 | 1838 |
| Deisped .. | 53555 | Brig | 291 | 1865 |
| Delight | 42505 | Schooner | 48 | 1862 |
| Delhi | 38059 | Brig | 305 | 1857 |
| Delhi | 36107 | Schooner | 19 | 1857 |
| Delta | 37921 | Schooner | 25 | 1858 |
| Delta | 36286 | Schooner | 15 | 1860 |
| Delta | 42357 | Schooner | 115 | 1863 |
| Dennis Horton | 38187 | Barque..... | 706 | 1861 |
| Desieda | 38368 | Schooner | 35 | 1849 |
| Despatch | 36028 | Brigantine ... | 135 | 1856 |
| Despatch | 46100 | Brigantine ... | 225 | 1863 |
| Despatch | 48100 | Schooner | 73 | 1864 |
| Despatch | 38306 | Schooner | 32 | 1855 |
| Deaux Auguste | 36181 | Schooner | 45 | Unk'n |
| Dezette | 42018 | Schooner | 31 | 1860 |
| Diadem | 46039 | Schooner | 107 | 1862 |
| Dielytris | 46483 | Schooner | 58 | 1865 |
| Digby Packet | 37213 | Schooner | 44 | 1845 |
| Digby | 37353 | Schooner .. | 55 | 1858 |
| Diligent | ... | Schooner | 36 | 1816 |
| Diligence..... | ..... | Schooner | 32 | 1830 |

| Where built. | Registered Owner. | Port of Registry. |
|-------------------|-------------------------------|-------------------|
| Mahone Bay..... | John Strachan..... | Halifax. |
| Granville | Elias Quereau, et al..... | Halifax. |
| Sheet Harbor..... | Jonas Balcan..... | Halifax. |
| Truro | John Dait, et al..... | Halifax. |
| Mahone Bay..... | William Zwickel..... | Lunenburg. |
| Meteghan..... | J. B. Stoneman..... | Yarmouth. |
| Country Harbor... | James W. Cleaveland..... | Halifax. |
| Walton..... | Robert Hazel, et al..... | Windsor. |
| Liscomb Harbor... | John Gillis, et al..... | Halifax. |
| Pompquet | Elisha W. Randall, et al..... | Halifax. |
| Yarmouth..... | Dennis Horton..... | Yarmouth. |
| L'Ardoise..... | Charles Boudroit..... | Arichat. |
| Barrington..... | Snow & Collins, et al..... | Liverpool, N. S. |
| Shelburne..... | Benjamin Crowley..... | Shelburne. |
| Chebogue | William Currier..... | Yarmouth. |
| Margaree | M. Blanshet, et al..... | Arichat. |
| Unknown | Patrick Tobin..... | Halifax. |
| St. Mary's Bay... | John Abbott, et al..... | Digby. |
| Cornwallis..... | William Bezanson, et al..... | Windsor. |
| Lunenburg..... | Charles Anderson, et al..... | Lunenburg. |
| Digby..... | Hugh McManus..... | Yarmouth. |
| Digby..... | William Watt, et al..... | Digby. |
| Harbor au Bouche. | George Coombs..... | Halifax. |
| Argyle..... | Lewis D'Entrement, et al..... | Halifax. |

| Vessel's Name. | Official Number. | Reg. | Tons. | When built. |
|-----------------------|------------------|----------------|-------|-------------|
| Dolphin | 38418 | Schooner | 36 | 1861 |
| Dolphin ..... | ..... | Brig | 176 | 1853 |
| Dolphin | 36506 | Schooner | 31 | 1860 |
| Dolphin | 37922 | Schooner | 13 | 1858 |
| Dolphin ..... | ..... | Schooner | 19 | 1821 |
| Dolphin ..... | ..... | Schooner | 41 | 1810 |
| Dolphin ..... | ..... | Schooner ... | 24 | 1835 |
| Dolphin ..... | ..... | Schooner | 42 | 1833 |
| Dolphin ..... | ..... | Schooner | 22 | 1843 |
| Dolphin | ... | Schooner | 16 | 1843 |
| Dolphin ..... | ..... | Schooner | 20 | 1846 |
| Dolphin .... | ..... | Schooner | 38 | 1842 |
| Dolphin ..... | ..... | Schooner | 14 | 1851 |
| Dolphin | 39275 | Schooner | 29 | 1847 |
| Dolphin | 38374 | Schooner | 23 | 1844 |
| Don Gola ..... | ..... | Schooner | 80 | 1847 |
| Don | 36033 | Brigantine ... | 79 | 1856 |
| Dorothy ..... | ..... | Schooner | 18 | 1849 |
| Dot | 38257 | Schooner | 40 | 1863 |
| Douglas ..... | ..... | Brigantine... | 98 | 1846 |
| Dove | 49028 | Brigantine ... | 259 | 1865 |
| Dove ..... | ..... | Schooner | 17 | 1852 |
| Dove | 38340 | Schooner | 30 | 1840 |
| Dove | 37463 | Schooner | 40 | 1845 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|------------------------------|-------------------|
| Cheticamp..... | Filbert Godet..... | Arichat. |
| Tednish..... | Roneldo Chappell..... | Pictou. |
| Liverpool..... | T. Lloyd..... | Liverpool, N. S. |
| Hantsport.... | John H. Harvie, et al..... | Windsor. |
| Digby..... | John Hays..... | Halifax. |
| Lunenburg..... | Jacob Tanner..... | Halifax. |
| Bay of Islands.... | George Harvey..... | Halifax. |
| Harbor au Bouche. | John Thornburn, et al..... | Halifax. |
| Pubnico.... | Eli Arnero, et al..... | Halifax. |
| La Have..... | J. Fogarty..... | Halifax. |
| Antigonish..... | George Wootten..... | Halifax. |
| Jeddore..... | James Murphy..... | Halifax. |
| Torbay..... | Peter Boudroit..... | Halifax. |
| Three Rivers, P.E.I. | John McNeil..... | Halifax. |
| Janvrin, C. B..... | James Stoddart..... | Halifax. |
| Granville..... | John H. Crosseup, et al..... | Halifax. |
| Pope's Harbor.... | John Esson, et al..... | Halifax. |
| Tednish..... | James Chappell..... | Pictou. |
| Westport..... | Ryerson & Co..... | Yarmouth. |
| Maitland..... | Adam Douglas, et al..... | Halifax. |
| Economy..... | David P. Soley, et al..... | Parrsborough. |
| Strait of Canso.... | John Reeves..... | Arichat. |
| Cornwallis..... | George H. Bissett..... | Arichat. |
| La Have..... | S. Delisser, et al..... | Liverpool, N. S. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------------------|------------------|---------------|-------|-------------|
| Dove | 37052 | Schooner | 18 | 1860 |
| Dove | 37264 | Schooner | 14 | 1855 |
| Dove ..... | ..... | Schooner | 19 | 1828 |
| Dove ..... | ..... | Schooner | 23 | 1819 |
| Dove ..... | ..... | Schooner | 22 | 1830 |
| Dove ..... | ..... | Schooner | 79 | 1819 |
| Dove ..... | ..... | Schooner | 41 | 1817 |
| Dove ..... | ..... | Schooner | 5 | 1839 |
| Dove ..... | ..... | Schooner | 34 | 1839 |
| Dove ..... | ..... | Schooner | 29 | Unk'n |
| Dove ..... | ..... | Schooner | 17 | 1847 |
| Dove ..... | ..... | Schooner | 26 | 1836 |
| Dove | 35173 | Schooner | 32 | 1839 |
| Dragon (Steamer) | 43084 | Barque..... | 59 | 1863 |
| D. R. DeWolf | 37822 | Barque... .. | 232 | 1853 |
| Dream | 53562 | Schooner | 31 | 1865 |
| Dreadnought | 53901 | Schooner | 10 | 1865 |
| Dromo ..... | ... . | Schooner | 19 | 1839 |
| Drudee ..... | ..... | Schooner | 16 | 1837 |
| Duireland | 50774 | Barque..... | 528 | 1847 |
| Dunkeld | 43065 | Barque..... | 378 | 1862 |
| Dundanah | 49109 | Schooner | 71 | 1865 |
| Dundee | 46022 | Schooner ... | 125 | 1862 |
| Duncan | 52053 | Barque..... | 452 | 1865 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|--------------------------------------|-------------------|
| Guysborough | Herbert R. Cunningham | Guysborough. |
| Clementsport | Susan Winchester | Digby. |
| Lunenburg | John Park | Halifax. |
| Lunenburg | Joseph Murphy | Halifax. |
| Economy | John Gourlay | Halifax. |
| New Brunswick . . . | Thomas Curry | Halifax. |
| La Have | Thomas Irvine | Halifax. |
| Wilmot | Hanly Starrat, et al | Halifax. |
| Cornwallis | Mayhew Beckwith | Halifax. |
| Unknown | Jeremiah Spinney, et al | Halifax. |
| Chezzetcooke | Duncan Waddell, et al | Halifax. |
| Yarmouth | Andrew Spinney, et al | Halifax. |
| Cornwallis | William Gormley, et al | Halifax. |
| Miramichi | General Mining Association | Pictou. |
| Hantsport | G. P. Payzant, et al | Windsor. |
| Newdiquoddy . . . | John B. Fay | Halifax. |
| Parrsborough | John Williger, Junr | Parrsborough. |
| Shelburne | John Matthews Dall | Halifax. |
| Country Harbor . . . | Joseph P. Bickers | Halifax. |
| Zwiker, Holland . . . | William J. Fraser | Halifax. |
| New Glasgow | Donald McDonald, et al | Pictou. |
| Tatamagouche | Charles Reilly | Pictou. |
| Parrsborough | John Loomer, et al | Windsor. |
| Maitland | James McDougall, et al | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------|------------------|--------------|-------|-------------|
| Dun Robbin..... | ..... | Schooner ... | 8 | 1836 |
| Durago..... | ..... | Schooner ... | 20 | 1836 |
| Durham..... | 42352 | Schooner ... | 29 | 1840 |
| Dusky Laske..... | 42213 | Schooner ... | 44 | 1861 |
| D. Grant..... | 42359 | Schooner ... | 116 | 1863 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

E.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Eagle..... | ..... | Schooner | 29 | 1847 |
| Eagle | 42511 | Schooner | 15 | 1861 |
| Eagle | 38015 | Brig | 254 | 1856 |
| Eagle | 37894 | Schooner | 65 | 1848 |
| Eagle | 37290 | Brigantine ... | 91 | 1856 |
| Eagle | 42084 | Brigantine ... | 213 | 1861 |
| Eagle | 42118 | Schooner | 32 | 1861 |
| Eagle..... | ..... | Schooner | 36 | 1831 |
| Eagle..... | ..... | Schooner | 75 | 1836 |
| Eagle | 35883 | Schooner | 33 | 1847 |
| Eagle | 37453 | Schooner | 40 | 1855 |
| Eagle | 42321 | Schooner | 19 | Unk'n |
| Eagle | 42327 | Schooner | 77 | Unk'n |
| E. A. Nickerson | 37016 | Schooner | 16 | 1865 |
| Earl Mulgrave | 41812 | Schooner | 79 | 1858 |
| Eastern Star | 36207 | Schooner | 42 | 1859 |
| E. Bigelow | 46364 | Brigantine ... | 134 | 1863 |
| Elro | 37685 | Brigantine .. | 68 | 1862 |
| Echo | 37445 | Brigantine ... | 33 | 1836 |
| Echo | 37537 | Brigantine.... | 38 | 1846 |
| Echo | 38201 | Brig | 265 | 1862 |
| Echo | 34740 | Schooner | 36 | 1855 |
| Eclipse | 37662 | Schooner | 99 | 1860 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|----------------------------------|-------------------|
| Mahone Bay..... | G. Eisenhaur, et al | Lunenburg. |
| La Have..... | J. G. Richard..... | Lunenburg. |
| Yarmouth..... | Gilbert Sanderson, et al..... | Yarmouth. |
| Digby..... | William G. Anthony, et al..... | Digby. |
| Clare | Ambrose Comeau, et al | Digby. |
| Hillsburgh..... | Alpheus Marshul, et al | Digby. |
| Cornwallis..... | Wellington Eagles, et al | Windsor. |
| Indian Harbor.... | Thomas Latter..... | Halifax. |
| New Brunswick... | William Edward Smith, et al | Halifax. |
| La Have..... | Neil Bolong | Halifax. |
| Eagle Head..... | Neil Bolong | Halifax. |
| State of Maine, U.S. | William Hall, Junr | Halifax. |
| United States..... | Edward G. Stayner, et al | Halifax. |
| Barrington | Edward H. Nickerson | Shelburne. |
| Baddeck..... | Charles J. Campbell | Baddeck. |
| Isaac's Harbor.... | Duncan McMillan, et al | Halifax. |
| Cornwallis | Ebenezer Bigelow..... | Windsor. |
| Minudie | Rufus Seaman..... | Parrsborough. |
| Shelburne..... | J. Democitu, et al..... | Liverpool, N. S. |
| La Have | G. W. Richardson..... | Lunenburg. |
| Beaver River... | Aaron Goudry, et al | Yarmouth. |
| Wilnot..... | Amos Patterson..... | Digby. |
| Parrsborough..... | Patrick Blake | Parrsborough. |

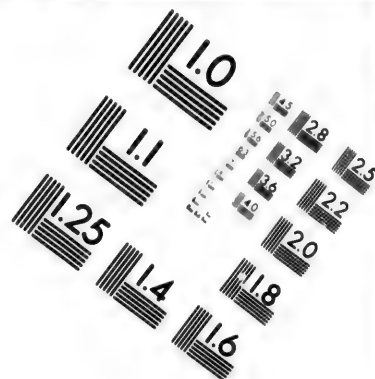
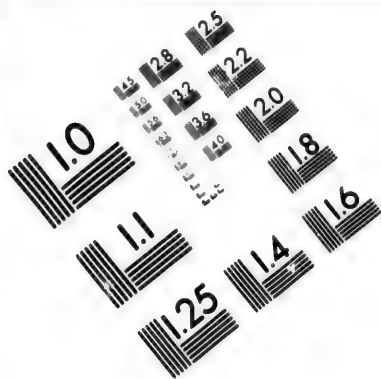
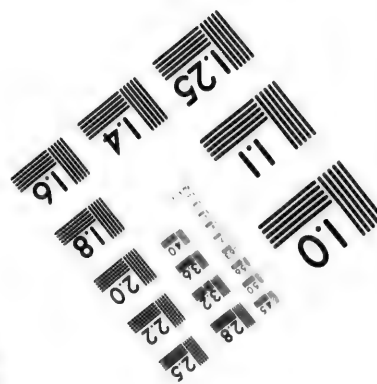
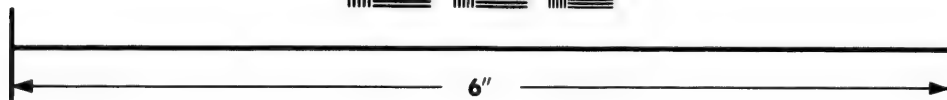
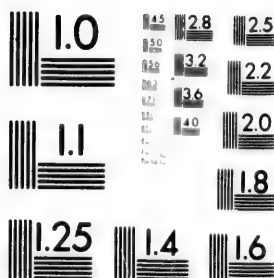


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| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------------|------------------|----------------|-------|-------------|
| Eclipse | 38573 | Brig | 137 | 1854 |
| Eclipse ..... | ..... | Brig | 195 | 1840 |
| Eclipse | 37867 | Schooner | 37 | 1833 |
| Eclipse | 35615 | Schooner | 129 | 1854 |
| Eclipse | 35615 | Schooner | 109 | 1854 |
| Economy ..... | ..... | Schooner | 21 | 1840 |
| Eda | 49453 | Brigantine ... | 243 | 1864 |
| Edith | 48141 | Schooner ... | 121 | 1864 |
| Edmund A. Sonder | 38216 | Barque. | 429 | 1862 |
| Edmiston Brothers | 38014 | Brig | 340 | 1856 |
| Edwin ..... | ..... | Schooner | 35 | 1841 |
| Edward | 30383 | Brigantine... | 182 | Unk'n |
| Edward | 38432 | Schooner | 91 | 1863 |
| Edward Albro | 49087 | Barque. | 394 | 1864 |
| Edward A. Lond | 39298 | Brigantine ... | 140 | 1856 |
| Edward & Amelia ..... | ..... | Brig | 113 | 1832 |
| Edward Hincken | 38256 | Barque. | 546 | 1863 |
| Edward & Mary ..... | ..... | Schooner | 31 | 1831 |
| Edward Walker ..... | ..... | Brigantine ... | 118 | 1831 |
| Effort ..... | ..... | Schooner | 28 | 1852 |
| Egeta | 48139 | Brigantine... | 142 | 1864 |
| Eight Brothers ..... | ..... | Schooner | 23 | 1840 |
| Eight Sisters ..... | ..... | Schooner | 20 | 1815 |
| Eigar Bachelor | 48039 | Schooner | 97 | 1864 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|---------------------------------------|-------------------|
| P. E. Island | Thomas Townsend. | Sydney, C. B. |
| Newport. | T. S. Harding, et al. | Windsor. |
| New Brunswick . . . | John McNeil, et al. | Halifax. |
| La Have | James G. A. Creighton. | Halifax. |
| La Have | James G. A. Creighton. | Halifax. |
| Granville | Joshua Brinton, et al. | Halifax. |
| Amherst | Elisha Embree, et al. | Pugwash. |
| Londonderry | Samuel D. Spencer, et al. | Halifax. |
| St. Mary's Bay. . . . | Ryerson & Moses, et al. | Yarmouth. |
| Yarmouth. | Thomas Killam, et al. | Yarmouth. |
| L'Ardoise | G. H. Bissett | Arichat. |
| Hampden, U. S. . . . | N. K. Clements. | Yarmouth. |
| Arichat. | Edward Bahin. | Arichat. |
| Pictou. | James Kitchen. | Pictou. |
| Hillsburg | Alpheus Marshall, et al. | Digby. |
| Digby. | William Taylor | Halifax. |
| Tusket. | J. W. Lovett, et al. | Yarmouth. |
| Yarmouth | Edward Crossby, Junr., et al. | Halifax. |
| Shubenacadie. . . . | John Tremain | Halifax. |
| Sheet Harbor. . . . | John Coleman Keily. | Guysborough. |
| Guysborough | John W. Young, et al. | Halifax. |
| Musquodoboit. . . . | Leonard Gaetaz | Halifax. |
| Jeddore | Nicholas Seffer. | Halifax. |
| Wilmot. | John Harris, et al. | Annapolis. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|---------------------------|------------------|----------------|-------|-------------|
| E. K. Brown | 36156 | Schooner | 50 | 1860 |
| Elba | 37272 | Brigantine.... | 91 | 1855 |
| Elba | 42110 | Brig | 156 | 1860 |
| Eldorado | 52016 | Barque | 280 | 1865 |
| Eleanor ..... | ..... | Schooner | 27 | 1835 |
| Eleanor Jane ..... | ..... | Schooner | 49 | 1819 |
| Eleabthona | 38600 | Schooner | 27 | 1865 |
| Elegant ... .. | ..... | Schooner | 41 | 1831 |
| Electra | 35719 | Schooner | 39 | 1848 |
| Elgin | 49107 | Brigantine.... | 321 | 1865 |
| Elizabeth | 35656 | Schooner | 32 | 1843 |
| Elizabeth | 37717 | Schooner | 66 | 1842 |
| Elizabeth | 38335 | Schooner | 17 | 1856 |
| Elizabeth | 38364 | Schooner | 21 | 1857 |
| Elizabeth | 38405 | Schooner | 115 | 1860 |
| Elizabeth | 41141 | Schooner | 23 | 1856 |
| Elizabeth ..... | ..... | Schooner | ... | 1838 |
| Elizabeth ..... | ..... | Schooner | 26 | 1836 |
| Elizabeth ..... | ..... | Schooner | 78 | 1852 |
| Elizabeth | 36162 | Schooner | 58 | 1848 |
| Elizabeth | 37247 | Schooner | 107 | 1855 |
| Elizabeth .... | ..... | Schooner | 33 | 1799 |
| Elizabeth ..... | ..... | Brig..... | 134 | 1821 |
| Elizabeth ..... | ..... | Schooner | 28 | 1833 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|-----------------------------------|-------------------|
| Port Medway..... | S. E. Barss, et al. | Liverpool, N. S. |
| St. Mary's Bay.... | Lewis McDonald. | Digby. |
| Cornwallis | Joseph Steele, et al. | Windsor. |
| Shelburne..... | Jacob Locke, et al. | Shelburne. |
| La Have | Nathaniel Smith, et al. | Halifax. |
| Londonderry..... | William Burgher. | Halifax. |
| Big Pond, C. B. . . | John Cox. | Sydney, C. B. |
| Digby..... | Elkanah Trask, et al. | Halifax. |
| Petite Riviere.... | Frederick Publicover, et al. | Halifax. |
| Pictou..... | Alexander Thomson, et al. | Pictou. |
| Arichat..... | Peter Landry, et al. | Arichat. |
| Little Arichat.... | J. & W. LeVesconte. | Arichat. |
| River Bourgeois.. | Basil Bouche. | Arichat. |
| Pompquet..... | Cyril Beneva. | Arichat. |
| Arichat..... | Abraham Lavashe. | Arichat. |
| Pictou..... | John Skinner. | Pictou. |
| Yarmouth..... | John McCormie. | Liverpool, N. S. |
| Guysborough..... | James Fitzgerald. | Guysborough. |
| Baddeck..... | John Graham, et al. | Sydney, C. B. |
| Christmas Island.. | Michael Kennedy | Sydney, C. B. |
| Clements | Ezekiel Crosseup, et al. | Digby. |
| Lunenburg | James Heed. | Halifax. |
| Pompquet..... | William Irish. | Halifax. |
| Mary Joseph..... | Thomas Smith. | Halifax. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|--------------------------------|------------------|---------------|-------|-------------|
| Elizabeth ..... | ..... | Schooner | 27 | 1834 |
| Elizabeth ..... | ..... | Schooner | 31 | 1835 |
| Elizabeth | .. | Schooner | 24 | 1830 |
| Elizabeth ..... | ..... | Schooner | 29 | 1825 |
| Elizabeth ..... | ..... | Schooner | 13 | 1820 |
| Elizabeth ..... | ..... | Schooner | 18 | 1836 |
| Elizabeth ..... | ..... | Schooner | 51 | 1831 |
| Elizabeth ..... | ..... | Schooner | 17 | 1836 |
| Elizabeth ..... | ..... | Schooner | 9 | 1838 |
| Elizabeth ..... | ..... | Schooner | 50 | 1832 |
| Elizabeth ..... | ..... | Schooner | 22 | 1840 |
| Elizabeth ..... | ..... | Schooner | 68 | 1843 |
| Elizabeth ..... | ..... | Schooner | 16 | 1824 |
| Elizabeth | 37492 | Schooner | 19 | 1833 |
| Elizabeth | 35715 | Schooner | 43 | 1853 |
| Elizabeth | 36022 | Schooner | 56 | 1854 |
| Elizabeth | 38463 | Schooner | 29 | 1865 |
| Elizabeth Ann ..... | ..... | Schooner | 12 | 1829 |
| Elizabeth Ann | ... | Schooner | 17 | 1838 |
| Elizabeth Bowlby ..... | ..... | Schooner | 105 | 1854 |
| Elizabeth Cann | 38045 | Barque | 495 | 1857 |
| Elizabeth Ellen | 41904 | Schooner | 28 | 1844 |
| Elizabeth Graham | 36330 | Schooner | 100 | 1864 |
| Elizabeth Jenkins | 38214 | Barque | 667 | 1862 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------------|--|-------------------|
| Manchester | Joseph Hadley | Halifax. |
| Merigomish | John Simpson, et al | Halifax. |
| Tatamagouche | Henry Dwyer | Halifax. |
| Pictou | William Creighton | Halifax. |
| Liverpool | Samuel Foster, et al | Halifax. |
| Argyle | Prosper Surratt, et al | Halifax. |
| New Brunswick | John Homer Doane | Halifax. |
| Jordan River | Lauchlin McPherson | Halifax. |
| Long Island | Thomas Parry | Halifax. |
| Pictou | George Barron | Halifax. |
| New Harbor | William James Munroe | Halifax. |
| St. Mary's | Thomas O'Dell, et al | Halifax. |
| Clare | William Cunningham, Senr. | Halifax. |
| Barrington | Elisha Hopkins, Senr., et al | Halifax. |
| La Have | Daniel H. Pitts | Halifax. |
| Gabarous, C. B. | Samuel Foreman | Halifax. |
| River Bourgeois | William Urquhart | Arichat. |
| Kempt | John Johnston | Halifax. |
| Yarmouth | John B. White, et al | Halifax. |
| Wilmot | Adam Bowlby, et al | Digby. |
| Yarmouth | Symon Cann, et al | Yarmouth. |
| Strait of Canso | Charles Delory, et al | Port Hawkesbury. |
| Antigonish | Annie Langlois, et al | Halifax. |
| Beaver River | A. F. Stoneman, et al | Yarmouth. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Elizabeth Ryan | 38653 | Schooner | 122 | 1864 |
| Eliza | 49461 | Brigantine... | 139 | 1865 |
| Eliza | 38925 | Schooner | 51 | 1842 |
| Eliza | 38435 | Schooner | 28 | 1864 |
| Eliza ..... | ..... | Schooner | 28 | 1847 |
| Eliza | 37051 | Schooner | 99 | 1860 |
| Eliza | 38575 | Brigantine... | 116 | 1856 |
| Eliza | 37259 | Schooner | 18 | 1849 |
| Eliza | 42083 | Brigantine ... | 212 | 1861 |
| Eliza ..... | ..... | Schooner | 110 | 1832 |
| Eliza ..... | ..... | Schooner | 46 | 1834 |
| Eliza ..... | ..... | Schooner | 24 | 1844 |
| Eliza ..... | ..... | Schooner | 14 | 1834 |
| Eliza ..... | ..... | Schooner | 83 | 1848 |
| Eliza | 35992 | Schooner | 15 | 1844 |
| Eliza | 35926 | Schooner | 21 | 1850 |
| Eliza | 36333 | Schooner | 31 | 1860 |
| Eliza ..... | ..... | Schooner | 24 | 1848 |
| Eliza | 49491 | Schooner | 104 | 1865 |
| Eliza Ann ..... | ..... | Schooner | 59 | 1829 |
| Eliza Ann ..... | ..... | Schooner | 31 | 1828 |
| Eliza Ann | 35954 | Schooner | 28 | 1850 |
| Eliza Ann ..... | ..... | Schooner | 48 | 1831 |
| Eliza DeWolf | 46292 | Brigantine... | 183 | 1863 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------------|-----------------------------------|-------------------|
| Gabarouse | Philip Bagnell. | Sydney, C. B. |
| Tidnish | James W. Delaney, et al. | Pugwash. |
| D'Escouse | Louis Young, et al. | Arichat. |
| Grand River | Hector Murchison, et al. | Arichat. |
| Magdalen Islands. | Peter Power | Arichat. |
| Guysborough | Duncan McDonald | Pictou. |
| Baddeck | Charles J. Campbell | Guysborough. |
| Cornwallis | Hayden Guptell. | Sydney, C. B. |
| Sandy Cove. | Jonas Morehouse, et al. | Digby. |
| New Brunswick. | Richard W. Card. | Digby. |
| Arichat. | Michael Kennedy | Halifax. |
| Barrington | Richard Smith, et al. | Halifax. |
| Port Mutton. | Jacob Pandy | Halifax. |
| La Have | George Walker. | Halifax. |
| Little River. | John Ehler | Halifax. |
| Liscomb Harbor. | George Wambolt. | Halifax. |
| Pompquet. | Ephraim Doyson, et al. | Halifax. |
| Gut of Canso. | John McDougall, et al. | Halifax. |
| St. Mary's Bay | William C. Warner. | Digby. |
| Lunenburg. | Frederick Slaughenwhite | Halifax. |
| Lunenburg. | George Peete, et al. | Halifax. |
| Musquodoboit. | Simon Pebrine. | Halifax. |
| Barrington | Josiah Coffin | Halifax. |
| Sable River. | J. DeWolf, et al. | Liverpool, N. S. |

| Vessel's Name. | Official Number. | Rlg. | Tons. | When built. |
|-------------------------------|------------------|----------------|-------|-------------|
| Eliza Catherine | 38551 | Schooner | 88 | 1855 |
| Eliza Ellen | 37221 | Schooner | 29 | 1848 |
| Eliza Hooper | 35927 | Schooner | 50 | 1855 |
| Eliza Jane | 37059 | Schooner | 46 | 1861 |
| Eliza Jane ..... | ..... | Schooner | 39 | 1821 |
| Eliza Jane | 35751 | Schooner | 84 | 1854 |
| Eliza Mary | 38554 | Schooner | 21 | 1852 |
| Eliza McLaughlin | 38179 | Ship | 684 | 1861 |
| Eliza Susan ..... | ..... | Brig..... | 152 | 1850 |
| Eliza A. Wilson | 48263 | Schooner | 41 | 1864 |
| Eliza Young | 38078 | Barque | 530 | 1858 |
| Elk ..... | ..... | Schooner | 34 | 1838 |
| Ellen ..... | ..... | Schooner ... | 35 | 1843 |
| Ellen ..... | ..... | Schooner | 15 | 1850 |
| Ellen ..... | ..... | Schooner | 16 | 1852 |
| Ellen ..... | ..... | Brigantine ... | 122 | 1854 |
| Ellen ..... | ..... | Schooner | 88 | 1858 |
| Ellen ..... | ..... | Schooner | 20 | 1849 |
| Ellen ..... | ..... | Schooner | 42 | 1853 |
| Ellen ..... | ..... | Brig | 252 | 1858 |
| Ellen ..... | ..... | Schooner | 25 | 1845 |
| Ellen ..... | ..... | Schooner | 61 | 1837 |
| Ellen ..... | ..... | Schooner | 41 | 1846 |
| Ellen ..... | ..... | Schooner | 22 | 1846 |

| When built. | Where built. | Registered Owner. | Port of Registry. |
|-------------|--|----------------------------------|-------------------|
| 1855 | Louisburg | Joseph Martell. | Sydney, C. B. |
| 1848 | Yarmouth | Michael Meritt, et al | Digby. |
| 1855 | Forche | William H. McAlpine, et al. | Halifax. |
| 1861 | Tracadie, C. B. | John Givinor, et al | Guysborough. |
| 1821 | Grand Passage ... | Duncan McMillan, et al | Halifax. |
| 1854 | Guysborough | Thomas Walsh, et al. | Halifax. |
| 1852 | Newfoundland | Alexander McKenzie | Sydney, C. B. |
| 1861 | Pleasant Cove | A. C. Robbins, et al | Yarmouth. |
| 1850 | Little Bras d'Or, CB Benjamin Carr | | Sydney, C. B. |
| 1864 | Barrington | Israel Wilson, et al ... | Shelburne. |
| 1858 | Beaver River. | John Young, et al | Yarmouth. |
| 1838 | Ragged Island | Andrew Devine, et al | Halifax. |
| 1843 | Cheticamp | Joseph Chassar, et al. | Arichat. |
| 1850 | River Bourgeois ... | G. H. Bissett. | Arichat. |
| 1852 | River Bourgeois ... | G. E. Bissett. | Arichat. |
| 1854 | P. E. Island. | Louis Boudroit | Arichat. |
| 1858 | Pompquet | Alexander Boudroit, et al. | Arichat. |
| 1849 | River John. | Joseph Boudroit | Pictou. |
| 1853 | Pugwash | George McKenzie. | Pictou. |
| 1858 | Meteghan | John W. Lovett, et al | Yarmouth. |
| 1845 | Clare | Thomas Gorrer, et al | Yarmouth. |
| 1837 | Antigonish | Hugh McDonald. | Halifax. |
| 1846 | Canso | William Strachan | Halifax. |
| 1846 | La Have | John Hubley | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------------|------------------|----------------|-------|-------------|
| Ellen | 36021 | Schooner | 50 | 1856 |
| Ellen | 48134 | Brigantine ... | 175 | 1864 |
| Ellen Anna | 48148 | Brigantine ... | 142 | 1864 |
| Ellen Dickie | 42319 | Brigantine ... | 215 | 1862 |
| Ellen Linton | 48144 | Schooner | 121 | 1864 |
| Ellen Miles ..... | ..... | Schooner | 56 | 1849 |
| Ellen Nancy | 35770 | Schooner | 72 | 1849 |
| Ella | 46003 | Brigantine ... | 167 | 1863 |
| Ella Vail | 46660 | Brigantine.... | 203 | 1863 |
| Ellora | 49083 | Barque. . . . | 295 | 1864 |
| Elmira | 36316 | Schooner | 33 | 1860 |
| Elvienta | 38543 | Schooner | 67 | 1852 |
| Elsey | 49099 | Brigantine .. | 158 | 1864 |
| Elsie | 42329 | Brigantine ... | 112 | 1862 |
| Elsinore | 42015 | Brigantine.... | 99 | 1848 |
| Elthea Jane | 46365 | Schooner | 35 | 1863 |
| Emblem | 35870 | Schooner | 57 | 1854 |
| Emblem | 35870 | Schooner | 63 | 1854 |
| E. McLeod | 46305 | Brigantine ... | 177 | 1864 |
| Emerald | 37532 | Schooner | 32 | 1847 |
| Emerald | 34758 | Schooner | 28 | 1845 |
| Emerald | 36012 | Schooner | 103 | 1856 |
| Emeline | 42079 | Schooner | 116 | 1861 |
| Emeline | 49431 | Brigantine ... | 161 | 1864 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|---------------------------------|-------------------|
| Sheet Harbor. | Henry Shellnutt. | Halifax. |
| Londonderry. | Joseph Wier, et al. | Halifax. |
| Amherst. | Thomas A. Pugsley, et al. | Halifax. |
| Truro. | John B. Dickie, et al. | Halifax. |
| Economy. | Daniel McKenzie, et al. | Halifax. |
| Cape Cove. | Lovekin Hilton. | Digby. |
| Parrsborough. | Lawrence Wood. | Parrsborough. |
| River Herbert. | William K. Chapman, et al. | Parrsborough. |
| Weymouth. | William B. Vail, et al. | Digby. |
| Pietou. | Robert Doull, et al. | Pietou. |
| Country Harbor. | John C. Nickerson. | Halifax. |
| Main-a-Dieu. | John Martell. | Sydney, C. B. |
| Tatamagouche. | Robert Purvis. | Pietou. |
| Lunenburg. | Daniel Cronan. | Halifax. |
| Granville. | William Hamilton, et al. | Digby. |
| Cornwallis. | Walter Warneck. | Windsor. |
| St. Mary's. | Daniel H. Green. | Halifax. |
| St. Mary's. | Daniel H. Green. | Halifax. |
| Liverpool. | M. McLeod, et al. | Liverpool. |
| La Have. | Jacob Crouse. | Lunenburg. |
| La Have. | Isaac Nickerson, et al. | Halifax. |
| Sydney, C. B. | Robert Lavashe. | Arichat. |
| Clements. | William Reed, et al. | Digby. |
| Tusket. | Nathaniel Churchill. | Yarmouth. |

| Vessel's Name. | Official Number. | Rlg. | Tons. | When built. |
|--------------------------------|------------------|----------------|-------|-------------|
| Emedine | 38193 | Schooner | 96 | 1856 |
| Emily | 38620 | Schooner | 14 | 1854 |
| Emily | 35648 | Schooner | 40 | 1855 |
| Emily | 33039 | Schooner | 45 | 1839 |
| Emily | 35865 | Schooner | 38 | 1853 |
| Emily | 42390 | Barque. | 334 | 1863 |
| Emily ..... | ..... | Schooner | 46 | 1833 |
| Emily | 38280 | Schooner | 65 | 1855 |
| Emily | 38343 | Schooner | 32 | 1828 |
| Emily Ann ..... | ..... | Schooner | 40 | 1831 |
| Emily & Agnes | 36095 | Schooner | 19 | 1857 |
| Emily Jane | 37589 | Brigantine.... | 114 | 1857 |
| Emily A. Rogers | 38039 | Schooner | 25 | 1857 |
| Emma | 38611 | Schooner | 92 | 1860 |
| Emma | 38578 | Schooner | 63 | 1852 |
| Emma | 37879 | Schooner | 32 | 1856 |
| Emma | 35347 | Schooner | 9 | 1848 |
| Emma ..... | ..... | Schooner ... | 52 | 1847 |
| Emma ..... | ..... | Schooner | 24 | 1846 |
| Emma | 37815 | Schooner | 89 | 1854 |
| Emma | 48130 | Schooner | 153 | 1864 |
| Emma | 39087 | Schooner | 39 | 1856 |
| Emma Adelaine ..... | ..... | Brig | 126 | 1847 |
| Emma Jars | 43054 | Brigantine.... | 248 | 1862 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|---|-------------------|
| Canada | Abraham N. Whitman | Halifax. |
| Louisburg | Charles C. McAlpine | Sydney, C. B. |
| Barrington | John G. Dallinger, et al | Yarmouth. |
| Yarmouth | Duncan McPhee, et al | Halifax. |
| Sheet Harbor | James G. A. Creighton | Halifax. |
| Maitland | Jeremiah Northup, et al | Halifax. |
| Little Arichat | D. McKenzie, et al | Arichat. |
| Grand Ruisseau | Feliz Terrio | Arichat. |
| Essex, U. S | Joshua Briggs | Arichat. |
| Musquodoboit | James Webber, et al | Halifax. |
| La Have | William G. Bragg | Halifax. |
| La Have | Nathaniel L. West, et al | Halifax. |
| Argyle | Benjamin Rogers, Junr., et al | Yarmouth. |
| North Sydney | Theodore Churchill | Sydney, C. B. |
| Big Bras d'Or | William Muggah | Sydney, C. B. |
| Horton | Thomas Killam, et al | Yarmouth. |
| Grand Manan | Joseph Johnson, et al | Digby. |
| Isaac's Harbor | William Stewart | Windsor. |
| Chezzetcook | William Henry Rudolf | Halifax. |
| Greave's Creek | James B. Oxley | Halifax. |
| Pompquet | Elisha W. Randall | Halifax. |
| P. E. Island | Richard Siteman, et al | Halifax. |
| Sheet Harbor | Daniel Cronan | Halifax. |
| Pictou | John Lowrey, Junr., et al | Sydney, C. B. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------|---------------------|----------------|-------|----------------|
| Emma Johnston ..... | ..... | Schooner | 45 | 1841 |
| Emma Muir | 49449 | Barque | 375 | 1865 |
| Emperor ..... | ..... | Brigantine.... | 125 | 1836 |
| Emperor | 36317 | Schooner | 26 | 1847 |
| Enchantress | 36447 | Schooner | 35 | 1860 |
| Endeavour | ... | Schooner | 35 | 1835 |
| Endeavour ..... | ..... | Schooner | 62 | 1837 |
| Endeavour | 37397 | Schooner | 95 | 1857 |
| Endeavour | 35693 | Schooner | 38 | 1851 |
| Enoch Berner | 41815 | Schooner | 32 | 1861 |
| Ensign ..... | ..... | Schooner | 30 | 1815 |
| Enterprise ..... | ..... | Schooner | 15 | 1837 |
| Enterprise ..... | ..... | Schooner | 25 | 1847 |
| Enterprise | 36606 | Schooner | 104 | 1859 |
| Enterprise ..... | ..... | Schooner | 122 | 1842 |
| Enterprise | 36258 | Schooner | 20 | 1846 |
| Enterprise | 36280 | Schooner | 22 | 1860 |
| Enterprise | 35971 | Schooner | 26 | 1847 |
| Enterprise | 41538 | Schooner | 29 | 1859 |
| Enterprise | 42344 | Barque. | 473 | 1863 |
| Enterprise | 53560 | Schooner | 109 | 1865 |
| Envoy | 37053 | Schooner | 17 | 1860 |
| Eolus ..... | ..... | Schooner | 80 | 1853 |
| Eolus | 42399 | Brigantine.... | 234 | 1863 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------------|---------------------------------------|-------------------|
| Parrsborough | John Kerr, et al. | Halifax. |
| Shelburne | Robert Guest, et al. | Yarmouth. |
| Cornwallis | Thomas Young, et al. | Halifax. |
| St. Mary's | W. H. Tidmarsh, et al. | Halifax. |
| Shelburne | Jonathan Locke, et al. | Shelburne. |
| New Dublin | Levi Houghton, et al. | Halifax. |
| Antigonish | Angus McDonald | Halifax. |
| Pompquet, C. B. | Christopher McDonald | Guysborough. |
| Weymouth | Thomas C. Kinnear | Halifax. |
| St. Ann's | George McIves, et al. | Halifax. |
| Barrington | Ensign Nickerson | Halifax. |
| Tracadie | John Duplex | Arichat. |
| L'Ardoise | G. A. Bissett | Arichat. |
| Cornwallis | William Gould, et al. | Windsor. |
| Truro | James Longhead, Junr., et al. | Halifax. |
| Chezzetcook | Stephen Walch | Halifax. |
| St. Margaret's Bay | Galeton Gerrior | Halifax. |
| Sable River | Martin Lisk, et al. | Halifax. |
| Indian Harbor | John Fraser | Halifax. |
| Londonderry | Frederick M. Pearson | Halifax. |
| Londonderry | James Gourley, et al. | Halifax. |
| Guysborough | James Dickey, et al. | Guysborough. |
| Westport | Robert Brown | Digby. |
| Richmond | Benjamin Willett, et al. | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------|------------------|----------------|-------|-------------|
| Erina ..... | ..... | Schooner | 13 | 1852 |
| Esperance ..... | ..... | Schooner | 30 | 1843 |
| Esperance ..... | ..... | Schooner | 25 | 1844 |
| Esquimaux | 36164 | Brigantine ... | 92 | 1858 |
| Essex | 33898 | Schooner | 28 | Unk'n |
| Estello | 52034 | Brig | 334 | 1865 |
| Esther ..... | ..... | Schooner | 15 | 1825 |
| Ethel | 48264 | Brigantine ... | 87 | 1864 |
| Eudora | 38227 | Barque | 708 | 1862 |
| Eudora | 36081 | Schooner | 19 | 1857 |
| Eugede | 46083 | Schooner | 25 | 1862 |
| Eugenia | 48075 | Barque | 433 | 1863 |
| Eugenie | 29140 | Barque | 533 | 1860 |
| Eugine | 36026 | Schooner | 23 | 1856 |
| Eunice | 48449 | Brigantine ... | 174 | 1864 |
| Euphates ..... | ..... | Brigantine ... | 81 | 1848 |
| Eureka | 36888 | Schooner | 126 | 1860 |
| Eureka | 46035 | Barque | 574 | 1862 |
| Eureka | 46380 | Brigantine ... | 256 | 1863 |
| Eureka | 42030 | Brigantine ... | 98 | 1860 |
| Eureka | 52017 | Brigantine ... | 211 | 1865 |
| Eva | 38074 | Barque | 498 | 1858 |
| Evergreen | 46904 | Schooner | 52 | 1864 |
| Evelina ..... | ..... | Schooner | 68 | 1837 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|-------------------------------------|-------------------|
| Parrsborough. | William C. Murray, et al. | Parrsborough. |
| Cheticamp. | Peter Chatham, et al. | Arichat. |
| River Bourgeois. . . | C. Landry, et al. | Arichat. |
| Indian Harbor. | George Wilson. | Halifax. |
| Gloucester, U. S. . . | L. LeBlanc, et al. | Arichat. |
| Granville. | David Delap, et al. | Annapolis. |
| Truro. | George Cook, et al. | Halifax. |
| Jordan River. | Howard Locke. | Shelburne. |
| Meteghan. | Amasa Durkee, et al. | Yarmouth. |
| Indian Harbor. | John Church. | Halifax. |
| Shelburne. | Isaac C. Crowell. | Shelburne. |
| Hillsburgh. | J. K. Ryerson, et al. | Yarmouth. |
| Newport. | Nicholas Mosher, Senr., et al. | Windsor. |
| Chezzetcook. . . . | Philip W. Nicolls. | Halifax. |
| Cornwallis. | Silvanus Whitney, et al. | Windsor. |
| Cornwallis. | John McMullan, et al. | Halifax. |
| Pompquet. | A. LeBlanc. | Arichat. |
| Newport. | John A. Harvie, et al. | Windsor. |
| Noel. | Levi Densmore, et al. | Windsor. |
| Wilmot. | John Cronan. | Halifax. |
| Shelburne. | John C. Williams. | Shelburne. |
| Church Point. | A. Lovett, et al. | Yarmouth. |
| Strait of Canso. . . | Alexander Johnson, et al. | Port Hawkesbury. |
| Clare. | Joshua Snow. | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------------|------------------|----------------|-------|-------------|
| E. W. Forrest | 52004 | Schooner | 41 | 1852 |
| E. W. Ross | 52035 | Schooner | 63 | 1865 |
| Example | 43083 | Brigantine... | 183 | 1863 |
| Exchange | 37521 | Schooner | 86 | 1839 |
| Exchange | 38246 | Barque..... | 370 | 1863 |
| Exchange | 42098 | Schooner | 24 | Unk'n |
| Excelsior | 49102 | Brigantine ... | 128 | 1864 |
| Excelsior | 38267 | Brig | 278 | 1863 |
| Exeter | 37489 | Barque..... | 212 | 1856 |
| Exemplar | 42833 | Schooner | 105 | 1845 |
| Exile ..... | ..... | Brigantine ... | 109 | 1845 |
| Exile ..... | ..... | Schooner | 42 | 1844 |
| Exile | 49029 | Brigantine ... | 169 | 1865 |
| Express | 37679 | Schooner .. | 59 | 1862 |
| Express | 38428 | Schooner | 31 | 1862 |
| Express | 42512 | Schooner | 71 | 1863 |
| Express | 42026 | Schooner ... | 38 | 1860 |
| Express | 35652 | Brig | 143 | 1851 |
| Express | 36168 | Brigantine ... | 91 | 1858 |
| Express | 36264 | Schooner | 16 | 1860 |
| Express | 42174 | Schooner ... | 68 | 1861 |
| Express | 36113 | Schooner | 24 | 1843 |
| Export | 48043 | Brigantine ... | 178 | 1864 |
| Experiment (Steamer) | 37302 | Schooner | 73 | Unk'n |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|-----------------------------------|-------------------|
| Essex, U. S. | David Smith, et al. | Shelburne. |
| Wilnot. | Samuel B. Slocumb, et al. | Annapolis. |
| Tatamagouche ... | Robert Purvis. | Pictou. |
| United States. | F. Larkin, et al. | Liverpool, N. S. |
| Tusket. | Gilbert Abben, et al. | Yarmouth. |
| United States. | Edward M. Marshall. | Digby. |
| Tatamagouche . . | Robert Bryden, et al. | Pictou. |
| Shelburne. | Joseph J. Brown, et al. | Yarmouth. |
| Liverpool. | W. Mortimer. | Liverpool, N. S. |
| Newport. | Robert Davison, et al. | Windsor. |
| St. Mary's Bay . . | Charles Cornwell, et al. | Digby. |
| Parrsborough. . . | Charles Hatfield. | Halifax. |
| Parrsborough. | Patrick Blake. | Parrsborough. |
| Parrsborough. | Robson M. Dixon. | Parrsborough. |
| River Bourgeois. . | G. H. Bissett. | Arichat. |
| Lunenburg. | S. Riser, et al. | Lunenburg. |
| Weymouth. | John Lary. | Digby. |
| Dartmouth. | William Pryor, Senr., et al. | Halifax. |
| Shelburne. | Nathaniel L. West, et al. | Halifax. |
| Chezzetcook. | James Conway, et al. | Halifax. |
| P. E. Island. | John Evans. | Halifax. |
| Newdiquoddy. | Alfred J. Cook. | Halifax. |
| Granville. | John Smith, et al. | Annapolis |
| Unknown. | Oliver Jones. | Digby. |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|-------------------------------|-------------------|
| Beaver River. | James McDaniel. | Halifax. |
| Tracadie. | Joseph Delaney, et al. | Halifax |
| Parrsborough. | William Knowlton, et al. | Halifax. |

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| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
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| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
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| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------|------------------|----------------|-------|-------------|
| Fair Haven | 46369 | Brigantine... | 142 | 1863 |
| Fair Lady ..... | ..... | Schooner | 13 | 1841 |
| Fair Lady ..... | ..... | Schooner | 17 | 1840 |
| Fair Lady ..... | ..... | Schooner | 22 | 1834 |
| Fair Maid ..... | ..... | Schooner | 18 | 1826 |
| Fair Play | 35909 | Schooner | 44 | 1853 |
| Fair Trader ..... | ..... | Schooner | 48 | 1817 |
| Fair Trader ..... | ..... | Schooner | 34 | Unk'n |
| Faithful ..... | ..... | Schooner | 18 | 1821 |
| Falcon | 35833 | Schooner | 67 | 1843 |
| Falcon ..... | ..... | Schooner | 19 | 1827 |
| Falcon | 40124 | Brigantine ... | 80 | Unk'n |
| Faulkener ..... | ..... | Schooner | 18 | 1846 |
| Fame ..... | ..... | Schooner | 56 | 1814 |
| Fame ..... | ..... | Schooner | 16 | 1834 |
| Fame ..... | ..... | Schooner | 113 | 1842 |
| Fame | 36011 | Schooner | 32 | 1838 |
| Fanny | 46293 | Schooner | 72 | 1864 |
| Fanny ..... | ..... | Schooner | 70 | 1850 |
| Fanny ..... | ..... | Schooner | 18 | 1826 |
| Fanny | 38877 | Schooner | 28 | 1852 |
| Fanny Fern | 38137 | Ship | 622 | 1860 |
| Fanny Givan | 42844 | Schooner | 86 | 1862 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|----------------------------------|-------------------|
| Newport. | Ezekiel N. Masters, et al. | Windsor. |
| Barrington. | Absalom Nickerson. | Halifax. |
| La Have. | Charles E. Wiswell. | Halifax. |
| Chester. | John Hawbolt. | Halifax. |
| La Have. | William Corkum. | Halifax. |
| East Point. | Daniel James Seely. | Halifax. |
| New Brunswick. . | James Calhoon, et al. | Halifax. |
| Unknown. | John Barss, et al. | Halifax. |
| La Have. | Henry Henritey. | Halifax. |
| L'Ardoise. | Cowen D. Grant. | Port Hawkesbury. |
| Halifax. | Sir Rupert D. George. | Halifax. |
| Bermuda. | John T. Wainwright. | Halifax. |
| Tangier. | Simon Mayers, et al. | Halifax. |
| Unknown. | Joshua McCrae, et al. | Halifax. |
| Cape Negro. | Alexander R. Greenwood, et al. . | Halifax. |
| Kempt. | John Brown, et al. | Halifax. |
| Barrington. | Joshua Nickerson, et al. | Halifax. |
| Liverpool. | D. & J. Dunlop. | Liverpool, N. S. |
| Shelburne. | H. R. Locke, et al. | Liverpool, N. S. |
| Brier Island. | John Baptiste. | Halifax. |
| P. E. Island. | Gilbert W. Sanders, et al. | Halifax. |
| Tusket. | Simon Cann. | Yarmouth. |
| Harborville. | Alexander H. Givan, et al. | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Fanny Heron ..... | ..... | Brigantine ... | 135 | 1852 |
| Fanny Lewis | 49120 | Barque | 402 | 1865 |
| Fancy ..... | ..... | Schooner | 14 | 1823 |
| Farewell | .. . | Schooner | 26 | 1853 |
| Farewell ..... | ..... | Schooner | 12 | 1839 |
| Farewell ..... | ..... | Schooner | 20 | 1839 |
| Farewell | 37016 | Schooner | 27 | 1853 |
| Farewell ..... | ..... | Schooner | 25 | 1830 |
| Farewell ..... | ..... | Schooner ... | 27 | 1837 |
| Farewell ..... | ..... | Schooner | 23 | 1840 |
| Farewell | 41537 | Schooner | 27 | 1848 |
| Fashion | 42797 | Schooner | 99 | 1862 |
| Fashion | 37321 | Schooner | 54 | 1852 |
| Father Matthew | 38292 | Schooner | 75 | 1844 |
| Faugh a Ballagh | 43076 | Barque | 326 | 1863 |
| Favorite | 37875 | Schooner ... | 72 | 1856 |
| Fawn | 48418 | Brigantine... | 155 | 1864 |
| Fawn | 42092 | Schooner | 63 | 1862 |
| Fawn | 36103 | Schooner | 17 | 1846 |
| Fawn | 33416 | Brig | 127 | 1854 |
| Fearless | 42227 | Brigantine ... | 255 | 1861 |
| Fearless | 36602 | Schooner | 15 | 1859 |
| Fearless | 49450 | Barque | 349 | 1865 |
| Fedelia | 34798 | Schooner | 53 | 1849 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|-------------------------------|-------------------|
| Sheet Harbor. | William Hall. | Sydney, C. B. . |
| Tatamagouche | Robert Purvis. | Pictou. |
| Yarmouth. | Samuel Cann, et al. | Halifax. |
| Shelburne. | J. N. Jones. | Liverpool, N. S. |
| Lunenburg. | John Deal, et al. | Lunenburg. |
| Lunenburg. | John Lynch. | Lunenburg. |
| Shelburne. | James McGill. | Shelburne. |
| Yarmouth. | Oliver Doucett, et al. | Halifax. |
| Mahone Bay. | John Cole, et al. | Halifax. |
| Lunenburg. | Michael Lonas. | Halifax. |
| La Have. | John McMillan. | Halifax. |
| Shelburne. | L. M. M. Willett, et al. | Shelburne. |
| Granville. | Simon Connell, et al. | Halifax. |
| Harbor Bouche. | Henry Boudroit. | Arichat. |
| Point Brule. | John Mockler. | Pictou. |
| Quaco. | Robert Dexter, et al. | Windsor. |
| Salmon River. | Townsend & Son. | Yarmouth. |
| Meteghan. | John H. Parker. | Digby. |
| Little Harbor. | Nehemiah Crowell. | Halifax. |
| Dartmouth. | George P. Mitchell. | Halifax. |
| Granville. | John Johnson, et al. | Annapolis. |
| Newport. | Alexander Knowlton. | Windsor. |
| Shelburne. | Benjamin Crawley. | Yarmouth. |
| Parrsborough. | John K. Morris. | Halifax. |

| Vessel's Name. | Official Number. | Rlg. | Tons. | When built. |
|---------------------------------|------------------|----------------|-------|-------------|
| Felicity ..... | ..... | Schooner | 37 | 1842 |
| Ferdinand Tapper ..... | ..... | Schooner | 31 | 1824 |
| Fides | 46081 | Brigantine... | 99 | 1862 |
| Fidelity ..... | ..... | Schooner | 26 | 1845 |
| Firefly ..... | ..... | Schooner | 9 | 1839 |
| Firm | 41560 | Brigantine ... | 185 | 1859 |
| Fisher ..... | ..... | Schooner | 13 | 1828 |
| Fisher ..... | ..... | Schooner | 28 | 1833 |
| Fish Hawk ..... | ..... | Schooner | 27 | 1845 |
| Five Brothers ..... | ..... | Schooner | 33 | 1847 |
| Five Sons ..... | ..... | Schooner | 22 | 1843 |
| Flash | 42519 | Schooner | 40 | 1864 |
| Fleet Wing | 42218 | Brigantine ... | 212 | 1859 |
| Flirt | 41773 | Schooner | 44 | 1853 |
| Flirt ... | 37421 | Schooner | 46 | 1853 |
| Florence | 38436 | Schooner | 24 | 1864 |
| Florence | 34741 | Schooner | 39 | 1854 |
| Florence | 23124 | Barque..... | 208 | 1852 |
| Florence | 46388 | Brigantine ... | 149 | 1863 |
| Florence | 35674 | Schooner | 65 | 1845 |
| Florence | 35612 | Brig..... | 120 | 1853 |
| Florence C. Laurence ... | 46907 | Schooner | 69 | 1864 |
| Florence Nightingale .. | 35861 | Barque..... | 260 | 1855 |
| Flying Cloud | 41603 | Schooner | 35 | 1859 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|-------------------------------------|-------------------|
| Margaree | James Galland | Arichat. |
| Arichat | William Mason | Halifax. |
| Sable River | Lovett Willett, et al | Shelburne. |
| Bay of Islands | Benjamin Wier | Halifax. |
| Westport | William Coggin, et al | Digby. |
| Parrsborough | William Church, et al | Windsor. |
| Shelburne | Conrad Crow, et al | Halifax. |
| Ragged Island | William Hayden | Halifax. |
| Torbay | Francis Cooke | Halifax. |
| New Dublin | Frederick Croft, et al | Halifax. |
| Barrington | Abraham Malone | Halifax. |
| Mahone Bay | Nathaniel Strum, et al | Lunenburg. |
| Pictou | E. Seely, et al | Liverpool, N. S. |
| La Have | S. Oxner, et al | Lunenburg. |
| Shelburne | Benjamin Wier | Halifax. |
| Margaree | Samuel Laurence | Arichat. |
| Granville | William H. Gilleat, et al | Digby. |
| Horton | Henry J. Creighton | Windsor. |
| Walton | Joseph Stephens, et al | Windsor. |
| United States | Samuel Perry, et al | Halifax. |
| Shelburne | Thomas C. Kinnear | Halifax. |
| Margaree | William Buckles | Port Hawkesbury. |
| Wallace | Donald Murray | Halifax. |
| Tracadie | Peter Petipas, et al | Pictou. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------------|-------------|
| Flying Cloud | 36275 | Schooner | 53 | 1860 |
| Flying Fish ..... | ..... | Schooner | Prize 28 | 1814 |
| Flying Fish ..... | ..... | Schooner | 19 | 1837 |
| Flying Fish ..... | ..... | Schooner | 14 | 1846 |
| Flying Mist | 51772 | Schooner | 50 | 1865 |
| Flying Robin | 38612 | Schooner | 26 | 1860 |
| Flying Arrow | 42099 | Brigantine ... | 121 | 1854 |
| Fly | 38990 | Schooner | 19 | 1860 |
| Fly ..... | ..... | Schooner | 16 | 1824 |
| Fly ..... | ..... | Schooner | 13 | 1836 |
| Fly ..... | ..... | Schooner | 8 | 1845 |
| Fly ..... | ..... | Schooner | 19 | 1846 |
| Fly ..... | ..... | Schooner | 22 | 1847 |
| Foam | 36992 | Schooner | 40 | 1861 |
| Foam | 36354 | Schooner | 102 | 1861 |
| Foaming Billow | 42276 | Schooner | 66 | 1861 |
| Forest Oak | 38239 | Schooner | 92 | 1860 |
| Forest Queen | 43053 | Barque | 575 | 1862 |
| Forest | 35663 | Schooner | 39 | 1828 |
| Formosa | 46654 | Brigantine ... | 127 | 1863 |
| Fortune | 38589 | Schooner | 40 | 1844 |
| Fortitude ..... | ..... | Schooner | 33 | 1823 |
| Forward | 37889 | Brigantine ... | 131 | 1856 |
| Foster ..... | ..... | Schooner | 68 | 1851 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------------------|-------------------------------------|-------------------|
| Ship Harbor | James Murphy | Halifax. |
| U. S. (not known). | D. Campbell | Liverpool, N. S. |
| Clare | Joseph Bellevieu, et al | Halifax. |
| Argyle | Israel Wilson, et al | Halifax. |
| Vogler's Cove | W. Vogler, et al | Liverpool. |
| Little Bras d'Or, C B | Richard Richard, et al | Sydney, C. B. |
| Hillsburgh | Edward Everett, et al | Digby. |
| Yarmouth | Abraham Lent | Digby. |
| Lunenburg | Andrew Wentzell | Halifax. |
| Granville | James Handwick | Halifax. |
| Cornwallis | William Hamilton | Halifax. |
| New Dublin | Joseph Smith, et al | Halifax. |
| Barrington | Seth Reynolds, et al | Halifax. |
| La Have | Joseph Daces, et al | Lunenburg. |
| Guysborough | Reuben I. Hart, et al | Halifax. |
| Sheet Harbor | Francis Glawson | Halifax. |
| United States | Theodore Churchill, et al | Yarmouth. |
| River John | Robert W. Mirriam, et al | Pictou. |
| Clyde River | Benjamin Perry, et al | Halifax. |
| Clare | Maturine Robichau, et al | Yarmouth. |
| Main-a-Dieu | Michael Leonard | Sydney, C. B. |
| Cape Breton | Joseph Kennedy | Halifax. |
| Cornwallis | Nathaniel L. West, et al | Halifax. |
| New Brunswick | Amos Seaman | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------------------|------------------|----------------|-------|-------------|
| Four Brothers ..... | .... | Schooner | 40 | 1849 |
| Four Brothers | 37586 | Schooner | 33 | 1857 |
| Four Brothers | 38385 | Schooner | 104 | 1859 |
| Four Brothers ..... | .... | Schooner | 32 | 1841 |
| Four Friends ..... | .... | Schooner | 26 | Unk'n |
| Four Sisters ..... | .... | Schooner | 23 | 1826 |
| Four Sisters ..... | .... | Schooner | 32 | 1828 |
| Four Sons ..... | .... | Schooner | 67 | 1824 |
| Fousine ..... | .... | Schooner | 25 | 1854 |
| Fowler | 36282 | Schooner | 64 | 1841 |
| Fox ..... | .... | Schooner | 14 | 1850 |
| Fox ..... | .... | Schooner | 18 | 1817 |
| Fox ..... | .... | Schooner | 68 | 1820 |
| Fox ..... | .... | Schooner | 14 | 1844 |
| Frances | 38116 | Schooner | 94 | 1860 |
| Frances | 42221 | Schooner | 37 | 1860 |
| Frances Jane | 38071 | Schooner | 104 | 1858 |
| Frances Jane ..... | .... | Brigantine ... | 95 | 1848 |
| Frances Mary ..... | .... | Schooner | 51 | 1832 |
| Francis | 35903 | Schooner | 48 | 1854 |
| Francis Bourneuf | 49439 | Barque | 570 | 1865 |
| Francis Hilyard | 38217 | Ship | 976 | 1862 |
| Francis Harris | 35459 | Schooner | 42 | 1856 |
| Frank | 38633 | Schooner | 92 | 1862 |

| Where built. | Registered Owner. | Port of Registry. |
|-------------------------|--------------------------------------|-------------------|
| Little Arichat | Alexander McPherson | Arichat. |
| Lunenburg | J. Spindler, et al | Lunenburg. |
| Grand Ruissseau . . . | William LeBlanc | Arichat. |
| La Have | George Munroe, et al | Halifax. |
| Unknown | John Croft, et al | Halifax. |
| Bay of Islands | P. DeCarteret, et al | Arichat. |
| Jeddore | Peter Webber, Senr., et al | Halifax. |
| Digby | Joseph Fitz Randolph | Halifax. |
| Cheticamp | Gilbert Hatchet | Arichat. |
| Maitland | Isaac Lake | Windsor. |
| Canso | Samuel Swaine | Guysborough. |
| Clare | William Webber | Halifax. |
| Barrington | Pelez Holmes, Junr. | Halifax. |
| Barrington | Abraham Penney, et al | Halifax. |
| Beaver River | Reuben Perry, et al | Shelburne. |
| Granville | John Robinson, et al | Annapolis. |
| Tusket | G. S. Brown | Yarmouth. |
| Digby | Clement M. Melancer | Digby. |
| Windsor | John Fottell, et al | Halifax. |
| St. Mary's | James B. Oxley | Halifax. |
| Bellevue Cove | William D. Lovitt | Yarmouth. |
| St. John, N. B. | J. W. Lovitt, et al | Yarmouth. |
| Mount Desert, U.S. . . | J. A. & J. B. Hatfield | Yarmouth. |
| Sydney | Edward P. Archbold | Sydney, C. B. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------------|------------------|----------------|-------|-------------|
| Frank | 36024 | Brig | 131 | 1856 |
| Frank Lovett | 38135 | Barque. | 598 | 1860 |
| Frank W. | 42977 | Brigantine.... | 203 | 1862 |
| Franklin | 36338 | Schooner | 32 | 1861 |
| Freeman Dennis | 49445 | Barque. | 808 | 1865 |
| Freestone | 49452 | Brigantine.... | 195 | 1864 |
| Freedom | 36343 | Schooner | 50 | 1844 |
| Freedom ..... | .... | Schooner | 44 | 1850 |
| Freedom ..... | .... | Schooner | 23 | 1844 |
| Freedom | 48110 | Schooner | 28 | 1840 |
| Frederick | 53786 | Barque. | 162 | Unk'n |
| Frederick P. Ladd | 34723 | Schooner | 79 | 1854 |
| Frederick H. Parker | 48472 | Brig | 132 | 1865 |
| Friend | 42517 | Schooner | 88 | 1863 |
| Friend | 38910 | Schooner | 84 | 1853 |
| Friends ..... | .... | Schooner | 28 | 1847 |
| Friendship ..... | .... | Schooner | 40 | 1838 |
| Friendship ..... | .... | Schooner | 41 | 1835 |
| Friendship ..... | .... | Schooner | 15 | 1836 |
| Friendship ..... | .... | Schooner ... | 102 | 1840 |
| F. R. Goodman | 35968 | Schooner | 50 | 1851 |
| Frink | 50770 | Schooner | 106 | 1861 |
| F. Tupper | 51800 | Barque. | 298 | 1865 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|---------------------------------|-------------------|
| Shelburne | Thomas C. Kinnear... .. | Halifax. |
| Clare | Andrew Lovitt, et al. | Yarmouth. |
| P. E. Island. | George C. Harvey. | Halifax. |
| United States. | John Major | Halifax. |
| Beaver River. | Freeman Dennis, et al. | Yarmouth. |
| Wallace | T. Batter, et al. | Pugwash. |
| Londonderry . . . | Gasper Oxner | Halifax. |
| New Brunswick. . | Josiah Coffee | Halifax. |
| P. E. Island. . . . | Israel Nickerson | Halifax. |
| Essex, U. S. | Peter S. Pebrine. | Halifax. |
| Foreign | Frederick Fader | Liverpool. |
| Hillsburgh | Willoughby Anthony, et al. | Digby. |
| Harborville | Daniel B. Parker, et al. | Windsor. |
| Mahone Bay | J. M. Watson. | Lunenburg. |
| River Bourgeois. . | Simon Richard, et al. | Arichat. |
| Lunenburg | S. Conrod, et al. | Lunenburg. |
| Barrington | William Crawford, et al. | Digby. |
| Cornwallis | James Huntley, et al. | Halifax. |
| Granville | Henry Greenwood, et al. | Halifax. |
| Londonderry | John Walker | Halifax. |
| Guysborough | Joseph McGrigor. | Guysborough. |
| Hampden, U. S. . | Henry James Creighton | Halifax. |
| Liverpool | James Collie, et al. | Liverpool, N. S. |

| Where built. | Registered Owner. | Port of Registry |
|--------------|-------------------|------------------|
| | | |

G.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|------------------|----------------|-------|-------------|
| Galaxy | 41926 | Schooner | 34 | 1859 |
| Garland ..... | ..... | Brigantine.... | 95 | 1846 |
| Garibaldi | 42207 | Schooner | ... | 1861 |
| Gazelle | 48478 | Barque | 305 | 1865 |
| Gazelle | 37018 | Schooner | 69 | 1861 |
| Gazelle | 41786 | Schooner | 28 | 1859 |
| Gazelle | 39031 | Schooner | 24 | 1853 |
| Gem | 38613 | Brigantine.... | 94 | 1860 |
| Gem | 41551 | Schooner | 9 | 1859 |
| Gem | 35879 | Schooner | 29 | 1835 |
| General Canrobert | 38281 | Brigantine ... | 134 | 1855 |
| General Doyle | 48092 | Schooner | 67 | 1864 |
| General Pelissier | 38336 | Brigantine.... | 162 | 1856 |
| General Taylor | 52012 | Schooner | 40 | 1840 |
| General Williams | 37483 | Brigantine.... | ... | 1856 |
| General Williams | 35024 | Ship | 989 | 1856 |
| General Washington | 35640 | Brigantine ... | 70 | 1848 |
| Genetta | 37315 | Schooner | 12 | 1856 |
| Gentle Annie | 48454 | Brigantine.... | 199 | 1864 |
| George | 49089 | Brigantine.... | 228 | 1864 |
| George ..... | ..... | Brig | 147 | 1850 |
| George ..... | ..... | Schooner | 56 | 1833 |
| George ..... | ..... | Schooner | 44 | 1843 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|--------------------------------|-------------------|
| Liverpool | J. Gardner, et al..... | Liverpool, N. S. |
| Cornwallis | George McLellan | Parrsborough. |
| Liverpool | H. Gardner, et al | Liverpool, N. S. |
| Windsor | P. M. Cunningham, et al..... | Windsor. |
| Barrington | Joseph Swain, et al..... | Shelburne. |
| La Have | Thomas Jean..... | Lunenburg. |
| Spry Harbor | Michael Burke..... | Halifax. |
| Sydney, C. B.... | Clement H. Harrington... .. | Sydney, C. B. |
| Newport | Ira Burgess, et al..... | Windsor. |
| United States.... | Richard Potter | Halifax. |
| Grand Ruisseau.. | Constant Ferguson..... | Arichat. |
| Tusket | Isaac S. Hatfield, et al..... | Yarmouth. |
| Little Arichat.... | Mellum Porrier | Arichat. |
| United States.... | Whitman B. Long, et al..... | Shelburne. |
| Shelburne..... | Muir & Doan..... | Liverpool, N. S. |
| Portland, N. B.... | George S. Brown, et al..... | Yarmouth. |
| Yarmouth | John E. Cummins..... | Halifax. |
| Petit Passage.... | James Outhouse, et al..... | Digby. |
| Newport..... | Robert H. Burgess | Windsor. |
| Merigomish | David Graham, et al..... | Pictou. |
| Wilmot..... | Granville B. Reed | Digby. |
| Barrington | Theophilus Crossby, et al | Halifax. |
| East River..... | Alexander Cantley | Halifax. |

| Vessel's Name. | Official Number. | Rlg. | Tons. | When built. |
|--------------------------------|------------------|----------------|-------|-------------|
| George | 35622 | Schooner | 29 | Unk'n |
| George | 36071 | Schooner | 13 | 1855 |
| George Brown | 42093 | Schooner | 103 | 1862 |
| George S. Brown | 38268 | Barque | 537 | 1863 |
| George Bell | 48071 | Barque | 599 | 1863 |
| George Durkee | 38008 | Barque | 568 | 1856 |
| George F. | 46661 | Brigantine... | 204 | 1863 |
| George Farewell ..... | ..... | Schooner | 36 | 1845 |
| George H. Jenkins | 49409 | Barque. . . . | 582 | 1864 |
| George McKeen | 36492 | Schooner | 100 | 1859 |
| George Parker | 42843 | Schooner | 77 | 1862 |
| George M. Soule ..... | ..... | Brig | 154 | 1840 |
| George Spicer | 52015 | Brigantine ... | 191 | 1865 |
| George Thompson ..... | ..... | Schooner | 48 | 1844 |
| George Washington | 22777 | Brigantine ... | ... | 1849 |
| Gertrude | 43200 | Brig | 198 | Unk'n |
| G. F. Peniston | 48466 | Brigantine ... | 217 | 1864 |
| Gipsy | 36112 | Schooner | 162 | 1857 |
| Gipsy | 49021 | Schooner ... | 73 | 1865 |
| Gipsy Lass | 37488 | Schooner | 26 | 1856 |
| Gipsy Queen | 36057 | Schooner | 24 | 1855 |
| G. I. L. | 36574 | Schooner | 56 | 1860 |
| Glacier | 42371 | Barque | 480 | 1853 |
| Gladiator | 46482 | Schooner | 105 | 1864 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|---------------------------------|-------------------|
| United States..... | John Purney..... | Halifax. |
| H. M. Dockyard .. | George Scarfe..... | Halifax. |
| St. Mary's Bay ... | James P. Specht..... | Digby. |
| Yarmouth..... | Nathan Utley..... | Yarmouth. |
| Bellivùe Cove | Andrew Lovitt, et al..... | Yarmouth. |
| Yarmouth..... | Amasa Durkee, et al..... | Yarmouth. |
| Beaver River..... | William F. Marshall, et al..... | Digby. |
| Mahone Bay..... | William Lawson..... | Halifax. |
| Beaver River..... | Freeman Dennis..... | Yarmouth. |
| La Have | P. Henritey, et al..... | Lunenburg. |
| Cornwallis | Samuel Dodge, et al..... | Windsor. |
| Kempt | Robert Marsters..... | Halifax. |
| Jordan River..... | Joshua Snow, et al..... | Shelburne. |
| Granville | Richard Heans, et al..... | Halifax. |
| Cornwallis | L. Sponagle, et al..... | Liverpool, N. S. |
| Unknown..... | A. F. Freeman..... | Liverpool, N. S. |
| Walton | Francis G. Parker..... | Windsor. |
| Shelburne... .. | William Arnold..... | Liverpool. |
| Minudie | Gilbert Seaman, et al..... | Parrsborough. |
| Jordan River..... | L. P. Churchill, et al..... | Liverpool, N. S.. |
| Chezzetcook..... | Dennis Smith..... | Halifax. |
| St. Mary's | John Roode..... | Guysborough. |
| United States..... | Catheart Thomson, et al..... | Halifax. |
| Lunenburg | Benjamin Westhaver, et al..... | Lunenburg. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------------|------------------|----------------|-------|-------------|
| Glad Tidings | 51771 | Schooner | 53 | 1864 |
| Glasgow | 48490 | Barque | 480 | 1864 |
| Glen Gall | 43058 | Barque | 498 | 1862 |
| Glenalladale | 49096 | Barque | 313 | 1864 |
| Glencairn | 49104 | Barque | 351 | 1864 |
| Glenrose | 49006 | Schooner | 75 | 1864 |
| Gleaner ..... | ..... | Brigantine... | 105 | 1852 |
| Glide | 36497 | Schooner | 50 | 1860 |
| Glide ..... | ..... | Schooner | 21 | 1837 |
| Glide ..... | ..... | Schooner | 24 | 1844 |
| Glide | 35632 | Schooner .. | 21 | 1855 |
| Glory | 34974 | Schooner | 41 | 1836 |
| Glorie | 42848 | Ship | 1138 | 1862 |
| Globe ..... | ..... | Schooner | 13 | 1839 |
| Globe | 52058 | Brig | 288 | 1865 |
| Golden Age | 35801 | Schooner ... | 93 | 1852 |
| Golden Eagle | 36479 | Schooner .. | 43 | 1859 |
| Golden Era | 42292 | Schooner | 44 | 1861 |
| Golden Rule ..... | ..... | Schooner | 16 | 1837 |
| Golden Rule | 36038 | Schooner | 15 | 1856 |
| Golden Rule | 35814 | Brigantine ... | 78 | 1852 |
| Golden West | 46473 | Schooner | 53 | 1864 |
| Gold Hunter | 34904 | Brig | 197 | 1854 |
| Gold Hunter | 35683 | Schooner | 45 | 1853 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|------------------------------------|-------------------|
| Port Medway | B. P. Dunn, et al. | Liverpool, N. S. |
| Hantsport | Daniel Hutley, et al. | Windsor. |
| Point Brule | John Mockler | Pictou. |
| Antigonish | Donald McDonald | Pictou. |
| Tatamagouche | Archibald Campbell | Pictou. |
| Parrsborough | Andrew Thompson | Parrsborough. |
| Port Medway | H. Fader | Liverpool, N. S. |
| Mahone Bay | C. Lord, et al. | Lunenburg. |
| Shelburne | Lewis E. Tremain | Arichat. |
| Ship Harbor | David Weeks | Halifax. |
| Liscomb Harbor . . . | John McKinlay | Halifax. |
| Arichat | William McEwen | Arichat. |
| Hantsport | Ezra Churchill, et al. | Windsor. |
| Brier Island | William Merritt, et al. | Digby. |
| Cornwallis | William H. Payzant, et al. | Windsor. |
| Lunenburg | William B. Hamilton | Halifax. |
| Mahone Bay | George Smilzer, et al. | Lunenburg. |
| Marie Joseph | John Gillam, et al. | Halifax. |
| Lewis Head | Nehemiah Crowell | Halifax. |
| Chester | George Doray | Halifax. |
| Mahone Bay | Conrad West, et al | Halifax. |
| La Have | William Smith, et al. | Lunenburg. |
| Argyle | Gilbert Sanderson, et al | Yarmouth. |
| Lunenburg | James B. Kenny | Halifax. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|---------------------------------|------------------|----------------|-------|-------------|
| Gold Liner | 42297 | Schooner | 50 | 1862 |
| Gold Miner | 53554 | Schooner | 88 | 1865 |
| Gondola | 38228 | Ship | 726 | 1862 |
| Gondolien | 49013 | Brigantine ... | 176 | 1865 |
| Good Fortune ..... | ..... | Schooner | 62 | 1818 |
| Good Fortune ..... | ... . | Schooner | 20 | 1824 |
| Good Intent ..... | ..... | Schooner | 15 | 1859 |
| Good Intent | 41903 | Schooner | 14 | 1844 |
| Good Intent ..... | ..... | Schooner | 40 | 1844 |
| Good Intent ..... | ..... | Schooner | 54 | 1819 |
| Good Intent ..... | ..... | Schooner | 16 | 1820 |
| Good Intent ..... | ..... | Schooner | 30 | 1846 |
| Good Will ..... | ..... | Schooner | 27 | 1843 |
| Good Will ..... | ..... | Schooner | 35 | 1838 |
| Grace Darling | 42787 | Schooner | 50 | 1861 |
| Grand Pre | 42705 | Schooner | 51 | 1861 |
| Grand Pre | 52059 | Schooner | 114 | 1865 |
| Graham | 42305 | Schooner | 93 | 1862 |
| G. R. C | 42124 | Schooner | 87 | 1861 |
| Greyhound | 41818 | Schooner | 23 | 1863 |
| Greyhound | 37482 | Schooner | 56 | 1856 |
| Greenwood | 36219 | Schooner | 20 | 1859 |
| Grumbling Mistress ..... | ..... | Schooner | 25 | 1833 |
| G. T. Winson | 52010 | Schooner | 99 | 1865 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|-----------------------------------|-------------------|
| Country Harbor... | James B. Oxley..... | Halifax. |
| Indian Harbor... | Isaac Willett | Halifax. |
| Yarmouth | Dennis Horton, et al..... | Yarmouth. |
| Parrsborough..... | Peter Roberts, et al..... | Parrsborough. |
| Yarmouth..... | Thomas Flint, et al..... | Halifax. |
| Barrington | John Crowell, et al | Halifax. |
| Strait of Canso ... | James Reeves, et al..... | Port Hawkesbury. |
| Kempt..... | Caleb Skaling, et al..... | Windsor. |
| Yarmouth | Robert Hewitt, et al | Halifax. |
| New Brunswick... | William Stephens | Halifax. |
| Parrsborough..... | James Ratchford, Junr., et al.... | Halifax. |
| La Have | Adam Andrews..... | Halifax. |
| D'Esouse | Peter D. Carteret, et al..... | Arichat. |
| Granville | Christopher Dunn | Halifax. |
| Barrington | Joseph Swain, et al..... | Shelburn. |
| Gilbert's Cove.... | Joseph LeBlanc..... | Digby. |
| Parrsborough..... | George E. Forsyth, et al..... | Windsor. |
| Antigonish | James W. Bigelow ... | Halifax. |
| Cornwallis | William Clark, et al..... | Windsor. |
| St. Ann's | Donald McRitchie, et al | Baddeck. |
| Liverpool | J. Eaton..... | Liverpool, N. S. |
| Shelburne..... | Alexander Greenwood | Halifax. |
| Lunenburg | Anthony Graves | Halifax. |
| Shelburne | Charles Stalker, et al..... | Shelburne. |

| Vessel's Name. | Official Number. | | Tons. | When built. |
|---------------------------|---------------------|----------------|-------|----------------|
| Guiding Star | 38080 | Barque | 621 | 1859 |
| Guiding Star | 49446 | Brigantine ... | 311 | 1865 |
| Guide | 52018 | Brigantine ... | 60 | 1865 |
| Guinea | 48446 | Barque | 654 | 1864 |
| Gulnare ..... | ..... | Schooner | 14 | 1838 |
| G. W. Giffin | 41930 | Schooner | 52 | 1859 |
| G. W. Halls | 48149 | Brigantine ... | 169 | 1864 |
| G. Wheelwright ... | 42218 | Brigantine ... | 229 | 1862 |
| Gipsy Queen | 37396 | Schooner | 54 | 1857 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|---------------------------------|-------------------|
| Yarmouth | Dennis Horton, et al. | Yarmouth. |
| Shelburne | Benjamin Crawley | Yarmouth. |
| Shelburne | William McLean, et al | Shelburne. |
| Horton | John W. Barss, et al. | Windsor. |
| Halifax | Charles E. Reed | Halifax. |
| Sable River | J. Giffin | Liverpool, N. S. |
| Beaver Harbor . . . | Henry Balcam, et al. | Halifax. |
| Liverpool, N. S. . . | J. Sponagle, et al. | Liverpool, N. S. |
| Pompquet. | Daniel Dorion | Guysborough. |

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| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

H.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Haidee | 42350 | Brig..... | 123 | 1863 |
| Halifax | 35838 | Barque..... | 220 | 1854 |
| Hamilton | 52064 | Brig | 305 | 1865 |
| Hamlet | 41091 | Schooner | 126 | 1864 |
| Hannah | 37017 | Brigantine.... | 116 | 1861 |
| Hannah | 37017 | Brigantine .. | 116 | 1861 |
| Hannah | 37987 | Schooner | 34 | 1834 |
| Hannah ..... | ..... | Schooner | 17 | 1818 |
| Hannah ..... | ..... | Schooner | 31 | 1827 |
| Hannah ..... | ..... | Schooner | 31 | 1829 |
| Hannah | 37017 | Brigantine ... | 116 | 1861 |
| Hannah Johnston ..... | ..... | Schooner | 49 | 1825 |
| Hants ..... | ..... | Schooner | 21 | 1851 |
| Happy Return | 38283 | Schooner | 76 | 1840 |
| Happy Return | 38375 | Schooner | 24 | 1858 |
| Happy Return ..... | ..... | Schooner | 68 | 1832 |
| Happy Return ..... | ..... | Schooner | 30 | 1832 |
| Happy Return ..... | ..... | Schooner | 15 | 1842 |
| Happy Return | 48121 | Schooner | 15 | 1864 |
| Harriet | 38326 | Schooner | 26 | 1857 |
| Harriet | 38426 | Schooner | 49 | Unk'n |
| Harriet | 38445 | Schooner | 59 | 1863 |
| Harriet | 38651 | Schooner | 57 | 1864 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|-----------------------------------|-------------------|
| Shelburne | Thomas C. Kinnear, et al. | Halifax. |
| Cornwallis | John M. Watson, et al. | Halifax. |
| Cornwallis | Mary E. Harris, et al. | Windsor. |
| Pompquet | Charles Broussard, et al. | Pictou. |
| Shelburne | John Henry Tobin, et al. | Halifax. |
| Shelburne | John M. Tobin, et al. | Shelburne. |
| Yarmouth | Nathan Weston | Yarmouth. |
| Chester | Andrew Walker | Halifax. |
| Parrsborough | Archibald Lamb, et al. | Halifax. |
| Marie Joseph | William Smith | Halifax. |
| Shelburne | John Henry Tobin | Halifax. |
| Granville | John Johnston, et al. | Halifax. |
| Noel | Samuel Godfrey, et al. | Windsor. |
| Arichat | Eustice Tobin | Arichat. |
| Margaree | Joseph Doucette | Arichat. |
| Sheet Harbor | Felix Cochran | Halifax. |
| New Brunswick | James White | Halifax. |
| Spry Harbor | William Hanes | Halifax. |
| Bay of Islands | Charles E. Wiswell | Halifax. |
| Poulamond | Peter Porrier, et al. | Arichat. |
| Bucksport, U.S | Michael Belfontine | Arichat. |
| Poulamond | Henry Boudroit, et al. | Arichat. |
| North Sydney | John A. Moore | Sydney, C. B. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------------------|------------------|----------------|-------|-------------|
| Harriet..... | ..... | Schooner | 30 | 1841 |
| Harriet | 35337 | Schooner | 49 | 1851 |
| Harriet | 36178 | Schooner | 55 | 1858 |
| Harriet..... | ..... | Schooner | 12 | 1843 |
| Harriet | 36089 | Brigantine ... | 151 | 1857 |
| Harriet Livesly | 49095 | Barque..... | 365 | 1864 |
| Harriet Newall | 35802 | Schooner | 51 | 1851 |
| Harriet White | 35620 | Schooner . . . | 58 | 1850 |
| Harry & Aubray | 49456 | Brigantine ... | 222 | 1864 |
| Harrison Leib | 46290 | Barque..... | 238 | 1863 |
| Hariza | 35678 | Schooner | 52 | 1857 |
| Harvest Home | 36483 | Schooner | 29 | 1859 |
| Harvest Queen | 42116 | Schooner | 104 | 1860 |
| Harp..... | ..... | Schooner | 26 | 1844 |
| Harold | 43100 | Schooner | 85 | 1864 |
| Harmony | 41622 | Schooner ... | 50 | 1852 |
| Harmony | 41782 | Schooner | 68 | 1859 |
| Harmony..... | ..... | Schooner | 56 | 1830 |
| Harmony..... | ..... | Schooner | 56 | 1835 |
| Harmony..... | ..... | Schooner | 20 | 1839 |
| Hardscrabble | 38982 | Schooner | 26 | 1846 |
| Havelock | 38373 | Schooner ... | 61 | 1858 |
| Hawk | 36437 | Schooner | 41 | 1859 |
| Hawk..... | ..... | Schooner | 29 | 1846 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|-----------------------------------|-------------------|
| Cheticamp | Lazar White, et al. | Pictou. |
| New Edinburgh . . . | Daniel Walsh. | Digby. |
| Chezzetcook | William A. Bond. | Yarmouth. |
| Barrington. | John Bell. | Halifax. |
| Truro | Charles A. DeWolfe, et al | Halifax. |
| Pictou | William Ives, et al. | Pictou. |
| Hampshire, U. S . . | Roderick G. Morrison. | Arichat. |
| Little Arichat. . . . | E. Forrest | Arichat. |
| Pugwash | Charles Aubray DeWolf, et al . . | Pugwash. |
| Liverpool. | Collin Tapper, et al. | Liverpool, N. S. |
| Petite Riviere . . . | Frederick Croft, et al | Halifax. |
| Lunenburg | Jacob Gaetz, et al. | Lunenburg. |
| Cornwallis | Charles R. Burgess. | Windsor. |
| Westport | James Purcell | Halifax. |
| Tatamagouche . . . | James B. Oxley. | Halifax. |
| Tidnish. | Andrew Hays | Sydney, C. B. |
| Lunenburg | W. & J. Taylor, et al | Lunenburg. |
| Pictou | Henry Lawson. | Halifax. |
| Torbay | Kendall Homes. | Halifax. |
| Barrington | John Kenney, et al | Halifax. |
| Magdalen Islands. . | Thomas McDonald | Pictou. |
| Cape Breton | Henry Townsend. | Halifax. |
| Shelburne | Daniel Cronan. | Shelburne. |
| Sheet Harbor. . . . | Andrew Shires. | Halifax. |

| Vessel's Name. | Official Number. | Rlg. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Hawk | 36437 | Schooner | 41 | 1859 |
| Hazard | 37254 | Schooner | 51 | 1837 |
| Heber | 38640 | Schooner | 57 | 1862 |
| Hector | 38000 | Schooner | 14 | 1845 |
| Hector | 38468 | Schooner | 34 | 1865 |
| Heddy Vickars | 36243 | Brigantine ... | 188 | 1859 |
| Helena | 48253 | Barque. | 368 | 1863 |
| Helena | 48035 | Schooner | 118 | 1864 |
| Helen | 48266 | Brigantine ... | 195 | 1864 |
| Helen ..... | ..... | Schooner | 50 | 1849 |
| Helen ..... | ..... | Schooner | 37 | 1845 |
| Helen | 37493 | Schooner | 30 | 1849 |
| Helen | 42366 | Schooner | 42 | 1863 |
| Helen McGregor | 37468 | Schooner | 44 | 1853 |
| Helen Mar | 37818 | Brigantine... | 108 | 1838 |
| Helen Maria | 35792 | Schooner | 72 | 1853 |
| Hemisphere | 36064 | Schooner | 76 | 1843 |
| Henry | 37721 | Brigantine... | 108 | 1852 |
| Henry | 52008 | Schooner ... | 78 | 1865 |
| Henry Gilbert | 46016 | Brigantine ... | 184 | 1863 |
| Henry Le Blanc | 49472 | Schooner | 94 | 1864 |
| Henry Poole ..... | ..... | Brig. | 240 | 1853 |
| Henry Pelham | 49432 | Ship | 817 | 1864 |
| Henrietta | 38629 | Schooner | 73 | 1861 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|--|-------------------|
| Shelburne | William Pryor | Halifax. |
| Granville | Frederick W. Ruggles | Digby. |
| North Sydney | Benjamin Killam, Junr., et al. | Yarmouth. |
| Yarmouth | Crocker Hemeon | Yarmouth. |
| River Bourgeois . . . | G. H. Bissett | Arichat. |
| Green's Creek | Samuel Rettie, et al. | Halifax. |
| Shelburne | Thomas Coffin, et al | Shelburne. |
| Wilmot | Isaac P. Bonnett, et al | Annapolis. |
| Shelburne | Howard Locke, et al. | Shelburne. |
| Sable River | D. G. Ferguson, et al | Liverpool, N. S. |
| Musquodoboit | William Saunders, et al. | Halifax. |
| Port Mutton | Donald Campbell, et al. | Halifax. |
| Ship Harbor | William Hare | Halifax. |
| Shelburne | Lewis P. Churchill, et al | Shelburne. |
| Kempt | William Hinds, et al. | Windsor. |
| Lunenburg | Benjamin W. Salter, et al. | Halifax. |
| Maitland | Thomas D. Fletcher | Halifax. |
| Grand River | Desive LaVashe | Arichat. |
| Shelburne | Lewis P. Churchill, et al. | Shelburne. |
| Parrsborough | William Phinney, et al. | Parrsborough. |
| Clare | Henry LeBlanc, et al | Digby. |
| New Glasgow | George McKenzie | Pictou. |
| Tusket | John M. Lovett, et al. | Yarmouth. |
| Mira, C. B. | Henry James Gesner | Sydney, C. B. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|------------------|----------------|-------|-------------|
| Henrietta | 37214 | Schooner | 58 | 1844 |
| Henrietta | 38200 | Schooner | 33 | 1862 |
| Henrietta | 50798 | Schooner | 55 | 1864 |
| Herbert | 42077 | Schooner | 79 | 1861 |
| Herbert | 38272 | Schooner | 99 | 1863 |
| Herbert | 48475 | Brigantine ... | 111 | 1865 |
| Herbert Huntington .... | ..... | Schooner | 45 | 1848 |
| Herald | 37939 | Schooner | 38 | 1834 |
| Herald | 37827 | Brig | 231 | 1854 |
| Herald ..... | ..... | Schooner | 33 | 1841 |
| Herald | 36167 | Schooner | 43 | 1858 |
| Hercules..... | ..... | Schooner | 80 | 1833 |
| Hero | 38236 | Schooner | 56 | 1862 |
| Hero | 38188 | Barque..... | 373 | 1861 |
| Hero ..... | ..... | Schooner | 29 | 1816 |
| Hero .... | ..... | Schooner | 21 | Unk'n |
| Hero ..... | ..... | Schooner | 12 | 1840 |
| Hero ..... | ..... | Schooner .. | 42 | 1831 |
| Hero ..... | ..... | Schooner | 16 | 1845 |
| Hero | 42295 | Schooner ... | 46 | 1850 |
| Hero | 35975 | Schooner | 85 | 1856 |
| Heroine ..... | ..... | Schooner | 33 | 1832 |
| Herring ..... | ..... | Schooner | 57 | 1826 |
| Hesther | 38310 | Schooner | 89 | 1853 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|-------------------------------------|-------------------|
| Digby | Hubbard Davis | Digby. |
| Tusket | Prosper Smith, et al | Yarmouth. |
| Smith's Cove | Thomas Shaw | Halifax. |
| Clare | St. Clair Jones | Digby. |
| Yarmouth | George Killam, et al | Yarmouth. |
| Newport | Nelson Woolaver, et al | Windsor. |
| Jordan River | John McPherson, et al | Halifax. |
| St. Martin | William E. Clements, et al | Digby. |
| Hantsport | John B. North, et al | Windsor. |
| Petite Riviere | Samuel Drew, et al | Halifax. |
| Sable River | George Giffin, et al | Halifax. |
| Horton | Benjamin Borden, Junr., et al | Halifax. |
| Shelburne | J. C. Anderson, et al | Yarmouth. |
| Tusket | Aaron Goudry, et al | Yarmouth. |
| La Have | Henry Balcam | Halifax. |
| Unknown | Edward Nickerson, et al | Halifax. |
| Barrington | David Horton, et al | Halifax. |
| New Edinburg | Jacob Spicer | Halifax. |
| Cape Negro | John Doane, et al | Halifax. |
| La Have | Charles W. Wright | Halifax. |
| Argyle | Seth W. Crowell, et al | Halifax. |
| New Brunswick | Samuel Lynds | Halifax. |
| Cornwallis | Nathaniel K. Jenkins, et al | Halifax. |
| Grand Ruisseau | Victor LeBlanc | Arichat. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Hibernia ..... | ..... | Schooner | 37 | 1823 |
| Hibernia | 35675 | Schooner ... | 32 | 1848 |
| Highlander | 36438 | Schooner | 32 | 1859 |
| Highlander | 41816 | Schooner | 54 | 1862 |
| Highland Lass ..... | ..... | Schooner | 23 | 1851 |
| Highland Mary | 74114 | Brig | 161 | 1854 |
| Highland Mary | 49421 | Barque | 640 | 1864 |
| Highland Nancy | 46659 | Brig | 151 | 1863 |
| Hiram Spoffard | 48088 | Schooner | 76 | 1864 |
| Hiram ..... | ..... | Schooner | 53 | 1834 |
| Hiram | 37569 | Schooner | 66 | 1856 |
| Hirandale | 38460 | Schooner | 30 | 1846 |
| Home | 38189 | Ship | 1271 | 1861 |
| Homer | 36603 | Brigantine ... | 136 | 1859 |
| Hope | .. . | Schooner | 41 | 1847 |
| Hope | 37609 | Schooner | 117 | 1861 |
| Hope ..... | ..... | Schooner | 44 | 1829 |
| Hope ..... | ..... | Schooner | 55 | 1861 |
| Hope | 37962 | Schooner | 22 | 1839 |
| Hope | 37487 | Schooner | 68 | 1853 |
| Hope ..... | ..... | Schooner | 30 | 1820 |
| Hope ..... | ..... | Schooner | 31 | 1835 |
| Hope ..... | ..... | Schooner | 16 | 1838 |
| Hope ..... | ..... | Schooner | 21 | 1840 |

| Where built. | Registered Owner. | Port of Registry |
|----------------------------|-----------------------------------|------------------|
| Granville | William Outhouse, et al. | Halifax. |
| United States | William Outhouse, et al. | Halifax. |
| Ragged Island | Howard Locke | Shelburne. |
| Baddeck | Frederick McGrae | Baddeck. |
| Grand River | Joseph S. Catteny | Arichat. |
| Cornwallis | Ezra Churchill, et al. | Windsor. |
| Tusket | Nathaniel Gardner, et al. | Yarmouth. |
| Sandy Cove | John S. Eldridge, et al | Digby. |
| Tusket | Benjamin Rogers | Yarmouth. |
| Shelburne | Josiah Coffin, et al. | Halifax. |
| Lunenburg | James Challoner | Halifax. |
| Poulamond | I. & W. LeVesconte | Arichat. |
| Tusket | Samuel Killam | Yarmouth. |
| Parrsborough | John E. Crane, et al | Windsor. |
| Richibucto | Amos Seaman, et al. | Pictou. |
| Parrsborough | Francis Carroll, et al. | Parrsborough. |
| Yarmouth | John A. Ernst | Lunenburg. |
| Petite Riviere | J. C. Sperry | Liverpool, N. S. |
| Yarmouth | John B. Melancer | Digby. |
| Hillsburgh | Israel Setteney, et al. | Digby. |
| Cornwallis | Gery Elderkin, et al | Halifax. |
| Shelburne | Charles Beuce | Halifax. |
| St. Martin River | Peter Harman | Halifax. |
| Mahone Bay | George Zwicker | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------|------------------|----------------|-------|-------------|
| Hope | 35897 | Schooner | 17 | 1855 |
| Hope | 36124 | Schooner | 15 | 1855 |
| Hope | 36569 | Schooner | 34 | 1859 |
| Hope | 50775 | Schooner | 17 | 1840 |
| Hopeful ..... | ..... | Schooner | 37 | 1852 |
| Hopewell ..... | ..... | Schooner | 27 | 1824 |
| Horizon ..... | ..... | Schooner | 46 | 1831 |
| Hornet ..... | ..... | Schooner | 16 | 1836 |
| Horton | 48142 | Schooner | 121 | 1864 |
| Hound | 36091 | Brig | 149 | 1857 |
| H. O. Warren | 37337 | Schooner | 99 | 1857 |
| Howard | 49407 | Barque | 597 | 1864 |
| Humming Bird | 37566 | Schooner | 17 | 1854 |
| Humming Bird ..... | ..... | Schooner | 27 | 1847 |
| Humming Bird ..... | ..... | Schooner | 17 | 1832 |
| Humming Bird ..... | ..... | Schooner | 17 | 1845 |
| Humming Bird ..... | ..... | Schooner | 16 | 1847 |
| Humming Bird | 42336 | Schooner | 49 | 1847 |
| Humming Bird | 48462 | Brigantine.... | 237 | 1864 |
| Humber | 35771 | Ship | 772 | 1853 |
| Huntington | 48083 | Schooner | 30 | 1848 |
| Hunter | 48135 | Brigantine ... | 119 | 1864 |
| Hyades ..... | ..... | Brig | 219 | 1850 |
| Hylas ..... | ..... | Brig | 202 | 1828 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|-------------------------------------|-------------------|
| St. George's Bay, N.F. | Moses Huelin | Halifax. |
| Chezzetcook | Richard Stone | Halifax. |
| Country Harbor | James McMullin, et al. | Halifax. |
| Mahone Bay | Samuel Cartile | Halifax. |
| Mira, C. B. | Peter Kerr | Sydney, C. B. |
| Barrington | Ensign Nickerson | Halifax. |
| Newport | William Burgher | Halifax. |
| Jeddore River | Joshua Snow | Halifax. |
| Maitland | Frederick Brown, et al. | Halifax. |
| Shelburne | Robert Boak, Junr. | Halifax. |
| Clementsport | John Balcomb, et al. | Digby. |
| Shelburne | William Rogers, et al | Yarmouth. |
| La Have | George Shenkle | Lunenburg. |
| Ragged Island | J. Harding | Liverpool, N. S. |
| Barrington | William Squires, et al. | Halifax. |
| Barrington | Moses Crowell, et al | Halifax. |
| Bay of Islands | John Warren | Halifax. |
| Mahone Bay | John Taylor, et al | Halifax. |
| Hantsport | George W. Churchill, et al. | Windsor. |
| Windsor | Bennet Smith, et al. | Windsor. |
| Yarmouth | James F. Richardson | Yarmouth. |
| Noel | William Hunter, et al | Halifax. |
| Tatamagouche | Alexander Campbell, et al. | Pictou. |
| Truro | John J. Fulton | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------------------|------------------|----------------------|-------|-------------|
| H. B. Lewis | 48106 | Sloop | 31 | 1858 |
| H. Brooks | 49492 | Schooner | 107 | 1865 |
| H. C. Burke | 36136 | Schooner | 79 | 1858 |
| H. C. Bazely | 43178 | Brig | 121 | Unk'n |
| H. Havelock | 38067 | Schooner | 103 | 1858 |
| H. Havelock | 36615 | Brigantine | 108 | 1860 |
| H. M. Moyle | 37535 | Schooner | 55 | 1853 |

| Where built. | Registered Owner. | Port of Registry |
|--------------------|-------------------------|------------------|
| United States..... | John M. Watson | Halifax. |
| Weymouth | Henry Brooks | Digby. |
| Londonderry | James McCulloch..... | Halifax. |
| Unknown | Lewis P. Fairbanks..... | Halifax. |
| Tusket | John Clune | Yarmouth. |
| Kempt | David Smith, et al..... | Windsor. |
| Lunenburg..... | Edward Young | Lunenburg. |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

I.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------------|------------------|----------------|-------|-------------|
| Ice King..... | ..... | Brigantine.... | 100 | 1855 |
| Ida | 42224 | Schooner | 6 | 1861 |
| Ida | 38574 | Schooner | 37 | .... |
| Ida | 37674 | Schooner | 82 | 1861 |
| Ida | 37341 | Brigantine.... | 210 | 1857 |
| Ida | 41554 | Schooner | 40 | 1859 |
| Ida ..... | ..... | Brig | 94 | 1821 |
| Ida ..... | ..... | Schooner | 77 | 1859 |
| Ida C | 52036 | Brigantine ... | 178 | 1865 |
| Ida May | 49442 | Schooner | 8 | 1865 |
| Idella | 43086 | Brigantine.... | 179 | 1857 |
| Idolique | 50563 | Barque | 334 | 1864 |
| I. D. Payson | 46665 | Schooner | 36 | 1831 |
| I'm Here | 36292 | Schooner | 19 | 1860 |
| Independence ..... | ..... | Schooner | 32 | 1853 |
| Industry ..... | ..... | Schooner | 72 | 1851 |
| Industry | 37520 | Brigantine ... | 102 | 1858 |
| Industry ..... | ..... | Schooner | 61 | 1831 |
| Industry ..... | ..... | Schooner | 47 | 1835 |
| Industry ..... | ..... | Schooner | 21 | 1842 |
| Industry ..... | ..... | Schooner | 12 | 1844 |
| Industry .. | 35965 | Schooner | 12 | 1856 |
| Industry | 36226 | Schooner | 27 | 1859 |

| Where built. | Registered Owner. | Port of Registry. |
|-------------------------|-------------------------------------|-------------------|
| Tatamagouche . . . | Robert Logan | Pictou. |
| Clements | John E. Chute | Annapolis. |
| United States | Thomas Wilcox | Sydney, C. B. |
| River Herbert | Job Seaman, et al | Parrsborough. |
| Hillsburgh | Haley Morrison | Digby. |
| Cornwallis | Sylvanus Whitney | Windsor. |
| Shelburne | Jabez Snow | Halifax. |
| Pope's Harbor | Abraham Bolong, et al | Halifax. |
| Granville | T. A. Gavaza, et al | Annapolis. |
| Yarmouth | Nelson Gardner | Yarmouth. |
| Wiscassett, U. S. . . . | Edwin Pinkham | Pictou. |
| Shelburne | J. M. M. Willett | Shelburne. |
| United States | Joseph D. Payson, et al | Digby. |
| Isaac's Harbor | John McMillan | Halifax. |
| Manchester, U. S. . . . | William Forrestall, et al | Guysborough. |
| St. Mary's Bay | Holland E. Payson | Digby. |
| Liverpool | A. Knout | Liverpool, N. S. |
| Douglass | William Parker | Windsor. |
| St. Mary's | James Henlow, et al | Halifax. |
| Mahone Bay | John Burgoine | Halifax. |
| Port Medway | James A. Moren | Halifax. |
| Jedore | John Mitchell, et al | Halifax. |
| Chester | Rowland B. Curry, et al | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|------------------|----------------|-------|-------------|
| Indus ..... | ..... | Brigantine.... | 98 | 1846 |
| India | 37924 | Brig..... | 210 | 1858 |
| Indian Queen | 42338 | Schooner | 76 | 1861 |
| Index ..... | ..... | Brigantine ... | 133 | 1842 |
| Ino .... | ..... | Schooner | 35 | 1834 |
| Ino ..... | ..... | Sloop..... | 37 | 1846 |
| Integrity | 35635 | Schooner | 65 | 1850 |
| Inverness ..... | ..... | Schooner | 77 | 1841 |
| Investigator | 37587 | Schooner | 25 | 1857 |
| Irene..... | ..... | Schooner | 21 | 1823 |
| Iris | 37760 | Brigantine ... | 127 | 1855 |
| Iris | 37660 | Schooner | 58 | 1859 |
| Irvine | 38020 | Schooner | 502 | 1856 |
| Isaac Goodwin | 52002 | Schooner | 65 | 1865 |
| Isabella | 37365 | Schooner | 72 | 1857 |
| Isabella ..... | ..... | Schooner | 68 | 1849 |
| Isabella ..... | ..... | Schooner | 72 | 1827 |
| Isabella | ... | Schooner | 12 | 1842 |
| Isabella | 36262 | Schooner | 12 | 1848 |
| Isabella Thompson | 37176 | Brigantine ... | 104 | 1861 |
| Isabel | 34728 | Schooner | 33 | 1849 |
| Isadore..... | ..... | Schooner | 22 | 1839 |
| Ises..... | ..... | Barque..... | 497 | 1854 |
| Island Gem | 49447 | Schooner | 60 | 1865 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|-----------------------------------|-------------------|
| Kempt | Benjamin DeWolf, et al | Windsor. |
| Cornwallis | Joshua Newton Freeman, et al. . . | Windsor. |
| United States..... | Thomas James..... | Halifax. |
| Cornwallis | Abel Coalfleet, et al..... | Halifax. |
| Brier Island..... | Jesse Wyman..... | Halifax. |
| Lunenburg | Thomas F. Fillman..... | Halifax. |
| Sheet Harbor..... | Lauchlan McDonald | Sydney, C. B. |
| Londonderry | William Chisholm | Halifax. |
| La Have | John E. Holt..... | Halifax. |
| Barrington | David Hall..... | Halifax. |
| Wallace | Richard B. Huestis..... | Pictou. |
| Parrsborough..... | Aaron Hatfield, et al..... | Parrsborough. |
| Beaver River..... | Aaron Goudrey..... | Yarmouth. |
| Shelburne..... | Isaac Goodwin, et al | Shelburne. |
| Guysborough | Joseph William Hadley.. .. | Guysborough. |
| Baddeck..... | John Muggah..... | Sydney, C. B. |
| P. E. Island..... | Edward Tomlinson | Halifax. |
| St. Mary's Bay ... | James Hawbolt, et al | Halifax. |
| Marie Joseph..... | Charles W. Wright..... | Halifax. |
| New Brunswick... | James McDaniel | Halifax. |
| Weymouth | Weston Delap, et al..... | Digby. |
| Mahone Bay..... | Walter Pearl..... | Halifax. |
| Wallace | Robert Brown | Pictou. |
| Argyle | Thomas Simons, et al | Yarmouth. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------|------------------|---------------|-------|-------------|
| Island Home | 36596 | Ship | 750 | 1859 |
| Itasca | 42788 | Schooner | 38 | 1861 |
| Italia | 48102 | Brigantine... | 116 | 1864 |
| Ivy | 34744 | Schooner | 12 | 1853 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|------------------------------|-------------------|
| Windsor. | Bennet Smith, et al. | Windsor. . |
| Shelburne. | Thomas Johnson. | Shelburne. |
| Lunenburg. | William B. Hamilton. | Halifax. |
| Clements | Daniel Coughlin, et al. | Digby. |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

J.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------------|------------------|----------------|-------|-------------|
| Jack Easy | 37649 | Schooner | 43 | 1858 |
| J. A. Jones | 42783 | Schooner | 18 | 1861 |
| James Bailey | 37229 | Schooner ... | 87 | 1855 |
| James Coffill | 52054 | Brigantine ... | 205 | 1865 |
| James B. Duffus | 49417 | Barque | 672 | 1864 |
| James G. ..... | ..... | Schooner | 43 | 1852 |
| James Crowe | 42332 | Brigantine ... | 217 | 1862 |
| James & Eleanor | 42341 | Schooner | 30 | 1853 |
| James Henry | 42507 | Schooner | 22 | 1862 |
| James Henry | 36279 | Schooner | 15 | 1849 |
| James P. Milledge | 43049 | Schooner | 98 | 1862 |
| James A. McKeen | 36997 | Schooner | 49 | 1861 |
| James Pope | 38622 | Schooner | 35 | 1861 |
| James Patrick ..... | ..... | Schooner | 26 | 1853 |
| James Primrose | 49110 | Barque | 374 | 1865 |
| James Ratchford ..... | ..... | Schooner | 23 | 1829 |
| James Richard ..... | ..... | Schooner | 38 | 1838 |
| James Wellington | 37412 | Brigantine ... | 81 | 1846 |
| James William ..... | ..... | Schooner | 40 | 1835 |
| James William ..... | ..... | Schooner | 23 | 1836 |
| James William | 37495 | Schooner | 41 | 1841 |
| James William ..... | ..... | Schooner | 21 | 1847 |
| James William | 48117 | Schooner | 21 | 1846 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|---------------------------------|-------------------|
| Parrsborough. | Sylvanus Whitney, et al. | Parrsborough. |
| Jordan River. | Jacob L. Jones. | Shelburne. |
| Wilmot. | George Stymest. | Windsor. |
| Cornwallis. | James Coffill, et al. | Windsor. |
| Yarmouth. | William Burrell, et al. | Yarmouth. |
| Ship Harbor. | James G. McKeen. | Arichat. |
| Truro. | John Crowe, et al. | Halifax. |
| Terence Bay. | John McFatridge, et al. | Halifax. |
| Mahone Bay. | W. J. Boutilier. | Lunenburg. |
| Chester. | Thomas Brackett, et al. | Halifax. |
| Pompquet. | Pascal Besroit. | Pictou. |
| East Medway. | J. & W. Henrity. | Lunenburg. |
| Main-a-Dieu, C. B. | Michael Pope, et al. | Sydney, C. B. |
| Main-a-Dieu, C. B. | Richard English, et al. | Sydney, C. B. |
| River John. | James Kitchen, et al. | Pictou. |
| Parrsborough. | Samuel Morris, et al. | Halifax. |
| La Have. | William Foster, Junr. | Halifax. |
| Yarmouth. | W. V. Andrews, et al. | Lunenburg. |
| Jeddore. | James Webber. | Halifax. |
| La Have. | George Geeser. | Halifax. |
| Mahone Bay. | William Ernst, et al. | Halifax. |
| Pope's Harbor. | Charles E. Wiswell. | Halifax. |
| La Have. | Thomas A. S. DeWolfe & Son. ... | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|------------------|----------------|-------|-------------|
| James William John | 53602 | Brigantine ... | 120 | 1865 |
| James | 38630 | Schooner | 68 | 1862 |
| James ..... | ..... | Schooner | 23 | 1843 |
| James | 48115 | Schooner | 16 | 1863 |
| Jane Ann ..... | ..... | Schooner | 7 | 1840 |
| Jane Bell | 37012 | Brigantine ... | 100 | 1860 |
| Jane Grousby | 38947 | Schooner | 24 | 1852 |
| Jane McDougall | 36535 | Schooner | 82 | 1863 |
| Jane Murcheson | 38465 | Schooner | 45 | 1865 |
| Jane Ross | 36899 | Barque | 520 | 1861 |
| Jane & Sarah ..... | ..... | Schooner | 69 | 1834 |
| Jane | 46098 | Schooner | 77 | 1863 |
| Jane ..... | ..... | Schooner | 75 | 1840 |
| Jane | 41555 | Schooner | 18 | 1857 |
| Jane ..... | ..... | Schooner | 14 | 1835 |
| Jane ..... | ..... | Schooner | 12 | 1839 |
| Jane ..... | ..... | Schooner | 71 | 1848 |
| Jane ..... | ..... | Barque | 194 | 1849 |
| Jane | 42283 | Brigantine ... | 140 | 1861 |
| Jane | 48103 | Schooner | 21 | 1832 |
| Janet | 42789 | Schooner | 69 | 1861 |
| Janet | 42416 | Barque | 366 | 1864 |
| Janet | 46294 | Schooner | 30 | 1864 |
| Janet | 42219 | Barque | 240 | 1862 |

| Where built. | Registered Owner. | Port of Registry |
|---------------------|---------------------------------|------------------|
| River Inhabitants.. | James McQuarrie | Port Hawkesbury. |
| North Sydney | John Fraser..... | Sydney, C. B. |
| Pope's Harbor.... | John Bollong, Junr., et al..... | Halifax. |
| Country Harbor... | William Lintlop..... | Halifax. |
| Point Brule..... | Thomas Langelle..... | Pictou. |
| Shelburne..... | Joshua Snow, et al. | Shelburne. |
| Poulamond | Donald Beaton, et al. | Arichat. |
| Whycocomagh | Alexander McDonald, et al. | Port Hawkesbury. |
| Grand River | Hector Murcheson, et al..... | Arichat. |
| Pictou..... | John T. Ives, et al. | Pictou. |
| Argyle | John Van Morden..... | Halifax. |
| Shelburne..... | Jonathan Locke, et al | Shelburne. |
| Londonderry | Leonard Martin, et al | Windsor. |
| Parrsborough.... | Isaiah Morris, et al | Windsor. |
| Barrington | Matthew Pinkham, et al..... | Halifax. |
| Porter's Lake.... | James Croucher..... | Halifax. |
| Shelburne..... | William Muir..... | Halifax. |
| Londonderry | Calvin P. Soule..... | Halifax. |
| Prince Port | George B. McLellan, et al | Halifax. |
| Essex, U. S | Stephen Crispe, et al..... | Halifax. |
| Barrington | Thomas Crowell, et al..... | Shelburne. |
| Wallace | Joseph O'Brien, et al..... | Pugwash. |
| Port Medway | A. Morine | Liverpool, N. S. |
| Yarmouth..... | A. Cowie & Sons..... | Liverpool, N. S. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------------|------------------|----------------|-------|-------------|
| Janet | 49415 | Schooner | 63 | 1864 |
| Janet Kidston | 29776 | Brigantine.... | 140 | 1860 |
| J. A. Pierce | 44409 | Brig | 300 | 1863 |
| Jasper ..... | ..... | Schooner | 31 | 1838 |
| Jasper | 35758 | Schooner | 64 | 1850 |
| Java | 48037 | Brigantine ... | 162 | 1864 |
| Java ..... | ..... | Brig | 219 | 1851 |
| J. Banks. | 42400 | Brigantine.... | 130 | 1863 |
| J. B. Elliott | 42318 | Brigantine ... | 174 | 1862 |
| J. B. Fay | 50780 | Schooner | 48 | 1865 |
| J. B. Hirey | 36213 | Schooner | 26 | 1859 |
| J. B. King | 37912 | Brigantine ... | 125 | 1857 |
| J. Blenkhorn | 42840 | Brigantine ... | 229 | 1861 |
| J. C. Harrison (Steam tug). | 38643 | ..... | 33 | 1862 |
| J. C. Miller | 41789 | Schooner | 41 | 1859 |
| J. Congdon | 36346 | Brigantine.... | 125 | 1861 |
| J. C. Shaw | 38164 | Brigantine.... | 126 | 1861 |
| J. Cummings | 42353 | Barque | 455 | 1863 |
| J. D. Lohnes | 37585 | Schooner | 20 | 1857 |
| J. D. Sperry | 36190 | Schooner | 61 | 1859 |
| J. E. Lockheart | 41558 | Barque | 405 | 1859 |
| J. E. Smith | 51773 | Schooner | 45 | Unk'n |
| Jean Lithgow | 50778 | Barque | 335 | 1854 |
| Jeanetta | 42502 | Schooner | 28 | 1862 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|-------------------------------------|-------------------|
| St. Mary's Bay ... | Benjamin Ellenwood, et al | Yarmouth. |
| Cornwallis | William Clarke, et al | Windsor. |
| Granville | Lawrence Delap, et al. | Annapolis. |
| Port LaBear | James Harding | Halifax. |
| Barrington | Samuel Nickerson, et al. | Halifax. |
| Granville | Richard Clark, et al | Annapolis. |
| River John | C. Foster | Liverpool, N. S. |
| Barrington | Joseph Banks, et al. | Halifax. |
| Maitland | John B. Elliott, et al. | Halifax. |
| Bay of Islands | George Fraser, et al | Halifax. |
| Petite Riviere | John B. Hirey, et al | Halifax. |
| Cornwallis | James Coffill, et al. | Windsor. |
| Parrsborough | John Blenkhorn, et al. | Windsor. |
| United States | Edward P. Archbold. | Sydney, C. B. |
| Lunenburg | Casper Miller. | Lunenburg. |
| Truro | — — — — — | Halifax. |
| Shelburne | Samuel Ryerson, et al. | Yarmouth. |
| Londonderry | John Geddes, Junr., et al | Halifax. |
| La Have | James Zink, et al. | Lunenburg. |
| Petite Riviere | John C. Sperry, et al. | Halifax. |
| Cornwallis | Jacob Lockhart, et al. | Windsor. |
| United States | J. D. McLearn, et al. | Liverpool. |
| Lubec, U. S. | John Lithgow. | Halifax. |
| La Have | B. Wentzell, et al | Lunenburg. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Jeanette | 50773 | Schooner | 32 | 1864 |
| Jedore.... | ..... | Schooner | 51 | 1847 |
| Jenny Lind | 37235 | Schooner | 69 | 1851 |
| Jenny Lind | 36188 | Schooner | 17 | 1859 |
| Jenny Lind | 36273 | Schooner | 14 | 1859 |
| Jenny Moody | 38185 | Brig | 312 | 1861 |
| Jenny Berteaux | 46400 | Barque..... | 597 | 1864 |
| Jenny | 36008 | Schooner | 10 | 1848 |
| Jessie Coffin | 46084 | Barque | 816 | 1862 |
| Jessie Campbell | 42085 | Barque..... | 335 | 1861 |
| Jessie ..... | ..... | Barque..... | 294 | 1850 |
| Jessie ..... | ..... | Brigantine ... | 168 | 1850 |
| Jessie | 41819 | Schooner | 27 | 1863 |
| Jessie | 38582 | Schooner | 33 | 1856 |
| Jessie | 34925 | Schooner | 33 | 1834 |
| Jessie | 46373 | Barque..... | 434 | 1863 |
| Jessie | ... | Schooner | 18 | 1845 |
| Jessie | 42185 | Schooner | 60 | 1861 |
| J. F. Spencer .. | 42397 | Barque..... | 308 | 1863 |
| J. G. Morton | 46296 | Barque..... | 359 | 1826 |
| J. J. Marshall | 37427 | Schooner | 34 | 1861 |
| J. L. Vogler | 42202 | Schooner | 52 | 1861 |
| J. M. Morales | 49084 | Barque..... | 513 | 1864 |
| J. M. Maguire | 53604 | Schooner .. | 114 | 1865 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|---|-------------------|
| Chezzetcook | William Smith | Halifax. |
| Jeddore | William Roche | Halifax. |
| Granville | William Mechan, et al | Digby. |
| Chezzetcook | Arthur Ball | Halifax. |
| Jeddore | John Mitchell | Halifax. |
| New Brunswick . . . | John W. Lovett, et al. | Yarmouth. |
| Cornwallis | Charles W. Barnstead, et al. | Windsor. |
| Halifax | John Campbell | Halifax. |
| Clyde River | Thomas Coffin, et al | Shelburne. |
| Weymouth | Colin Campbell, Junr | Digby. |
| Pugwash | Levi W. Eaton | Pictou. |
| Tatamagouche | Alexander Campbell, et al | Pictou. |
| Foreign | John Farnaux | Baddeck. |
| Baddeck, C. B. . . . | John Campbell | Sydney, C. B. |
| Bridgetown | Gilbert Sanderson | Yarmouth. |
| Maitland | Alexander A. McDougall, et al | Windsor. |
| Marie Joseph | Charles E. Wiswell | Halifax. |
| Three Rivers | George E. Slack, et al | Halifax. |
| Londonderry | Jeremiah F. Spencer, et al | Halifax. |
| Rochester, U. S. . . . | J. Eaton, et al | Liverpool, N. S. |
| Manchester | David Grouchy | Arichat. |
| Port Medway | G. L. Vogler, et al | Liverpool, N. S. |
| New Glasgow | Peter Crerar | Pictou. |
| Strait of Canso . . . | Alexander McGuire, et al | Port Hawkesbury. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|------------------|----------------|-------|-------------|
| J. Morton ... | 46389 | Schooner ... | 129 | 1863 |
| J. M. C. M. ... | 36223 | Schooner | 62 | 1859 |
| John Augustus .. | 38659 | Schooner | 15 | 1864 |
| John Bull | 36905 | Schooner | 107 | 1861 |
| John B. Campbell | 49454 | Brigantine... | 199 | 1864 |
| John Begley | 46009 | Schooner | 40 | 1863 |
| John Benzon | 37546 | Schooner .. . | 84 | 1849 |
| John Butler | 37577 | Brigantine ... | 138 | 1856 |
| John Alexander | . . . | Schooner | 33 | 1847 |
| John C. Archibald | 38544 | Schooner | 79 | 1848 |
| John Duffus | 37703 | Brigantine .. | 224 | 1852 |
| John G. Hall | 41809 | Schooner | 96 | 1860 |
| John Good | 49489 | Brig | 343 | 1865 |
| John Givan | 48473 | Brigantine ... | 185 | 1865 |
| John Gilpin..... | .... | Schooner ... | 55 | 1826 |
| John Gilpin | 34720 | Schooner | 62 | 1850 |
| John Geddes .. | 36222 | Schooner | 154 | 1859 |
| John Henry | 35720 | Schooner | 39 | 1846 |
| John Henry..... | ..... | Schooner | 35 | 1830 |
| John Henry..... | ..... | Schooner | 34 | 1845 |
| John Henry | 35888 | Schooner | 27 | 1843 |
| John Henry | 36072 | Schooner | 37 | 1857 |
| John Joseph | 35849 | Schooner | 29 | 1854 |
| John Lachlan | 41817 | Schooner | 34 | 1863 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|------------------------------------|-------------------|
| Parrsborough. . . | Nathan Bennet Morris, et al. . . . | Windsor. |
| Isaac's Harbor. . . | James R. Kenney | Halifax. |
| Newfoundland . . . | John McIsaac | Sydney, C. B. |
| Antigonish | Michael Boudroit, et al | Arichat. |
| Pugwash | John Henry Black, et al. | Pugwash. |
| Parrsborough. . . . | James Willager, et al | Parrsborough. |
| Cornwallis | W. J. Bigelow, et al | Windsor. |
| La Have | R. Butler, et al | Lunenburg. |
| Jeddore. | James A. Moren | Halifax. |
| Main-a-Dieu, C. B. | D. H. Fultz, et al | Sydney, C. B. |
| River John | A. LeMarchand. | Arichat. |
| Clements | James Gilleat, et al. | Annapolis. |
| Digby. | George N. Crowell, et al. | Digby. |
| Givan's Wharf. . . . | Alexander Givan, et al. | Windsor. |
| Cornwallis | James Kinsman, et al. | Halifax. |
| Shelburne. | James Muir, et al. | Halifax. |
| Londonderry | John Geddes, et al. | Halifax. |
| Port Medway. | James McDonald. | Lunenburg. |
| Jeddore. | Henry Myers, et al. | Halifax. |
| Mahone Bay | James McNab | Halifax. |
| La Have | John B. Hammer. | Halifax. |
| Guysborough. . . . | James McDaniel. | Halifax. |
| Newfoundland . . . | William Shaw | Halifax. |
| Boularderie. | John Livingston. | Baddeck. |

| Vessel's Name. | * Official Number. | Rig. | Tons. | When built. |
|-----------------------------------|--------------------|---------------|-------|-------------|
| John & Louisa Wallace. | ..... | Schooner | 33 | 1839 |
| John McKean | 37584 | Schooner | 48 | 1857 |
| John Nelson | 37911 | Schooner | 94 | 1857 |
| John Northup | 46376 | Schooner | 125 | 1860 |
| John Oliver | 37207 | Brigantine... | 89 | 1863 |
| John Pendleton | 37288 | Schooner | 85 | 1856 |
| John Richards | 37979 | Brig | 274 | 1855 |
| John & Rachel | 35623 | Schooner | 35 | 1833 |
| John S. Wainwright ... | 48257 | Schooner | 87 | 1864 |
| John Silver | 36032 | Schooner | 52 | 1856 |
| John Sanderson | 42389 | Schooner | 157 | 1863 |
| John Thomas | 35787 | Schooner | 42 | 1845 |
| John Tilton | 39068 | Schooner | 65 | 1856 |
| John Taylor | 50760 | Schooner | 32 | 1864 |
| John William | 36154 | Schooner | 31 | 1858 |
| John William | 42282 | Schooner ... | 34 | 1861 |
| John W. Lovitt | 38016 | Brig | 224 | 1856 |
| John | 37391 | Schooner | 36 | 1857 |
| John ..... | ..... | Sloop | 12 | 1854 |
| John ... .. | ..... | Schooner | 30 | 1834 |
| John ..... | ..... | Schooner | 42 | 1828 |
| John ..... | ..... | Schooner | 22 | 1828 |
| Jolly Tar | 36214 | Schooner | 35 | 1859 |
| Jona | 43073 | Barque. | 368 | 1863 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------------|--------------------------------------|-------------------|
| Mahone Bay | John Nause | Halifax. |
| La Have | James Ross, et al. | Luncenburg. |
| Green's Creek | Elias Nelson, et al. | Windsor. |
| Cornwallis | John Northup, et al. | Windsor. |
| Weymouth | Henry Grant, et al | Digby. |
| Digby | Elias Turean, et al | Digby. |
| Yarmouth | Thomas Killam | Yarmouth. |
| St. Margaret's Bay | John H. McNab | Halifax. |
| Jordan River | Samuel Locke, Senr., et al | Shelburne. |
| Spry Harbor | George H. Hillsbury | Halifax. |
| Truro | Thomas Carroll, et al | Halifax. |
| Sheet Harbor | Thomas Gallant, et al | Halifax. |
| P. E. Island | William H. Rudolf | Halifax. |
| P. E. Island | David Petrie | Halifax. |
| Corduroy, N. F. | William Carter, et al | Halifax. |
| Louisburg | John B. Elliott, et al | Halifax. |
| Yarmouth | J. W. Lovitt, et al | Yarmouth. |
| Cape Canso | James Bourgue | Guysborough. |
| Cornwallis | Ebenezer Bigelow | Windsor. |
| Jeddore | Paul Hall | Halifax. |
| New Brunswick | Charles D. Card, et al | Halifax. |
| Tusket River | David Crowell | Halifax. |
| Mahone Bay | George Garrison | Halifax. |
| New Glasgow | John Crerar, et al | Pictou. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|-------------------------------|------------------|----------------|-------|-------------|
| Joseph | 37696 | Brigantine ... | 141 | 1849 |
| Joseph | 39016 | Schooner | 30 | 1844 |
| Joseph | ... | Schooner | 57 | 1847 |
| Joseph Creighton | 36490 | Schooner | 43 | 1860 |
| Joseph Dexter | 37462 | Barque. | 203 | 1855 |
| Joseph Hume | 35859 | Brigantine ... | 171 | 1854 |
| Joseph Howe | ... | Schooner | 16 | 1847 |
| Josephine | 38346 | Schooner | 80 | 1856 |
| Josephine | 38604 | Schooner | 71 | 1859 |
| Josephine | 38182 | Barque. | 318 | 1861 |
| Josephine ..... | ..... | Schooner | 24 | 1853 |
| Josephine | 42284 | Brigantine ... | 227 | 1861 |
| J. R. C. | 37404 | Schooner | 14 | 1855 |
| J. R. Gorkum | 36471 | Schooner | 29 | 1859 |
| J. Steele | 46375 | Barque. | 568 | 1863 |
| J. S. Risser | 41778 | Schooner . . . | 35 | 1859 |
| Jubilee | 37700 | Schooner | 86 | 1849 |
| Judique ..... | ..... | Schooner | 29 | 1816 |
| Judique ..... | ..... | Schooner | 20 | 1838 |
| Julia | 38410 | Schooner | 40 | 1860 |
| Julia | 42122 | Schooner | 125 | 1861 |
| Julia ..... | ..... | Schooner | 20 | 1853 |
| Julia | 36296 | Schooner | 113 | 1860 |
| Julia | 38909 | Schooner | 99 | 1854 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|-----------------------------------|-------------------|
| Little Arichat. | Simon Terrio. | Arichat. |
| P. E. Island. | Benjamin W. Franty. | Guysborough. |
| Pope's Harbor. | Thomas Phelan. | Halifax. |
| Lunenburg. | J. Cruickshank, et al. | Lunenburg. |
| Liverpool. | H. M. Secretary of State for War. | Halifax. |
| Argyle. | William K. Dudman. | Yarmouth. |
| Jordan River. | Alexander McKenzie. | Halifax. |
| Arichat. | Augustus Terrio. | Arichat. |
| Grand Mire. | Patrick Farrell, et al. | Sydney, C. B. |
| Tusket. | George S. Brown. | Yarmouth. |
| Chezzetcook. | John Smith. | Halifax. |
| Truro. | William Mahon. | Halifax. |
| Canso. | William Johnston Bigelow. | Guysborough. |
| Lunenburg. | John Hunter, et al. | Lunenburg. |
| Cornwallis. | Ezra Churchill, et al. | Windsor. |
| Lunenburg. | John B. Fay. | Halifax. |
| Grand Ruisseau. | Charles Broussard. | Arichat. |
| Cape Breton. | John Fraser, et al. | Halifax. |
| Torbay. | Michael Pelrang. | Halifax. |
| Bras d'Or Lake. | Alexander LeBlanc. | Arichat. |
| Kempt. | George Armstrong, et al. | Windsor. |
| Chezzetcook. | William Oakes. | Halifax. |
| Old Barns. | Robert Smith, et al. | Halifax. |
| Clements. | Edward Fenton. | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|---------------------|----------------|-------|----------------|
| Julia | 38265 | Ship | 894 | 1863 |
| Julia Franklin | 37611 | Schooner | 21 | 1858 |
| Julia Kelly | 49017 | Brigantine.... | 251 | 1865 |
| Julia Ann..... | ..... | Schooner | 32 | 1845 |
| Juno ..... | ..... | Brig..... | 231 | 1864 |
| Juno | 48477 | Ship | 955 | 1865 |
| Junaita | 52007 | Schooner | 15 | 1865 |
| Junietia | 36482 | Schooner | 27 | 1859 |
| J. & R. Young | 38051 | Barque..... | 509 | 1857 |
| J. V. Troop | 37301 | Schooner | 80 | 1856 |
| J. W. Harris | 41553 | Brig | 205 | 1859 |
| J. W. Hatfield | 46024 | Schooner | 81 | 1862 |
| J. W. Johnson | 37498 | Brig..... | 166 | 1856 |
| J. W. Mullock | 36489 | Schooner | 43 | 1860 |
| J. W. Deering | 36527 | Schooner | 104 | 1858 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|------------------------------------|-------------------|
| St. Mary's Bay . . . | J. K. Ryerson, et al | Yarmouth. |
| La Have | Henry Lohnes | Lunenburg. |
| Parrsborough | John Blenkhorn, et al | Parrsborough. |
| Bay of Islands | George Harvey | Halifax. |
| Granville | Ezra Foster, et al | Annapolis. |
| Newport | Thomas Curry, et al | Windsor. |
| Jordan River | Samuel Doane, et al | Shelburne. |
| La Have | W. Wilkie, et al | Lunenburg. |
| Yarmouth | A. F. Stoneman | Yarmouth. |
| Wilmot | Granville B. Reed, et al | Digby. |
| Walton | James W. Harris, et al | Windsor. |
| Parrsborough | Henry Potter | Windsor. |
| Cole Harbor | G. H. Frink | Liverpool, N. S. |
| La Have | Joseph Mullock | Lunenburg. |
| Jonesboro | Edward Kennedy, et al | Port Hawkesbury. |

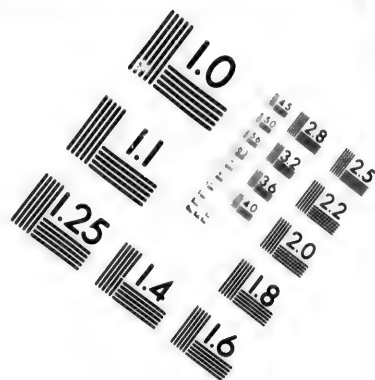
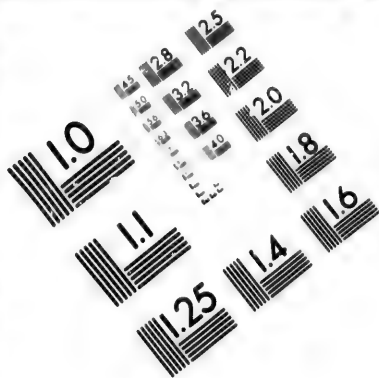
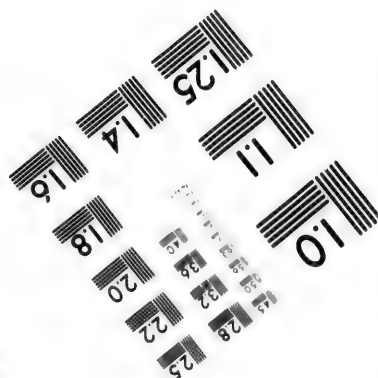
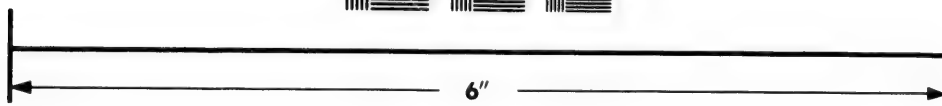
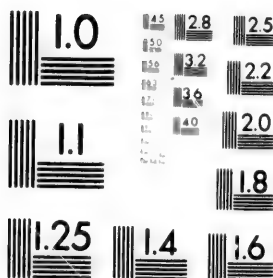


IMAGE EVALUATION TEST TARGET (MT-3)



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| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

K.

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Kalafat | 38295 | Schooner | 72 | 1854 |
| Kalamo | 46363 | Brigantine ... | 151 | 1863 |
| Katherine Maude | 43052 | Barque..... | 338 | 1862 |
| Katherine | 8088 | Barque..... | 587 | 1854 |
| Kate | 48086 | Schooner | 48 | 1849 |
| Kate | 36534 | Schooner | 25 | 1863 |
| Kate..... | ..... | Schooner | 20 | 1813 |
| Kate..... | ..... | Schooner | 16 | 1845 |
| Kate | 36079 | Schooner | 13 | 1850 |
| Kate | 36224 | Schooner | 14 | 1859 |
| Kate | 35803 | Schooner | 58 | 1853 |
| Kate | 42308 | Brigantine ... | 120 | 1862 |
| Kate Freeman | 46303 | Brigantine ... | 254 | 1864 |
| Kate Smith | 49422 | Barque..... | 409 | 1864 |
| Kedron | 49004 | Schooner | 118 | 1864 |
| Kensington .. | 38220 | Barque..... | 828 | 1862 |
| Kennett | 42796 | Brigantine ... | 98 | 1862 |
| Kildare | 52063 | Brigantine.... | 210 | 1865 |
| Kingbird | 46384 | Barque..... | 751 | 1863 |
| Kingfisher..... | ..... | Schooner | 24 | 1832 |
| Kingfisher..... | ..... | Schooner | 26 | 1836 |
| King of Tyre | 49117 | Barque..... | 259 | 1865 |
| Kingston | 42302 | Schooner | 84 | 1862 |

| Where built. | Registered Owner. | Port of Registry |
|---------------------|----------------------------------|------------------|
| Magdalen Islands . | John Martell, et al. | Arichat. |
| Advocate. | William Morris, et al. | Windsor. |
| New Glasgow | Daniel Ward Job. | Pictou. |
| Argyle | W. H. Moody, et al. | Yarmouth. |
| United States. | N. McConnell, et al. | Yarmouth. |
| Margaree | Henry Taylor, et al. | Port Hawkesbury. |
| Lunenburg | John Wilkie. | Halifax. |
| Petpiswick | Frederick Myers, et al. | Halifax. |
| Chezzetcook | Thomas C. Kinnear. | Halifax. |
| Liscomb Harbor. . | John McKinlay, Junr. | Halifax. |
| Sheet Harbor. | Charles J. Wylde. | Halifax. |
| Old Barns | Charles B. Whidden, et al. | Halifax. |
| Liverpool | Stewart Freeman. | Liverpool, N. S. |
| Shelburne | Benjamin Hilton, et al. | Yarmouth. |
| Parrsborough | Charles Smith, et al. | Parrsborough. |
| Tusket | Nathan Titley, et al. | Yarmouth. |
| Shelburne. | L. M. M. Willett, et al. | Shelburne. |
| Kempt | Archibald McDougall, et al. | Windsor. |
| Newport. | G. P. Payzant, et al. | Windsor. |
| Cape North | John Bourinot. | Sydney, C. B. |
| Ragged Island. | Samuel Locke, Junr., et al. | Halifax. |
| Tatamagouche . . | Robert Logan. | Pictou. |
| Guysborough | Benjamin Wier, et al. | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------|---------------------|----------------|-------|----------------|
| Kirtland | 7519 | Brigantine.... | 146 | 1850 |
| Kitty ..... | ..... | Schooner | 12 | 1819 |
| Kitty ... .. | ..... | Schooner | 14 | 1837 |

| Where built. | Registered Owner. | Port of Registry |
|--------------|-------------------|------------------|
| | | |

L.

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|---------------------------------|------------------|---------------|-------|-------------|
| La Bras d'Or | 35940 | Schooner | 82 | 1852 |
| Labaramus | 38208 | Barque | 464 | 1862 |
| Lady ..... | ..... | Schooner | 33 | 1820 |
| Lady ..... | ..... | Schooner | 11 | 1830 |
| Lady ..... | ..... | Schooner | 12 | 1852 |
| Lady Active ..... | ..... | Schooner | 28 | 1842 |
| Lady Catherine ..... | ..... | Schooner | 36 | 1844 |
| Lady Caroline ..... | ..... | Schooner | 36 | 1847 |
| Lady Dundas | 36402 | Schooner | 104 | 1859 |
| Lady Ellen ..... | ..... | Schooner | 37 | 1848 |
| Lady Franklin | 50785 | Brigantine... | 130 | 1865 |
| Lady Farewell ..... | .... | Schooner | 20 | 1842 |
| Lady Havelock | 41531 | Schooner | 71 | 1858 |
| Lady Hunter ..... | ..... | Schooner | 31 | 1839 |
| Lady Lilford ..... | ..... | Schooner | 35 | 1840 |
| Lady Mulgrave | 38372 | Schooner | 108 | 1858 |
| Lady Mulgrave | 38130 | Brig | 287 | 1860 |
| Lady Mulgrave | 41906 | Schooner | 129 | 1861 |
| Lady Mary | 37602 | Schooner | 13 | 1857 |
| Lady Milne | 36337 | Schooner | 49 | 1861 |
| Lady of Clare ..... | ..... | Schooner | 24 | 1837 |
| Lady of the Forest | 34879 | Schooner | 21 | 1848 |
| Lady Paget | 37570 | Schooner | 32 | 1845 |

| Where built. | Registered Owner. | Port of Registry |
|--------------------|-------------------------------|------------------|
| Voipe's Harbor.... | John Cronan .. | Halifax. |
| Pleasant Cove.... | George S. Brown, et al. | Yarmouth. |
| Shelburne..... | Othoneal Beale .. | Halifax. |
| Mahone Bay ... | George Conrod, et al. | Halifax. |
| New Dublin..... | George Worten .. | Halifax. |
| La Have ... | George Wolf..... | Halifax. |
| La Have | J. Brown, et al..... | Lunenburg. |
| La Have | Leonard Walter, et al .. | Halifax. |
| Murray Harbor... | Allen Landry..... | Halifax. |
| Lunenburg | Gasper Miller.... | Halifax. |
| Five Islands..... | Joseph C. Morrison, et al .. | Halifax. |
| La Have | R. Gorham | Halifax. |
| Little River..... | Elisha William Kindle .. | Guysborough. |
| Liverpool | Harrington McLeod, et al..... | Halifax. |
| New Dublin..... | Nicholas Romkey .. | Halifax. |
| Arichat..... | Albert Marraud .. | Arichat. |
| Beaver Harbor.... | George S. Brown, et al..... | Yarmouth. |
| Ship Harbor..... | John Carlin, et al..... | Halifax. |
| Lunenburg..... | T. N. Reynolds, et al .. | Lunenburg. |
| Ship Harbor..... | James O'Hara .. | Halifax. |
| Clare | William Anthony, et al..... | Halifax. |
| Barrington | John B. Lawrence..... | Halifax. |
| La Have | Benjamin Venot, et al..... | Lunenburg. |

| Vessel's Name. | Official Number. | Rlg. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Lady Sale | 38309 | Brigantine ... | 103 | 1849 |
| Lady Sale | 37761 | Schooner | 164 | 1855 |
| Lady Sale | 33832 | Schooner | 60 | 1844 |
| Lady Sale | 36040 | Schooner | 13 | 1856 |
| Lady Sale | 36056 | Schooner | 17 | 1847 |
| Lady Sarah | 35976 | Schooner | 22 | 1848 |
| Lady Speedwell | 36495 | Schooner | 53 | 1860 |
| Lady Seymour ... | 35907 | Schooner | 46 | 1853 |
| Lady Seymour | 36120 | Schooner | 22 | 1858 |
| Lady Wellington | 3 | Schooner | 71 | 1817 |
| Lady Washington | 36299 | Brigantine ... | 102 | 1860 |
| Lahave..... | ..... | Schooner | 18 | 1817 |
| Lakemba | 46398 | Barque..... | 584 | 1863 |
| Laleah | 38914 | Schooner | 35 | 1851 |
| Laleah | 37233 | Schooner | 108 | 1855 |
| Landoro | 46013 | Brigantine... | 215 | 1863 |
| Landho | 39856 | Schooner | 63 | 1854 |
| Landscape .. | 36598 | Schooner | 31 | 1859 |
| Lapwing | 37416 | Schooner | 35 | 1835 |
| Lapwing ..... | ..... | Schooner .. | 103 | 1853 |
| L'Ardoise | 38395 | Schooner | 27 | 1859 |
| Lark | 37322 | Schooner | 18 | 1838 |
| Lark | 37558 | Schooner | 66 | 1819 |
| Lark | 37590 | Schooner | 20 | 1856 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|---------------------------------|-------------------|
| Arichat..... | Isaiah Boudrot..... | Arichat. |
| Harbor au Bouche. | Edward Reeshaud..... | Arichat. |
| Guysborough..... | Edward J. Cunningham, et al.... | Halifax. |
| Torbay..... | Joseph Mahar, et al..... | Halifax. |
| Chezzetcoek..... | Dennis Smith..... | Halifax. |
| New Dublin..... | W. & C. Cohoon..... | Liverpool, N. S. |
| Mahone Bay..... | Jesse Heckman, et al..... | Lunenburg. |
| Petpiswick..... | Henry P. Bouring..... | Halifax. |
| Terence Bay..... | Thomas Murray, et al..... | Halifax. |
| New Brunswick... | Peleg Holmes, Senr..... | Halifax. |
| Five Islands..... | John Morrison, et al..... | Halifax. |
| La Have..... | Lewis Sarty..... | Halifax. |
| Hantsport..... | Ezra Churchill, et al..... | Windsor. |
| Wilmot..... | James McLeod..... | Digby. |
| Hillsborough..... | Wm. F. Marshall, et al..... | Digby. |
| Parrsborough..... | Charles Smith, et al..... | Parrsborough. |
| Sable River..... | J. P. Churchill, et al..... | Liverpool, N. S. |
| Walton..... | John M. Graham..... | Windsor. |
| Ragged Island.... | George Pench..... | Liverpool, N. S. |
| St. Mary's..... | James F. Foster, et al..... | Halifax. |
| L'Ardoise..... | James Butler..... | Arichat. |
| Brier Island..... | George Matthews..... | Digby. |
| Scituate, U. S..... | C. C. Paitz..... | Lunenburg. |
| La Have..... | George Zwicker..... | Lunenburg. |

| Vessel's Name. | Official Number. | Bldg. | Tons. | When built. |
|------------------------|------------------|---------------|-------|-------------|
| Lark..... | ..... | Schooner | 82 | 1832 |
| Lark..... | ..... | Schooner | 43 | 1816 |
| Lark..... | ..... | Schooner | 17 | 1817 |
| Lark..... | ..... | Schooner | 97 | 1829 |
| Lark..... | ..... | Schooner ... | 7 | 1838 |
| Lark..... | ..... | Schooner | 13 | 1839 |
| Lark... .. | 36147 | Schooner | 38 | 1847 |
| Lark..... | ..... | Schooner | 13 | 1843 |
| Lark..... | 33247 | Schooner | 61 | 1852 |
| Lark..... | 35756 | Schooner | 39 | 1844 |
| Lark..... | ..... | Schooner | 22 | 1855 |
| Lark... .. | 36260 | Schooner | 64 | 1817 |
| Last Shift..... | ..... | Schooner | 29 | 1821 |
| Latina..... | 34074 | Schooner | 92 | 1857 |
| Latour..... | 41911 | Schooner | 65 | 1858 |
| Laura..... | 38299 | Schooner | 74 | 1845 |
| Laura..... | 48036 | Schooner | 138 | 1864 |
| Laurel..... | ..... | Schooner | 28 | 1827 |
| Laurel..... | ..... | Schooner | 33 | 1845 |
| Lavinia..... | ..... | Schooner | 31 | 1848 |
| Lavinia..... | 41533 | Schooner | 31 | 1853 |
| Lavinia Elizabeth..... | 36076 | Schooner | 23 | 1857 |
| Lavinia & Sarah..... | 41532 | Schooner | 41 | 1858 |
| Laurence Power..... | ..... | Schooner | 36 | 1844 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|---------------------------------|-------------------|
| St. Martins, N. B.. | John Allison, et al..... | Windsor. |
| Barrington | James Ratchford..... | Halifax. |
| Port Mutoun..... | Robert Currie..... | Halifax. |
| Yarmouth..... | George Bingay..... | Halifax. |
| Clare | Charles Murray..... | Halifax. |
| Barrington | Joseph Atkinson..... | Halifax. |
| La Have | William Barry..... | Halifax. |
| Douglas | John Allison, Junr..... | Halifax. |
| La Have | John Allen, et al..... | Halifax. |
| Ship Harbor..... | Augustus E. Stayner, et al..... | Halifax. |
| New Harbor..... | Moses Bears, et al..... | Halifax. |
| United States..... | Simon F. Barss..... | Halifax. |
| Newfoundland | Robert M. Brown..... | Halifax. |
| Newfoundland | William P. West, et al. | Halifax. |
| Barrington | Peter Kerr..... | Halifax. |
| Averport | Simon LeBlanc..... | Arichat. |
| Clements | Charles Ditmars..... | Annapolis. |
| Argyle | John Carney, et al..... | Halifax. |
| Hillsborough | John Breton, et al..... | Halifax. |
| Wilmot..... | Benjamin R. Pentz..... | Pictou. |
| Guysborough..... | John Jost..... | Guysborough. |
| Spry Harbor..... | Richard Hauss..... | Halifax. |
| Guysborough..... | James Sutherland..... | Guysborough. |
| Harbor au Bouche. | Benjamin Wier..... | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Leader | 38288 | Brigantine... | 137 | 1850 |
| Leader | 10552 | Brigantine... | 146 | 1851 |
| Leader | 36155 | Schooner ... | 38 | 1858 |
| Leader ..... | ..... | Schooner ... | 35 | 1846 |
| Leading Breeze | 37285 | Schooner ... | 12 | 1856 |
| Leading Star | 42080 | Schooner ... | 8 | 1861 |
| Leading Star | 37592 | Schooner ... | 18 | 1857 |
| Leading Star | 37551 | Schooner ... | 38 | 1854 |
| Leander | 37582 | Schooner ... | 35 | 1856 |
| Leander ..... | ..... | Schooner ... | 24 | 1828 |
| Leander ..... | ..... | Schooner ... | 33 | 1826 |
| Leander | 37506 | Schooner ... | 35 | 1853 |
| Leander | 36068 | Schooner ... | 17 | 1851 |
| Leander | 35949 | Schooner ... | 19 | 1843 |
| L'Empereur ..... | ..... | Brig | 153 | 1847 |
| Leleah | 38914 | Schooner ... | 29 | 1851 |
| Le Noir | 38458 | Brigantine ... | 260 | 1864 |
| Leo | 37252 | Schooner ... | 73 | 1855 |
| Leo | 42328 | Brigantine ... | 254 | 1862 |
| Leon Porter | 49436 | Schooner ... | 55 | 1865 |
| Leone ..... | ..... | Brigantine ... | 138 | 1841 |
| Leonard | 37476 | Schooner ... | 56 | 1848 |
| Leopard ..... | ..... | Schooner ... | 16 | 1839 |
| Letart | .. . | Schooner ... | 38 | 1829 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|-----------------------------------|-------------------|
| Arichat | Andrew Belfontaine | Arichat. |
| Yarmouth | D. H. Madden, et al. | Arichat. |
| Port Medway | J. N. Cove | Liverpool, N. S |
| Harbor au Bouche . | Edward P. DeBlois, et al. | Halifax. |
| Clare | Thomas Killam | Digby. |
| Hillsburgh | Josiah Bunell, et al. | Digby. |
| La Have | H. D. Croucher | Lunenburg. |
| Lunenburg | William B. Hamilton | Halifax. |
| La Have | J. & H. Shenkle | Lunenburg. |
| Lunenburg | John Riser | Halifax. |
| Lunenburg | George W. Richardson | Halifax. |
| La Have | Simon Porrier, et al. | Halifax. |
| H. M. Dockyard . . | Alexander McIsaac | Halifax. |
| Lunenburg | Asa Megray, et al. | Halifax. |
| Arichat | Thomas Tyrell | Halifax. |
| Wilmot | John McConnell, et al. | Halifax. |
| Arichat | Thomas Lenoir | Arichat. |
| Westport | Robert Coggins | Digby. |
| Londonderry | James Campbell, et al. | Halifax. |
| Argyle | George S. Brown, et al. | Yarmouth. |
| Newport | John Liswell, et al. | Halifax. |
| Liverpool | William Beiswanger | Halifax. |
| Yarmouth | Nehemiah Crowell | Halifax. |
| Belfast, U. S. | William Owen Heffernan | Guysboeough. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------|------------------|----------------|-------|-------------|
| Let Her Rip | 50789 | Schooner | 15 | 1865 |
| Let Her B. | 49111 | Brigantine.... | 169 | 1865 |
| Levant | 46019 | Schooner | 62 | 1864 |
| Levi Hart | 37363 | Schooner | 43 | 1855 |
| Levanter | 38029 | Barque... .. | 551 | 1856 |
| Le Yard..... | ... . | Schooner | 39 | Unk'n |
| Levinia .. | 37276 | Schooner | 49 | 1851 |
| Leverett | 36073 | Schooner | 17 | 1851 |
| Liberty..... | ..... | Schooner | 14 | 1825 |
| Liberty | 37642 | Schooner | 64 | 1856 |
| Liberal | 37270 | Brigantine.... | 78 | 1855 |
| Liberal | 36122 | Schooner | 15 | 1857 |
| Liberal..... | ..... | Schooner | 17 | 1847 |
| Liberator | 38204 | Schooner | 47 | 1862 |
| Life Buoy .. | 42786 | Schooner | 72 | 1861 |
| Life Boat | 42082 | Schooner | 99 | 1861 |
| Life Boat | 42217 | Schooner | 48 | 1862 |
| Lighter..... | ..... | Schooner ... | 31 | 1834 |
| Light Brigade | 37710 | Barque..... | 289 | 1855 |
| Lillie M. | 50794 | Barque..... | 349 | 1865 |
| Lilly..... | ..... | Schooner | 18 | 1851 |
| Lilly..... | ..... | Schooner | 19 | 1852 |
| Lilly | 42089 | Schooner | 6 | 1862 |
| Lilly | 46386 | Brigantine.... | 250 | 1863 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|------------------------------|-------------------|
| Gegogan | Kenneth McLeod. | Halifax. |
| New Glasgow | James Hyland | Pictou. |
| Parrsborough. | J. W. Graham. | Parrsborough. |
| Crow Harbor. | Reuben Hart, et al. | Guysborough. |
| Beaver River. | A. C. Robbins, et al. | Yarmouth. |
| United States. | John E. Stratford | Halifax. |
| Wilmot. | John Ernst, et al. | Digby. |
| Port Medway. | James Stratford. | Halifax. |
| River Bourgeois. . | P. DeCarteret, et al. | Arichat. |
| Parrsborough. | Michael Carlin. | Sydney, C. B. |
| Clare | Henry Amero, et al. | Digby. |
| La Have | John Bush. | Halifax. |
| Port Medway. | Peter D. Cohoon, et al. | Halifax. |
| Tusket | Eli Porter, et al. | Yarmouth. |
| Shelburne | Joshua Snow | Shelburne. |
| St. Mary's Bay ... | Louis McDonald. | Digby. |
| Port Medway. ... | S. Foster, et al. | Liverpool, N. S. |
| New Brunswick. . | Silas D. Lockhart | Halifax. |
| New Glasgow | Joseph Crowell, et al. | Halifax. |
| Tatamagouche | John S. MacLean. | Halifax. |
| L'Ardoise. | G. E. Bissett. | Arichat. |
| Westport | William Smith, et al. | Digby. |
| Hillsburgh | Albert Morgan. | Digby. |
| Maitland | David B. Smith, et al. | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------------------|------------------|----------------|-------|-------------|
| Lilly..... | ..... | Schooner | 28 | 1823 |
| Lilly..... | ..... | Schooner | 22 | 1839 |
| Lilly..... | ..... | Schooner | 16 | 1827 |
| Lilly..... | ..... | Schooner | 14 | 1846 |
| Lilly | 36204 | Schooner | 12 | 1858 |
| Lillydale | 37683 | Schooner | 22 | 1860 |
| Lillydale | 46666 | Schooner | 13 | 1864 |
| Lillydale | 42363 | Schooner | 24 | 1863 |
| Lilian | 38251 | Ship | 728 | 1863 |
| Lillian | 36268 | Schooner | 25 | 1860 |
| Lillian | 42387 | Barque..... | 439 | 1863 |
| Lilly of the West | 37607 | Schooner | 36 | 1858 |
| Lillian M. Johnson | 46286 | Brigantine... | 135 | 1863 |
| Lima | 35878 | Schooner | 110 | 1855 |
| Linnet..... | ..... | Schooner | 11 | 1841 |
| Linnet..... | ..... | Schooner | 22 | 1809 |
| Linnet..... | ..... | Schooner | 8 | 1829 |
| Linnet..... | ..... | Schooner | 18 | 1835 |
| Linnet..... | ..... | Schooner | 23 | 1824 |
| Linnet..... | ..... | Schooner | 38 | 1843 |
| Linnet..... | ..... | Schooner | 26 | 1846 |
| Lion | 53564 | Schooner | 111 | 1865 |
| Lion..... | ..... | Schooner | 52 | 1834 |
| Lisbon | 48458 | Brigantine ... | 153 | 1864 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|------------------------------------|-------------------|
| Tatamagouche | Murray Waugh | Halifax. |
| Granville | George Winson | Halifax. |
| St. Martin's River. | Conrad Missener | Halifax. |
| Barrington | Andrew L. Swim, et al. | Halifax. |
| Chezzetcook | John Reyno | Halifax. |
| Apple River | Thomas Coughlin | Parrsborough. |
| Petite Papar | Benjamin Van Marcon | Digby. |
| Newdiquoddy | George Lorman, et al. | Halifax. |
| Tusket | Simon Cann, et al. | Yarmouth. |
| Barrington | Calvin Spinney, et al. | Halifax. |
| Truro | James Crowe, et al. | Halifax. |
| La Have | Andrew Wentzell, et al. | Lunenburg. |
| Port Medway | I. Rees | Liverpool, N. S. |
| Pope's Harbor | Thomas Winnett | Halifax. |
| Point Brule | Michael Langlelle, et al. | Pictou. |
| Lunenburg | Christian Ruland | Halifax. |
| Annapolis | John Winchester, et al. | Halifax. |
| Minudie | James Clarke, et al. | Halifax. |
| Clare | Robert A. Dakin | Halifax. |
| Mahone Bay | Michael Suttis | Halifax. |
| Clam Harbor | William Palmer | Halifax. |
| Londonderry | John Chisholm, et al. | Halifax. |
| Kempt | John Brown | Halifax. |
| Cambridge | Charles D. Burgess, et al. | Windsor. |

| Vessel's Name. | Official Number. | Rlg. | Tons. | When built. |
|-------------------------------|------------------|---------------|-------|-------------|
| Little Arichat..... | ..... | Schooner | 30 | 1813 |
| Little Fair Maid..... | ... . | Schooner | 39 | 1821 |
| Little Hugh | 48122 | Schooner | 31 | 1864 |
| Lively | 37256 | Schooner | 98 | 1855 |
| Lively..... | ..... | Schooner | 24 | 1818 |
| Lively..... | ..... | Schooner | 17 | 1814 |
| Lively..... | ... . | Schooner | 38 | 1816 |
| Lively..... | ..... | Schooner | 61 | 1810 |
| Lively..... | ..... | Schooner | 31 | 1824 |
| Lively..... | ..... | Schooner | 60 | 1833 |
| Lively..... | ..... | Schooner | 33 | 1815 |
| Lively..... | .... | Schooner | 32 | 1833 |
| Lively..... | ..... | Schooner | 21 | 1836 |
| Lively | 36341 | Schooner | 16 | 1813 |
| Lively | 42294 | Schooner | 12 | 1830 |
| Lively | 42413 | Schooner .. | 23 | 1863 |
| Lively Mary..... | ..... | Schooner | 45 | 1852 |
| Liverpool Packet..... | ..... | Schooner | 26 | 1814 |
| Lizzie Tupper | 53787 | Schooner | 68 | 1865 |
| Lochiel | 32981 | Schooner | 26 | 1846 |
| Lochiel | 43082 | Barque | 349 | 1863 |
| Loda | 42232 | Barque | 428 | 1862 |
| Lochness | 36565 | Schooner | 99 | 1861 |
| London | 33913 | Schooner | 94 | 1849 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|----------------------------------|-------------------|
| Arichat. | Robert Noble. | Halifax. |
| La Have. | John Ross. | Halifax. |
| Country Harbor. . | John McMillan. | Halifax. |
| St. Mary's Bay. . | William F. Marshall, et al. | Digby. |
| La Have. | Cornelius O'Neil. | Halifax. |
| Lunenburg. | William Rankin, et al. | Halifax. |
| Lunenburg. | Adam Hebb. | Halifax. |
| Lunenburg. | Peter Strum, et al. | Halifax. |
| La Have. | Garret Romkey, et al. | Halifax. |
| Annapolis. | Charles Jones. | Halifax. |
| Argyle. | David Van Buskirk. | Halifax. |
| Yarmouth. | David Swain, Senr., et al. | Halifax. |
| Mahone Bay. | Henry Laurence, et al. | Halifax. |
| Lunenburg. | Samuel Shadford. | Halifax. |
| Lunenburg. | John Taylor. | Halifax. |
| Wallace. | John O'Brien, et al. | Pugwash |
| La Have. | Michael Tobin, et al. | Halifax. |
| Unknown. | Joseph St. ain. | Halifax. |
| Liverpool. | S. Delisser, et al. | Liverpool, N. S. |
| Bouctouche, N. B. . | Edward E. Bivet. | Arichat. |
| New Glasgow. . . . | Finlay Cameron, et al. | Pietou. |
| Granville. | Robert Mill (2nd) et al. | Annapolis. |
| Pugwash. | Thomas Killam. | Yarmouth. |
| Sackville, N. B. . . | Daniel W. Job. | Arichat. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|------------------|----------------|-------|-------------|
| Londra | 49483 | Brigantine... | 142 | 1864 |
| Lone Star | 38270 | Brig | 208 | 1863 |
| Lone Star | 36605 | Brig | 244 | 1859 |
| Lone Star | 36404 | Schooner | 84 | 1859 |
| Lone Star | 36295 | Schooner | 15 | 1859 |
| Lone Star | 42375 | Schooner ... | 74 | 1863 |
| Lord Bury | 36211 | Schooner | 47 | 1859 |
| Lord Clyde | 41820 | Schooner | 93 | 1863 |
| Lord Chancellor | 49090 | Barque..... | 475 | 1864 |
| Lord Grough..... | ..... | Barque..... | 312 | 1850 |
| Lord Lyons | 36340 | Schooner | 28 | 1861 |
| Lord of the Isles | 46085 | Schooner | 81 | 1862 |
| Lord Raglan | 35726 | Schooner ... | 88 | 1855 |
| Look Out | 37678 | Schooner | 15 | 1861 |
| Look a Head... .. | .... | Schooner | 16 | 1841 |
| Lotus | 41805 | Brigantine... | 124 | 1859 |
| Louisa | 36439 | Brig | 213 | 1859 |
| Louisa | 48252 | Brigantine... | 259 | 1863 |
| Louisa | 36439 | Brig | 213 | 1859 |
| Louisa | 37618 | Schooner | 83 | 1858 |
| Louisa ..... | ..... | Brigantine... | 62 | 1840 |
| Louisa | 35610 | Brigantine ... | 72 | 1853 |
| Louisa | 49488 | Brigantine ... | 326 | 1865 |
| Louisa Agnes | 36476 | Schooner | 50 | 1859 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|---|-------------------|
| Bear River | William Miller & Son. | Digby. |
| Shelburne | Henry Kneally | Yarmouth. |
| Newport | Lewis A. Sponagle, et al. | Liverpool, N. S. |
| P. E. Island | John D. Cummins. | Halifax. |
| Newfoundland | William Manger | Halifax. |
| Cape Breton | John Farrell. | Halifax. |
| Isaac's Harbor | Whitman Giffin. | Halifax. |
| Boularderie | Kenneth McKenzie. | Baddeck. |
| Pictou | Clarence Primrose, et al. | Pictou. |
| River John | John Mockler. | Pictou. |
| New Harbor | Andrew Sangster. | Halifax. |
| Ragged Island | William J. Cummings, et al | Shelburne. |
| Ship Harbor | John W. Yeomans. | Halifax. |
| Parrsborough | Thomas Seaman. | Parrsborough. |
| Lunenburg | William Smith, et al | Halifax. |
| Granville | Wilkinson J. E. Ritchie, et al. | Annapolis. |
| Shelburne | Thomas Killam, et al. | Shelburne. |
| Barrington | Angus R. Atwood, et al | Shelburne. |
| Shelburne | Thomas Killam, et al. | Yarmouth. |
| Lunenburg | George Acker, et al | Halifax. |
| Barrington | Nehemiah Wilson, et al. | Halifax. |
| Shelburne | Conrad West, et al | Halifax. |
| Hillsburgh | W. F. Marshall, et al. | Digby. |
| Mahone Bay | Josiah Slaughenwhite | Halifax. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|-----------------------------|------------------|---------------|-------|-------------|
| Louisa Cook | 49419 | Barque. | 280 | 1864 |
| Louis Napoleon | 42020 | Schooner | 7 | 1860 |
| Lovely ..... | ..... | Schooner | 28 | 1840 |
| Lovely | 42413 | Schooner | 24 | 1863 |
| Loyal | 38194 | Schooner | 47 | 1848 |
| Lucy | 38361 | Schooner | 33 | 1850 |
| Lucy ..... | ..... | Schooner | 26 | 1852 |
| Lucy | 34690 | Schooner | 33 | 1835 |
| Lucy ..... | ..... | Schooner | 32 | 1819 |
| Lucy ..... | ..... | Schooner | 20 | 1817 |
| Lucy ..... | ..... | Schooner | 38 | 1828 |
| Lucy ..... | ..... | Schooner | 47 | 1816 |
| Lucy | 39049 | Schooner | 35 | 1830 |
| Lucy Ann | 38430 | Schooner | 151 | 1862 |
| Lucy Ann ..... | ..... | Schooner | 17 | 1848 |
| Lucy Ann ..... | ..... | Schooner | 29 | 1828 |
| Lucy Ann ..... | ..... | Schooner | 16 | 1835 |
| Lucy Ann | 36015 | Schooner | 70 | 1856 |
| Lucy Agnes | 38642 | Schooner | 32 | 1863 |
| Lucinda | 37823 | Schooner | 88 | 1850 |
| Lucinda | 36319 | Schooner | 24 | 1860 |
| Lucity Ann | 49462 | Schooner ... | 31 | 1865 |
| Lucknow | 41795 | Schooner | 57 | 1858 |
| Lucknow | 52003 | Schooner | 58 | 1865 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|-----------------------------------|-------------------|
| Clementsport . . . | David Cook, et al | Yarmouth. |
| Weymouth | Elair White | Digby. |
| Gut of Canso | David McPherson | Sydney, C. B. |
| River John | Alexander McFarlane | Pugwash. |
| Barrington | A. C. Robbins, et al | Yarmouth. |
| Cheticamp | Charles Boudroit, et al | Arichat. |
| Cheticamp | M. Mallet, et al | Arichat. |
| Digby | Urbain Doucett | Digby. |
| Nova Scotia | John Dauphinee | Halifax. |
| Chester | John Campbell, et al | Halifax. |
| Pictou | James Jones | Halifax. |
| Lunenburg | George Richardson | Halifax. |
| Tracadie | Patrick O'Bryan | Halifax. |
| Arichat | Charles Boudroit, et al | Arichat. |
| La Have | George Lohnas | Lunenburg. |
| Lunenburg | Nicholas Romkey | Halifax. |
| Lunenburg | James A. Moren | Halifax. |
| Amherst | Andrew Coffee, et al | Halifax. |
| Little Loraine, C.B. | James Kennedy | Sydney, C. B. |
| Cornwallis | William Gould | Windsor. |
| Isaac's Harbor . . . | John E. Fraser | Halifax. |
| Shinemicas | Jeremiah J. Brownell | Pugwash. |
| Wallace | Charles H. Sulis | Pugwash. |
| Shelburne | James Muir, et al | Shelburne. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|-------------------------------|------------------|----------------|-------|-------------|
| Lucretia | 35938 | Brigantine ... | 123 | 1851 |
| Lunenburg Packet | 37554 | Schooner | 61 | 1855 |
| Lupine ..... | ..... | Schooner | 9 | 1840 |
| L. W. Eaton | 46377 | Brigantine... | 140 | 1863 |
| L. B. Usher | 41810 | Schooner | 126 | 1860 |
| Lydia | 37060 | Schooner | 47 | 1861 |
| Lydia | 48076 | Ship | 700 | 1853 |
| Lydia | 38209 | Schooner | 93 | 1859 |
| Lydia | 35934 | Schooner | 62 | 1851 |
| Lydia Orenda..... | ..... | Schooner | 15 | 1839 |
| Lyman Cann | 38241 | Barque | 592 | 1863 |
| L. Hope..... | ..... | Schooner | 26 | 1834 |

| Where built. | Registered Owner. | Port of Registry. |
|-------------------------|----------------------------------|-------------------|
| Shelburne | John Lithgow | Halifax. |
| Lunenburg | J. M. Watson | Lunenburg. |
| Barrington | Thomas Garing, et al | Halifax. |
| Cornwallis | Levi W. Eaton, et al | Windsor. |
| Granville | Ebenezer Clarke | Annapolis. |
| Guysborough | Thomas McDonald | Guysborough. |
| United States | N. K. Clements | Yarmouth. |
| United States | George Killam, et al | Yarmouth. |
| Sheet Harbor | James E. Hart, et al | Halifax. |
| Horton | Thomas Jordon | Halifax. |
| Yarmouth | Lyman Cann | Yarmouth. |
| Rose Bay | Christian Zwick, et al | Halifax. |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
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| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|-----------------------------|------------------|---------------|-------|-------------|
| M. A. Benson | 48461 | Brig | 263 | 1864 |
| Macassar .. . | 37915 | Brigantine... | 210 | 1857 |
| Mackerel ..... | ..... | Schooner | 33 | 1826 |
| Madeline | 34794 | Schooner | 27 | 1839 |
| Madeira | 46378 | Schooner .. . | 114 | 1863 |
| Magna Charta | 10505 | Ship | 1466 | 1854 |
| Magna Charta | 42113 | Brigantine... | 171 | 1860 |
| Magna Charta | 48147 | Barque | 459 | 1864 |
| Magnolia | 37680 | Schooner | 113 | 1862 |
| Magnet | 46288 | Brigantine... | 259 | 1863 |
| Magnet | 35862 | Brig..... | 160 | 1852 |
| Magnet | 48136 | Brigantine... | 299 | 1864 |
| Maggie | 42781 | Schooner | 25 | 1861 |
| Maggie | 38082 | Schooner | 24 | 1844 |
| Maggie | 48109 | Brigantine .. | 222 | 1864 |
| Maggie Hammond | 38261 | Barque | 522 | 1863 |
| Maggie Louisa | 48073 | Schooner | 171 | 1863 |
| Magenta | 36601 | Brig | 195 | 1859 |
| Magdalen ..... | ..... | Schooner | 24 | 1814 |
| Magdalen ..... | ..... | Schooner | 27 | 1852 |
| Magdalene | 41540 | Schooner | 32 | 1859 |
| Mai | 50797 | Schooner | 71 | 1865 |
| Maid of Erin ..... | ..... | Schooner | 83 | 1850 |

| Where built. | Registered Owner. | Port of Registry |
|-------------------|--------------------------------|------------------|
| Cheverie..... | David Smith, et al. | Windsor. |
| Newport..... | William Chambers, et al..... | Yarmouth. |
| Clare | William Hull, et al | Halifax. |
| Yarmouth..... | Alexander Borden | Windsor. |
| Cornwallis | John Northup, et al..... | Windsor. |
| New Glasgow | George McKenzie | Pictou. |
| Walton | Alfred Tomlinson, et al..... | Windsor. |
| Maitland..... | William Aikins, et al..... | Halifax. |
| Parrsborough..... | George Dickey, et al..... | Parrsborough. |
| Liverpool | H. F. Conyers | Liverpool, N. S. |
| Yarmouth..... | Benjamin W. Salter, et al..... | Halifax. |
| Londonderry | Alexander Fulton, et al | Halifax. |
| Barrington | Asa D. Crowell..... | Shelburne. |
| Barrington | A. F. Stoneman, et al..... | Yarmouth. |
| Dartmouth | George Vigus | Halifax. |
| Meteghan | Amasa Durkee..... | Yarmouth. |
| Tusket | N. Atley, et al..... | Yarmouth. |
| Newport | William Chambers, et al..... | Windsor. |
| Cape Breton..... | Robert M. Cutler..... | Halifax. |
| P. E. Island..... | John Church | Halifax. |
| Guysborough..... | Benjamin Willett..... | Halifax. |
| Mahone Bay | John Copping..... | Halifax. |
| Londonderry | James O. Morrison | Parrsborough. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|---------------------------|------------------|----------------|-------|-------------|
| Maid of Erin | 36422 | Brigantine ... | 159 | 1858 |
| Majestic | 36046 | Schooner | 38 | 1853 |
| Majestic | 35978 | Schooner | 34 | 1850 |
| Malanta ..... | ..... | Schooner | 67 | 1847 |
| Manchester ..... | ..... | Schooner | 58 | 1840 |
| Mantheo | 37848 | Schooner | 27 | 1855 |
| Margaret ..... | ..... | Schooner . . | 11 | 1846 |
| Margaret ..... | ..... | Schooner | 19 | 1837 |
| Margaret | 38330 | Schooner | 26 | 1843 |
| Margaret | 43091 | Schooner | 31 | 1863 |
| Margaret | 37368 | Schooner | 61 | 1855 |
| Margaret | 37061 | Schooner | 37 | 1862 |
| Margaret ..... | ..... | Schooner | 17 | 1836 |
| Margaret | 36503 | Schooner | 15 | 1859 |
| Margaret ..... | ..... | Schooner | 13 | 1844 |
| Margaret | 35672 | Schooner | 64 | 1848 |
| Margaret | 35287 | Schooner | 30 | 1846 |
| Margaret | 32856 | Schooner | 104 | 1849 |
| Margaret .. | ..... | Schooner | 26 | 1846 |
| Margaret | 35919 | Schooner | 77 | 1849 |
| Margaret | ... | Schooner | 27 | 1817 |
| Margaret ..... | ..... | Schooner | 29 | 1826 |
| Margaret ... | ..... | Schooner | 43 | 1824 |
| Margaret ..... | .... | Schooner | 52 | 1829 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|-------------------------------|-------------------|
| P. E. Island. | Benjamin Davis. | Digby. |
| Chester. | James A. Moren. | Halifax. |
| P. E. Island. | John Hacket. | Halifax. |
| Cornwallis. | Zebiniah Roach. | Halifax. |
| Parrsborough. | James Cochran, et al. | Windsor. |
| Cornwallis. | Asahel Bligh, et al. | Windsor. |
| L'Ardoise. | Peter DeCarteret, et al. | Arichat. |
| D'Escouse. | Peter DeCarteret, et al. | Arichat. |
| Poulamond. | R. Gorham, et al. | Arichat. |
| Antigonish. | John McDonald. | Pictou. |
| Tracadie. | Joseph Delorie. | Guysborough. |
| Country Harbor. . | William Hodgson. | Guysborough. |
| La Have. | Edward Parks. | Lunenburg. |
| Ragged Island. . | Snow & Collins. | Liverpool, N. S. |
| Chezzetcook. | George Henderson. | Halifax. |
| Harbor au Bouche. | Moses Brough. | Halifax. |
| Yarmouth. | Isaac Graham. | Halifax. |
| Sheet Harbor. . | William Hall, et al. | Halifax. |
| Sheet Harbor. | Henry Lohnes, et al. | Halifax. |
| Sheet Harbor. | Michael Kennedy. | Halifax. |
| Cape Breton. | Andrew Norton. | Halifax. |
| Cole Harbor. | John Cogill. | Halifax. |
| St. Mary's. | Henry McDaniel. | Halifax. |
| Tatamagouche. . | George Smith. | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------------|------------------|----------------|-------|-------------|
| Margaret ..... | ..... | Schooner | 36 | 1828 |
| Margaret ..... | ..... | Schooner | 40 | 1819 |
| Margaret ..... | ..... | Schooner | 26 | 1836 |
| Margaret | 36202 | Schooner | 11 | 1858 |
| Margaret | 36240 | Brigantine ... | 180 | 1859 |
| Margaret | 42373 | Schooner | 30 | 1863 |
| Margaret | 53553 | Schooner | 55 | 1865 |
| Margaret Ann | 38634 | Schooner | 54 | 1862 |
| Margaret Ann | 38911 | Schooner | 33 | 1842 |
| Margaret Ann | 38392 | Schooner | 121 | 1859 |
| Margaret Ann ..... | ..... | Schooner | 44 | 1838 |
| Margaret Ann ..... | ..... | Schooner | 31 | 1840 |
| Margaret Ann ..... | ..... | Schooner | 43 | 1837 |
| Margaret Ann | 35908 | Schooner | 22 | 1852 |
| Margaret Barrington ... | 33411 | Schooner | 105 | 1852 |
| Margaret Bennett | 36043 | Schooner ... | 65 | 1856 |
| Margaret Catherine .... | ..... | Schooner | 12 | 1826 |
| Margaret Dill | 36150 | Schooner | 120 | 1858 |
| Margaret Hatfield | 49408 | Barque | 497 | 1864 |
| Margaret & Hope ..... | ..... | Schooner | 71 | 1833 |
| Margaret Jane ..... | ..... | Schooner | 16 | 1837 |
| Margaret McDougall .... | ..... | Schooner | 37 | 1850 |
| Margaretta | 48097 | Barque | 250 | 1864 |
| Margaretville ..... | ..... | Schooner | 54 | 1840 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|------------------------------------|-------------------|
| P. E. Island. | Frederick LeBlanc | Halifax. |
| Arichat | Michael Moser, et al. | Halifax. |
| Tracadie. | Joseph D. Lorey. | Halifax. |
| Chezzetcook. | Stephen Myntt | Halifax. |
| Salmon River. | William P. West, et al. | Halifax. |
| Petpiswick | William B. Hamilton. | Halifax. |
| Barrachois | Alexander Mitchell, et al. | Halifax. |
| Sydney. | Michael Lee, et al. | Sydney, C. B. |
| Guysborough | Thomas Thomas | Arichat. |
| Grand Ruisseau. | Mara LeBlanc. | Arichat. |
| Aylesford | John Ospin. | Halifax. |
| Onslow | Philip Higgins. | Halifax. |
| Sheet Harbor. | Abraham Bolong, et al. | Halifax. |
| P. E. Island. | Samuel Forman. | Halifax. |
| L'Ardoise, C. B. | James Ruddershaw. | Sydney, C. B. |
| Tangier. | George Ferguson, et al. | Halifax. |
| St. Mary's Bay | Enos Commo. | Halifax. |
| Londonderry | Robert Dill, 2nd | Halifax. |
| Clare | John S. Hatfield | Yarmouth. |
| New Brunswick. | William A. Young, et al. | Halifax. |
| St. Mary's Bay | Thomas McDermond. | Halifax. |
| Londonderry. | John McDougall, et al. | Halifax. |
| Argyle | Nelson Hammond, et al. | Yarmouth. |
| Wilmot. | William Blair. | Halifax. |

| Vessel's Name. | Official Number. | Btlg. | Tons. | When built. |
|-----------------------------|------------------|---------------|-------|-------------|
| Margaret Alice | 31689 | Schooner | 39 | 1854 |
| Mary Ann | 38459 | Schooner | 34 | 1847 |
| Mary Ann | 39024 | Schooner | 20 | 1850 |
| Mary Ann | 38327 | Schooner ... | 79 | 1848 |
| Mary Ann..... | ..... | Schooner | 45 | 1829 |
| Mary Ann..... | .... | Schooner ... | 21 | 1846 |
| Mary Ann | 38590 | Schooner | 19 | 1857 |
| Mary Ann..... | ..... | Schooner | 27 | 1851 |
| Mary Ann | 42076 | Schooner | 76 | 1850 |
| Mary Ann | 42086 | Schooner | 46 | 1862 |
| Mary Ann | 37399 | Schooner | 61 | 1857 |
| Mary Ann | 37062 | Schooner | 48 | 1863 |
| Mary Ann..... | ..... | Brigantine... | 90 | 1847 |
| Mary Ann..... | ..... | Schooner | 17 | 1832 |
| Mary Ann..... | ..... | Schooner | 11 | 1834 |
| Mary Ann..... | ..... | Schooner | 30 | 1821 |
| Mary Ann..... | ..... | Schooner | 18 | 1839 |
| Mary Ann..... | .... | Schooner | 27 | 1840 |
| Mary Ann..... | ..... | Schooner | 33 | 1842 |
| Mary Ann..... | ..... | Schooner | 54 | 1832 |
| Mary Ann | .. . | Schooner | 35 | 1846 |
| Mary Ann | 35843 | Schooner | 79 | 1849 |
| Mary Ann | 38560 | Schooner | 22 | 1852 |
| Mary Ann | 39021 | Schooner | 44 | 1850 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|--------------------------------------|-------------------|
| Cornwallis | Christopher Simmonds, et al. | Windsor. |
| Petit de Grat. | I. & W. LeVesconte. | Arichat. |
| Red Head | James McLeum. | Arichat. |
| Grand Ruisseau | Simon Terrio. | Arichat. |
| Pictou. | James Carmichael. | Pictou. |
| Bailey's Brook. | Donald McKinnon. | Pictou. |
| Mira. | Donald Scott. | Sydney, C. B. |
| Parrsborough. | Charles Taylor, et al. | Parrsborough. |
| St. Mary's Bay | John Cronan, et al. | Digby. |
| Long Island. | Bartholomew R. Harris. | Digby. |
| Oyster Ponds. | Alexander Grant. | Guysborough. |
| Country Harbor. | George Crooks. | Guysborough. |
| Cape Breton. | Arthur McNutt Wier. | Windsor. |
| Barrington. | John Smith, et al. | Halifax. |
| Tancook. | John Dauphiney. | Halifax. |
| La Have. | George Ernst, et al. | Halifax. |
| Lunenburg. | Frederick Lohnas. | Halifax. |
| La Have. | John Publicover, Junr. | Halifax. |
| Mahone Bay. | John Knock. | Halifax. |
| Malignant Bay. | John Quinn. | Halifax. |
| Sheet Harbor. | Frederick Curry. | Halifax. |
| Sheet Harbor. | Samuel Lang. | Halifax. |
| Chezzetcook. | George Hanring, et al. | Halifax. |
| Newfoundland. | James Redmond, et al. | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|------------------|----------------|-------|-------------|
| Mary Ann..... | ..... | Schooner | 25 | 1816 |
| Mary Ann..... | ..... | Schooner | 41 | 1816 |
| Mary Ann..... | ..... | Schooner | 28 | 1822 |
| Mary Ann..... | ..... | Schooner | 35 | 1821 |
| Mary Ann..... | ..... | Schooner | 19 | 1829 |
| Mary Ann..... | ..... | Schooner | 84 | 1821 |
| Mary Ann..... | ..... | Brig | 127 | 1827 |
| Mary Ann..... | ..... | Schooner | 33 | 1832 |
| Mary Ann..... | ..... | Schooner | 20 | 1833 |
| Mary Ann..... | ..... | Schooner | 88 | 1830 |
| Mary Ann..... | ..... | Schooner | 50 | 1831 |
| Mary Ann..... | ..... | Schooner | 22 | 1824 |
| Mary Ann | 36174 | Schooner | 17 | 1820 |
| Mary Ann | 36310 | Schooner | 66 | 1860 |
| Mary Ann | 42311 | Brigantine ... | 194 | 1862 |
| Mary Ann | 33413 | Brigantine... | 102 | 1852 |
| Mary Ann | 42394 | Barque..... | 499 | 1863 |
| Mary Alice | 37015 | Brigantine ... | 94 | 1861 |
| Mary Alice | 36265 | Schooner | 38 | 1859 |
| Mary Alice | 36281 | Schooner | 17 | 1860 |
| Mary Alice | 36344 | Schooner | 58 | 1861 |
| Mary Ann Catherine | 38599 | Schooner .. | 32 | 1858 |
| Mary Ann Star..... | ..... | Schooner | 55 | 1838 |
| Mary Bauld Lithgow | 50768 | Brigantine... | 288 | Unkn |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|--------------------------------|-------------------|
| Clare | Kelly Clements | Halifax. |
| Cape Breton | Michael Oats, et al. | Halifax. |
| Lunenburg | John Hiltz | Halifax. |
| Chester | Michael Power, et al. | Halifax. |
| Herring Cove. | Patrick Power, et al. | Halifax. |
| Argyle | Reuben Larkin | Halifax. |
| Truro | James Ratchford, et al. | Halifax. |
| Little River. | John Graham, et al. | Halifax. |
| Digby | James Gilliland | Halifax. |
| St. John, N. B. | William Donnelly | Halifax. |
| Mahone Bay | George Eisenhauer, et al. | Halifax. |
| Granville | John Croup, Senr. | Halifax. |
| Lunenburg | William Lintlop, et al. | Halifax. |
| Chezzetcook | Isaac Cann | Halifax. |
| Economy | John Jones, et al. | Halifax. |
| Sheet Harbor | Henry Baleam | Halifax. |
| Sherbrooke | Peter Sutherland | Halifax. |
| Shelburne | Robert West | Shelburne. |
| Lunenburg | Simon Ritey | Halifax. |
| Chezzetcook | James Smith | Halifax. |
| Barrington | Elijah Wood, et al. | Halifax. |
| Mira | Michael Burke | Sydney, C. B. |
| Granville | James Mason, et al. | Halifax. |
| New York | John Lithgow | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------------|------------------|----------------|-------|-------------|
| Mary Boucher | 46092 | Brig. | 180 | 1863 |
| Mary Bowen | 38443 | Schooner ... | 130 | 1864 |
| Mary E. Banks | 46096 | Schooner | 50 | 1863 |
| Mary Budd | 37334 | Brigantine ... | 96 | 1857 |
| Mary Baker | 38101 | Barque. | 361 | 1859 |
| Mary B. ..... | ..... | Schooner | 18 | 1841 |
| Mary & Charles | 38415 | Schooner | 50 | 1843 |
| Mary & Catherine ..... | ..... | Brigantine ... | 70 | 1820 |
| Mary Durkee | 38191 | Ship | 880 | 1861 |
| Mary Elizabeth | 38393 | Schooner | 110 | 1859 |
| Mary Elizabeth | 35295 | Schooner | 29 | 1843 |
| Mary Elizabeth | 36000 | Schooner | 24 | 1856 |
| Mary Elizabeth ..... | ..... | Schooner | 61 | 1840 |
| Mary & Elizabeth ..... | ... . | Schooner | 14 | 1821 |
| Mary & Elizabeth ..... | ..... | Schooner | 35 | 1842 |
| Mary Eliza | 35639 | Schooner | 58 | 1851 |
| Mary Eliza | 42078 | Schooner | 72 | 1850 |
| Mary Eliza ..... | ..... | Schooner | 11 | 1834 |
| Mary Edith | 52005 | Schooner | 42 | 1865 |
| Mary Ellen | 48124 | Schooner | 88 | 1864 |
| Mary E. Fraser | 46093 | Schooner | 91 | 1863 |
| Mary Fraser | 37892 | Schooner | 86 | 1854 |
| Mary Hart | 38379 | Schooner | 37 | Unk'n |
| Mary Hart | 38388 | Schooner | 26 | 1859 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|--------------------------------------|-------------------|
| Shelburne | Charles H. Boucher, et al. | Shelburne. |
| Arichat. | John & William Fretvill. | Arichat. |
| Barrington | A. J. Hood. | Yarmouth. |
| St. Mary's Bay | William Warner, et al. | Digby. |
| Yarmouth | Young & Baker. | Yarmouth. |
| New Brunswick . . . | Andrew DeWolf | Halifax. |
| Sheet Harbor | Joseph Bolin, et al | Arichat. |
| New Brunswick . . . | George Mitchell, et al. | Halifax. |
| Meteghan | Amasa Durkee, et al | Yarmouth. |
| Little Arichat. . . . | Reond King. | Arichat. |
| St. Martin's | Colin McKay. | Digby. |
| Liscomb Harbor . . . | William Hemlow | Halifax. |
| Parrsborough. | Elisha Pettis | Halifax. |
| Clare | John Godet, et al | Halifax. |
| Sheet Harbor | John Hall. | Halifax. |
| Argyle | G. Redding & G. R. Hammond. . | Yarmouth. |
| St. Mary's Bay | William Warren | Digby. |
| Long Island. | Christopher Gilicat, et al | Digby. |
| Jordan River. . . . | James Thorburn | Shelburne. |
| Harbor au Bouche. . | Michael Crisp, et al. | Halifax. |
| Shelburne | Joshua Snow, et al. | Shelburne. |
| Parrsborough. | John Hanning, et al. | Parrsborough. |
| United States. | Henry Taylor | Arichat. |
| Cheticamp | Michael Mallet. | Arichat. |

| Vessel's Name. | Official Number. | Rlg. | Tons. | When built. |
|--------------------------------|------------------|---------------|-------|-------------|
| Mary Hart | 38379 | Schooner | 57 | Unk'n |
| Mary Ida | 49420 | Brigantine... | 225 | 1864 |
| Mary Jane McMillan | 46905 | Schooner | 82 | 1864 |
| Mary Jane ..... | ..... | Schooner | 27 | 1838 |
| Mary Jane | 37490 | Schooner | 61 | 1851 |
| Mary Jane | 37305 | Schooner | 116 | 1856 |
| Mary Jane | 42223 | Schooner | 25 | 1861 |
| Mary Jane ..... | ..... | Schooner | 19 | 1829 |
| Mary Jane ..... | ..... | Schooner ... | 15 | 1834 |
| Mary Jane ..... | ..... | Schooner | 12 | 1840 |
| Mary Jane ..... | ..... | Schooner | 43 | 1817 |
| Mary Jane ..... | ..... | Schooner | 36 | 1828 |
| Mary Jane | 36131 | Schooner | 19 | 1858 |
| Mary Jane | 32285 | Schooner | 73 | 1860 |
| Mary Jane | 36882 | Schooner | 50 | 1860 |
| Mary Jane | 34753 | Schooner | 48 | 1859 |
| Mary Jane | 37692 | Schooner | 84 | 1862 |
| Mary Jane | 37872 | Schooner | 56 | 1851 |
| Mary ..... | ..... | Schooner | 22 | 1842 |
| Mary ..... | ..... | Schooner | 24 | 1843 |
| Mary ..... | ..... | Schooner | 28 | 1846 |
| Mary ..... | ..... | Schooner | 27 | 1845 |
| Mary ..... | ..... | Schooner | 22 | 1850 |
| Mary ..... | ..... | Schooner | 16 | 1851 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|-----------------------------------|-------------------|
| Unknown | E. Gaskill, et al. | Liverpool, N. S. |
| Green Cove. | Nathan Utley, et al. | Yarmouth. |
| Whycocomogh | John S. Maclean | Halifax. |
| Harbor au Bouche. | Peter DeCarteret, et al. | Arichat. |
| Granville | Walter Willett, et al | Digby. |
| Hillsborough. | Simon Boudroit | Digby. |
| Clements | Hugh Lynch. | Annapolis. |
| Chester. | P. Walker & Son. | Lunenburg. |
| Lunenburg. | Emanuel McKay. | Halifax. |
| Barrington | Joseph Greenlow. | Halifax. |
| Yarmouth. | David Flint, et al. | Halifax. |
| Indian River | Leonard Pye | Halifax. |
| Petpiswick | John Smith | Halifax. |
| Barrington | Archibald Hopkins, et al. | Halifax. |
| Tatamagouche | John S. Maclean, et al | Halifax. |
| Grand Ruisseau. . . . | Lewis Terrio, et al. | Arichat. |
| Little Arichat. . . . | Alexander Forest. | Arichat. |
| Parrsborough. | Robert Boyd, et al | Parrsborough. |
| Pompquet | Peter DeCarteret, et al. | Arichat. |
| River Bourgeois. . . . | Basil Beuche. | Arichat. |
| Margaree | P. LeBlanc, et al. | Arichat. |
| D'Escouse | Peter DeCarteret, et al. | Arichat. |
| Pompquet. | E. Orange, et al. | Arichat. |
| Poulamond. | Isaac LeVesconte, et al. | Arichat. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------|------------------|---------------|-------|-------------|
| Mary ..... | ..... | Schooner | 15 | 1852 |
| Mary ..... | ..... | Schooner | 49 | 1852 |
| Mary ..... | ..... | Schooner | 20 | 1852 |
| Mary ..... | ..... | Schooner | 20 | 1853 |
| Mary ..... | ..... | Schooner | 13 | 1852 |
| Mary ..... | ..... | Schooner | 21 | 1857 |
| Mary | 38377 | Schooner | 20 | 1847 |
| Mary | 38384 | Schooner | 21 | 1834 |
| Mary | 38400 | Schooner | 24 | 1860 |
| Mary | 38420 | Schooner | 101 | 1861 |
| Mary | 38425 | Schooner | 69 | 1831 |
| Mary | 37438 | Schooner | 63 | 1853 |
| Mary | 46082 | Schooner | 43 | 1862 |
| Mary | 38606 | Schooner | 59 | 1859 |
| Mary ..... | ..... | Schooner ... | 62 | 1850 |
| Mary ..... | ..... | Schooner | 20 | 1854 |
| Mary | 37376 | Schooner | 41 | 1855 |
| Mary | 46082 | Schooner | 43 | 1862 |
| Mary | 36158 | Barque..... | 269 | 1858 |
| Mary ..... | ..... | Schooner | 15 | 1837 |
| Mary ..... | ..... | Schooner | 14 | 1837 |
| Mary ..... | ..... | Schooner | 22 | 1829 |
| Mary ..... | ..... | Schooner | 29 | 1829 |
| Mary ..... | ..... | Schooner | 51 | 1844 |

| Where built. | Registered Owner. | Port of Registry. |
|-------------------------|-------------------------------|-------------------|
| River Bourgeois .. | Peter DeCarteret, et al. | Arichat. |
| Arichat | Victor LeBlanc, et al .. | Arichat. |
| Cheticamp | Charles O. Druim | Arichat. |
| River Bourgeois .. | G. E. Bissett | Arichat. |
| River Bourgeois .. | Peter DeCarteret, et al | Arichat. |
| Pompquet | Peter DeCarteret, et al. | Arichat. |
| P. E. Island. | Peter Landry | Arichat. |
| Arichat. | David Grumley | Arichat. |
| Poulamond | Simon Boudroit | Arichat. |
| Little Arichat. | Valentine Bunkin, et al. | Arichat. |
| Portland, U. S. | Abraham LaVashe | Arichat. |
| Liverpool, N. S. | John Day | Arichat. |
| Shelburne | Peter Paint | Shelburne. |
| Gabarous | James Bagnell, et al | Sydney, C. B. |
| Tracadie | Patrick Webb | Guysborough. |
| Sydney | John Frayer | Guysborough. |
| Strait of Canso | Walter Murray | Arichat. |
| Shelburne | Peter Paint, Senr. | Port Hawkesbury. |
| St. Peter's, C. B. | Martin P. Black | Halifax. |
| Yarmouth | Policarp Suratt, et al | Halifax. |
| Tracadie | Joseph Berrio | Halifax. |
| Barrington | John George | Halifax. |
| Ship Harbor. | Patrick O'Bryan, Junr | Halifax. |
| Pope's Harbor | James Bond | Halifax. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|----------------------------|------------------|----------------|-------|-------------|
| Mary ..... | ..... | Schooner | 11 | 1845 |
| Mary | 35638 | Schooner | 71 | 1852 |
| Mary ..... | ..... | Schooner | 43 | 1820 |
| Mary ..... | ..... | Schooner | 26 | 1821 |
| Mary ..... | ..... | Schooner | 23 | 1823 |
| Mary ..... | ..... | Schooner | 22 | 1814 |
| Mary ..... | ..... | Schooner | 42 | 1824 |
| Mary ..... | ..... | Schooner | 42 | 1827 |
| Mary ... . | ..... | Schooner | 24 | 1813 |
| Mary ..... | ..... | Schooner | 20 | 1834 |
| Mary | .. . | Schooner | 17 | 1835 |
| Mary | .. . | Schooner | 20 | 1831 |
| Mary ..... | ..... | Schooner | 59 | 1828 |
| Mary | 35997 | Schooner | 20 | 1856 |
| Mary | 40114 | Schooner | 43 | 1853 |
| Mary | 42374 | Barque..... | 642 | 1863 |
| Mary | 48106 | Brigantine ... | 129 | 1864 |
| Mary | 50772 | Schooner | 13 | 1858 |
| Mary | 50793 | Brig..... | 277 | 1865 |
| Mary Joseph | 38466 | Schooner ... | 26 | 1863 |
| Mary Jordan ..... | ..... | Schooner | 42 | 1849 |
| Mary Jones..... | ..... | Brigantine... | 149 | 1849 |
| Mary E. Jones..... | ..... | Schooner | 132 | 1864 |
| Mary Louisa | 38338 | Schooner | 37 | 1856 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|--------------------------------------|-------------------|
| Shelburne | George Scarfe | Halifax. |
| Newdiquoddy | Edmund O'Leary, et al. | Halifax. |
| Margaret's Bay | John Dauphine | Halifax. |
| La Have | John Publicover | Halifax. |
| Clare | Ebenezer Parry, Junr. | Halifax. |
| P. E. Island. | Donald Sutherland, et al. | Halifax. |
| St. Mary's Bay | John Parry, et al. | Halifax. |
| Little River | Joseph Naas, et al. | Halifax. |
| Lunenburg | Henry Puzich | Halifax. |
| River John | John Shea. | Halifax. |
| Chezzetcook | William Story, Senr. | Halifax. |
| St. Mary's Bay | George Turnbull, et al | Halifax. |
| Yarmouth. | Joseph Tooker, Senr., et al. | Halifax. |
| Jeddore. | Adam Ruser | Halifax. |
| Ragged Island. | James Nickerson, et al | Halifax. |
| Maitland. | William Lawrence, et al | Halifax. |
| Lunenburg. | Daniel Cronan. | Halifax. |
| Chezzeteook. | James Cleary. | Halifax. |
| Maitland. | George J. Troop, et al. | Halifax. |
| Cheticamp | P. Robins & Co. | Arichat. |
| Point Brule. | David Langelle | Pictou. |
| Pugwash | Levi W. Eaton | Pictou. |
| Clyde River. | James D. Coffin, et al | Shelburne. |
| Grand Passage. | Theophilus Rozier. | Arichat. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Mary LeBlanc | 38349 | Brigantine ... | 124 | 1856 |
| Mary the Lively .. | ..... | Schooner | 14 | 1817 |
| Mary McKenzie | 35979 | Schooner | 24 | 1856 |
| Mary Morton | 37835 | Brigantine ... | 181 | 1853 |
| Mary Mac | 42396 | Schooner | 31 | 1863 |
| Mary Raymond | 49414 | Ship | 678 | 1854 |
| Mary Richards | 38004 | Barque..... | 474 | 1856 |
| Mary Snow | 37280 | Schooner | 64 | 1849 |
| Mary Stephens | 36435 | Schooner | 27 | 1859 |
| Mary Salter | 41556 | Brigantine ... | 89 | 1840 |
| Mary White | 38457 | Schooner . . | 110 | 1864 |
| Mary E. Walker | 42021 | Schooner | 70 | 1860 |
| Maryetta | 42095 | Schooner | 135 | 1863 |
| Marietta ... | 37842 | Brigantine ... | 96 | 1855 |
| Maria | 38963 | Schooner | 46 | 1840 |
| Maria ..... | ..... | Schooner | 16 | 1849 |
| Maria | 37691 | Brigantine ... | 139 | 1850 |
| Maria | 38412 | Schooner | 28 | 1861 |
| Maria ..... | ..... | Schooner | 42 | 1842 |
| Maria | 46089 | Schooner | 101 | 1862 |
| Maria ..... | ..... | Schooner | 17 | 1853 |
| Maria ..... | ..... | Brigantine ... | 136 | 1851 |
| Maria | 36229 | Schooner | 45 | 1859 |
| Maria | 48074 | Barque..... | 590 | 1863 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|-------------------------------|-------------------|
| Pictou..... | P. Grimes, Junr | Arichat. |
| Londonderry | John Bollong, Junr | Halifax. |
| Sable River | William P. Crowell. | Halifax. |
| Cornwallis | Simon F. Barsa | Halifax. |
| Isaac's Harbor.... | Allan McMillan, et al..... | Halifax. |
| Salmon River..... | A. C. Robbins, et al..... | Yarmouth. |
| Yarmouth..... | Samuel Killam..... | Yarmouth. |
| Granville | William Johnston | Digby. |
| Green Harbor ... | John Reynolds, et al | Shelburne. |
| Parrsborough.... | Joseph McLellan, et al | Windsor. |
| Grand Ruisseau... | Simon LeBlanc, Junr | Arichat. |
| Little River..... | Silas Westcott, et al..... | Digby. |
| Hillsborough | David Rice, et al..... | Digby. |
| Cornwallis | Charles Eaton..... | Windsor. |
| Arichat. | Charles Blampied..... | Arichat. |
| Strait of Canso ... | James Reeves..... | Arichat. |
| D'Escouse | Charles Boudroit..... | Arichat. |
| River Bourgeois... | Abraham Fougere..... | Arichat. |
| Harbor au Bouche. | Malcolm Matthewson..... | Pictou. |
| Barrington | Thomas W. Wilson, et al | Shelburne. |
| Grand Mire..... | William M. Harrington..... | Sydney, C. B. |
| Weymouth | George Taylor, et al | Digby. |
| St Mary's | William Murdoch, et al..... | Guysborough. |
| Yarmouth..... | Ryerson & Co..... | Yarmouth. |

| Vessel's Name. | Official Number. | Tlg. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Maria ..... | ..... | Schooner | 12 | 1839 |
| Maria ..... | ..... | Schooner | 42 | 1844 |
| Maria | 35904 | Schooner | 61 | 1852 |
| Maria Catherine ..... | ..... | Schooner | 62 | 1817 |
| Maria Elizabeth | 37058 | Schooner | 44 | 1862 |
| Maria Jane ..... | ..... | Schooner | 17 | 1835 |
| Maria Moore | 36151 | Schooner | 41 | 1858 |
| Maria Moore | 48112 | Schooner | 105 | 1864 |
| Maria Teresa | 42309 | Schooner | 101 | 1862 |
| Marion | 38607 | Schooner ... | 11 | 1854 |
| Marion | 38608 | Schooner | 82 | 1859 |
| Marion | 36217 | Schooner | 49 | 1859 |
| Mariner | 37273 | Schooner | 58 | 1841 |
| Mariner ..... | ..... | Schooner | 19 | 1835 |
| Mariner ..... | ... . | Schooner . . . | 13 | 1839 |
| Mariner | 36029 | Schooner | 33 | 1843 |
| Mariner ..... | ..... | Schooner | 50 | 1832 |
| Mariner | 37846 | Schooner | 35 | 1838 |
| Marenda | 37543 | Schooner | 47 | 1828 |
| Mark ..... | ..... | Schooner | 112 | 1820 |
| Markland | 36334 | Brigantine ... | 250 | 1861 |
| Marco Polo | 37726 | Barque | 302 | 1855 |
| Marco Polo | 46372 | Brigantine ... | 178 | 1863 |
| Marlborough | 46385 | Ship | 1383 | 1863 |

| Where built. | Registered Owner | Port of Registry. |
|-------------------------|-----------------------------------|-------------------|
| New Brunswick . | John W. Moore | Halifax. |
| Bay of Islands | Benjamin Wier | Halifax. |
| Dartmouth | William Siteman | Halifax. |
| Canada | Thomas Bell | Halifax. |
| Strait of Canso | Alexander McGuire, Junr., et al . | Guysborough. |
| Newport | Ephraim Sandford | Halifax. |
| St. Margaret's Bay . | John Moore | Halifax. |
| Port Mulgrave | Martin May, et al | Halifax. |
| Harbor au Bouche . | Patrick Webb | Halifax. |
| Louisburg | John Blackett | Sydney, C. B. |
| Mira | Edward P. Archbold | Sydney, C. B. |
| Chester | James A. Moren | Halifax. |
| Clements | Joseph Babine, et al | Digby. |
| Argyle | Samuel Atwood | Halifax. |
| Parrsborough | Isaac Dexter | Halifax. |
| Seal Island | Mary Ann Lyle, et al | Halifax. |
| New Brunswick . . . | Silas Burgess | Windsor. |
| Granville | James Bennett | Windsor. |
| Newburyport, U. S . | J. Durland | Lunenburg. |
| New Brunswick . . . | Stephen Stoddart, et al | Halifax. |
| New Glasgow | Robert Morrow, et al | Halifax. |
| River John | James Kitchen | Pictou. |
| Walton | John McLellan, et al | Windsor. |
| Hantsport | Ezra Churchill, et al | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------------------|------------------|----------------|-------|-------------|
| Mars | 35916 | Schooner | 76 | 1848 |
| Marquis ..... | ..... | Schooner | 34 | 1839 |
| Martha | 40993 | Ship | 580 | Un'kn. |
| Martha | 38079 | Brig | 271 | 1858 |
| Martha | ... | Schooner .. | 28 | 1824 |
| Martha | 41648 | Brigantine ... | 162 | 1859 |
| Martha | 48113 | Schooner | 20 | 1863 |
| Martha Ann ..... | ..... | Schooner | 73 | 1826 |
| Martha K. Lockhart | 46391 | Barque | 415 | 1863 |
| Martha S. K. Thorne | 41806 | Schooner | 115 | 1859 |
| Matilda | 38945 | Schooner | 43 | 1854 |
| Matilda ..... | ..... | Schooner | 64 | 1829 |
| Matilda ..... | ..... | Schooner | 16 | 1846 |
| Matilda ..... | ..... | Schooner | 28 | 1848 |
| Matilda ..... | ..... | Schooner | 28 | 1820 |
| Matilda B. | 38656 | Brigantine ... | 178 | Un'kn. |
| Matilda Hopewell | 38614 | Schooner | 89 | 1860 |
| Matilda A. Lewis | 38203 | Barque | 269 | 1862 |
| Mattocks | 36566 | Schooner | 24 | 1859 |
| Match ..... | ..... | Schooner | 18 | 1843 |
| Matchless | 35738 | Schooner | 29 | 1836 |
| Matthew Mooney | 36277 | Schooner | 24 | 1859 |
| Maude | 49098 | Brigantine ... | 168 | 1864 |
| Maude | 36488 | Schooner | 37 | 1860 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------------|--------------------------------------|-------------------|
| Guysborough | William Pitts | Halifax. |
| Petite Riviere | W. Parks | Liverpool, N. S. |
| Unknown | Joseph Eaton | Halifax. |
| Beaver River | W. K. Dudman, et al | Yarmouth. |
| River Bourgeois | Peter DeCarteret, et al | Arichat. |
| Merigomish | Peter Ganion | Arichat. |
| Bay of Islands | Charles R. Woodin | Halifax. |
| Hillsburgh | Samuel Nelson | Halifax. |
| Newport | D. R. DeWolf, et al | Windsor. |
| Cornwallis | Asa Porter, et al | Annapolis. |
| Ship Harbor | Donald A. Shaw | Arichat. |
| Yarmouth | David Flint, Junr | Halifax. |
| Chezzetcook | George Henderson | Halifax. |
| P. E. Island | Henry Shellnut | Halifax. |
| Granville | Thomas Bell | Halifax. |
| Unknown | Henry LeCras, et al | Sydney, C. B. |
| Gabarouse | John Ormiston | Sydney, C. B. |
| Shelburne | William Rogers, et al | Yarmouth. |
| Guysborough | Joseph William Hadley | Guysborough. |
| Barrington | Alexander Greenwood, et al | Halifax. |
| New Dublin | William B. Hamilton | Halifax. |
| Chezzetcook | John Smith | Halifax. |
| Tatamagouche | Edward Kent | Pictou. |
| Lunenburg | James Butler, et al | Lunenburg. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------|------------------|----------------|-------|-------------|
| Mavourneen | 49405 | Barque. | 618 | 1864 |
| Maximilian | 48096 | Barque. | 268 | 1864 |
| May ..... | ..... | Schooner | 78 | 1815 |
| May | 49014 | Schooner | 144 | 1865 |
| May Belle | 42419 | Schooner | 89 | 1864 |
| Mayflower | 38930 | Schooner | 44 | 1849 |
| Mayflower | 37720 | Schooner | 64 | 1846 |
| Mayflower ..... | ..... | Schooner | 44 | 1840 |
| Mayflower ..... | ..... | Brig | 153 | 1851 |
| Mayflower | 46287 | Schooner | 122 | 1863 |
| Mayflower ..... | ..... | Schooner | 11 | 1846 |
| Mayflower ..... | ..... | Schooner | 77 | 1847 |
| Mayflower ..... | ..... | Schooner | 34 | 1838 |
| Mayflower | 37820 | Schooner | 58 | 1846 |
| Mayflower ..... | ..... | Schooner | 30 | 1819 |
| Mayflower ..... | ..... | Schooner | 31 | 1832 |
| Mayflower | 35728 | Schooner | 20 | 1855 |
| Mayflower | 36269 | Schooner | 16 | 1857 |
| May Queen | 42384 | Schooner | 42 | 1863 |
| Mazeppa | 38307 | Schooner | 72 | 1833 |
| Mazeppa | 51776 | Schooner | 71 | 1865 |
| M. B. Almon | 38049 | Barque. | 387 | 1864 |
| Meander | 34721 | Schooner | 72 | 1854 |
| M. E. Corning | 46099 | Barque. | 354 | 1863 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|-----------------------------------|-------------------|
| Tusket | N. Churchill, et al. | Yarmouth. |
| Shelburne | A. F. Stoneman, et al. | Yarmouth. |
| Granville | John Allen. | Halifax. |
| Maccan | Michael L. Tucker, et al. | Parrsborough. |
| Pugwash | John Thomas Smith, et al. | Pugwash. |
| Grand Ruisseau | Paulicamp Girroir. | Arichat. |
| Grand River | Paulicamp Girroir. | Arichat. |
| Pompquet | Benjamin Pettipas. | Pictou. |
| St. Martin's | George McKenzie. | Pictou. |
| Liverpool | D. Killaher | Liverpool, N. S. |
| Terence Bay | Charles Lordly | Halifax. |
| Cornwallis | Samuel Bigelow, et al. | Halifax. |
| Clam Harbor. | Henry O'Bryan. | Halifax. |
| Cape Breton. | N. S. DeMill, et al. | Windsor. |
| New Brunswick. | John Wilson | Halifax. |
| Lunenburg | Philip Ganet, et al. | Halifax. |
| St. Mary's Bay. | John E. Stratford | Halifax. |
| Torbay | Joseph Fongere. | Halifax. |
| Torbay | Joseph Peebrine | Halifax. |
| Clare | E. P. Archbold | Arichat. |
| Liverpool | J. E. Spurr, et al. | Liverpool, N. S. |
| North Sydney | Thomas D. Archbold. | Sydney, C. B. |
| Newport. | James H. Dennison. | Windsor. |
| Shelburne | Bonman Corning. | Shelburne. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------------------|------------------|----------------|-------|-------------|
| Mechanic | 35050 | Schooner | 23 | 1852 |
| Mechanic | 36187 | Schooner | 24 | 1847 |
| Medford ..... | ..... | Schooner | 19 | 1851 |
| Mediator | 37535 | Schooner | 56 | 1855 |
| Medway Belle | 37428 | Schooner | 50 | 1854 |
| Medium | 36094 | Schooner | 91 | 1857 |
| Medora | 52051 | Brig | 232 | 1865 |
| Melita | 38441 | Schooner | 153 | 1864 |
| Melford ..... | ..... | Schooner | 29 | 1850 |
| Mellville ..... | ..... | Schooner | 28 | 1839 |
| Melantha | 35880 | Schooner | 41 | 1855 |
| Melissa | 42506 | Schooner | 22 | 1862 |
| Mercy & Hope ..... | ..... | Schooner | 40 | 1846 |
| Mercy | 35780 | Brigantine ... | 82 | 1851 |
| Merchant | 42106 | Schooner | 48 | 1836 |
| Meridian ..... | ..... | Schooner | 40 | 1824 |
| Meridian | 37548 | Schooner | 48 | 1834 |
| Meridian ..... | ... . | Schooner ... | 25 | 1848 |
| Merlin | 46669 | Schooner | 53 | 1864 |
| Merlin | 35659 | Schooner | 57 | 1851 |
| Merlin (Steamer) | 35793 | Schooner | 268 | 1850 |
| Mermaid ..... | ..... | Schooner | 26 | 1819 |
| Mermaid ..... | ..... | Schooner | 13 | 1838 |
| Mermaid ..... | ..... | Schooner | 30 | 1839 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|--------------------------------|-------------------|
| Wilmot..... | Timothy Slocomb, et al. | Digby. |
| Cape Negro..... | James Smith, et al. | Halifax. |
| Pictou | Hugh McEachearn | Arichat. |
| La Have | S. Parks, et al. | Lunenburg. |
| Broad Cove | John Messervey | Halifax. |
| Chester..... | Gaspar Eisanhaur, et al. | Halifax. |
| Noel | John Niel, et al. | Windsor. |
| Guy'sborough | Joseph Faugere, et al. | Arichat. |
| Strait of Canso ... | James A. Torey | Guy'sborough. |
| Shelburne..... | John Purney..... | Halifax. |
| St. Mary's Bay ... | William Pryor..... | Halifax. |
| La Have | Henry Lawson | Halifax. |
| United States..... | J. G. McKea | Arichat. |
| Weymouth | James B. Oxley..... | Halifax. |
| Newburyport, U. S. | John V. McIlroy..... | Windsor. |
| Gloucester, U. S. ... | William Owen Heffernan | Guy'sborough. |
| Barrington | Isaac Nickerson, et al. | Halifax. |
| Argyle..... | Charles D'Eon, et al. | Halifax. |
| Digby..... | Crocher Woodman, et al. | Digby. |
| Port Medway..... | F. Vogler, et al. | Liverpool, N. S. |
| Glasgow, Scotland. | The Honorable Samuel Cunard .. | Halifax. |
| Lunenburg..... | Charles Gray..... | Halifax. |
| Spry Harbor | John Logan..... | Halifax. |
| Lunenburg..... | John Keiser..... | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|------------------|------------------|-------|-------------|
| Mermaid | | Schooner | 50 | 1839 |
| Mermaid | | Schooner | 15 | 1842 |
| Mermaid | | Schooner | 32 | 1832 |
| Messenger | 38417 | Schooner | 29 | 1861 |
| Messenger | 37013 | Schooner | 54 | 1860 |
| Messenger | | Schooner | 41 | 1817 |
| Messenger | | Schooner | 22 | 1854 |
| Meteor | 52038 | Schooner | 88 | 1865 |
| Micmac Indian | 41539 | Schooner | 41 | 1859 |
| Micmac | 42509 | Brigantine . . . | 143 | 1862 |
| Milo | 38380 | Brigantine . . . | 208 | 1858 |
| Milo | | Schooner | 16 | 1826 |
| Milo | 36083 | Brigantine . . . | 155 | 1857 |
| Milo | 35631 | Brig | 126 | 1854 |
| Minnie | 38466 | Schooner . . . | 54 | 1865 |
| Minnie Arnold | 46088 | Schooner | 82 | 1862 |
| Minnie Gordon | 36914 | Barque | 322 | 1861 |
| Minnehaha | 48150 | Brigantine . . . | 244 | 1864 |
| Minnehaha | 49484 | Brigantine . . . | 177 | 1864 |
| Minnesota | 52057 | Schooner | 7 | 1865 |
| Minnie K. | 46302 | Brigantine . . . | 254 | 1864 |
| Minna | 49437 | Schooner | 62 | 1865 |
| Minerva | | Schooner | 77 | 1820 |
| Minerva | | Schooner | 48 | 1821 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|--------------------------------------|-------------------|
| Green Harbor | Samuel Locke, Junr., et al | Halifax. |
| Spry Harbor | William H. Rudolf | Halifax. |
| Mahone Bay | John H. McNab | Halifax. |
| Cheticamp | John Burgess, et al | Arichat. |
| Shelburne | John W. Young | Lunenburg. |
| Clare | William Davison | Halifax. |
| Margarce, C. B. . . | Leman Chasson | Halifax. |
| Annapolis | Isaac B. Bonnett, et al | Annapolis. |
| Indian Harbor . . . | John Church | Guysborough. |
| La Have | Robert Dawson | Lunenburg. |
| Little Arichat . . . | William Bowen | Arichat. |
| New Edinburgh . . | William Rice | Halifax. |
| Londonderry | Francis J. Cortiso | Halifax. |
| Cornwallis | Thomas Townsend, et al | Halifax. |
| River Bourgeois . . | J. R. L. McLean | Arichat. |
| Shelburne | Joshua Snow, et al | Shelburne. |
| River John | Frank McGurn | Pictou. |
| Princeport | Joseph Wier, et al | Halifax. |
| Sandy Cove | Gersham Morehouse, et al | Digby. |
| Cornwallis | James Henry Horton | Windsor. |
| Liverpool | Daniel Kellaher | Liverpool. |
| Argyle | John P. Muse, et al | Yarmouth. |
| Truro | John Crow | Halifax. |
| Argyle | Philip D'Entremont | Halifax. |

| Vessel's Name. | Official Number. | Rlg. | Tons. | When built. |
|----------------------------|------------------|---------------|-------|-------------|
| Minerva ..... | ..... | Schooner | 96 | 1827 |
| Minerva | 36037 | Schooner | 27 | 1835 |
| Minerva | 36230 | Schooner | 119 | 1859 |
| Mira | 49460 | Barque | 418 | 1864 |
| Miranda | 38646 | Brigantine... | 137 | 1864 |
| Mirella | 50762 | Brigantine... | 129 | 1864 |
| Mischief ..... | ..... | Schooner | 41 | Un'kn. |
| Mischief | 42791 | Schooner | 93 | 1861 |
| Miscouche | 46656 | Schooner | 72 | 1863 |
| Mist | 36498 | Schooner | 32 | 1860 |
| Mizpah | 49485 | Schooner | 44 | 1865 |
| Mogue | 38422 | Schooner | 120 | 1861 |
| Montebello | 36442 | Schooner | 73 | 1859 |
| Montrose | 37004 | Schooner | 64 | 1860 |
| Montro | 42225 | Schooner | 23 | 1861 |
| Monitor | 38202 | Schooner | 105 | 1862 |
| Moro | 49003 | Schooner | 49 | 1829 |
| Morning Light | 37499 | Schooner | 76 | 1857 |
| Morning Light | 42345 | Schooner | 38 | 1863 |
| Morning Star ..... | ..... | Schooner | 17 | 1836 |
| Morning Star | 39033 | Schooner | 64 | 1838 |
| Morning Star | 38413 | Schooner | 25 | 1861 |
| Morning Star | 37893 | Ship | 697 | 1856 |
| Morning Star | 42114 | Schooner | 85 | 1861 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|----------------------------------|-------------------|
| New Edinburgh... | Stephen LeBlanc..... | Halifax. |
| La Have | William Whitman..... | Halifax. |
| Londonderry . . . | Thomas D. Fletcher..... | Halifax. |
| Lower Maccan.... | Benjamin Crawley, et al. | Pugwash. |
| North Sydney | Thomas D. Archbold | Sydney, C. B. |
| Mahone Bay | Benjamin W. Salter, et al. | Halifax. |
| United States..... | Samuel Lawrence | Arichat. |
| Shelburne | Lovitt Willet, et al | Halifax. |
| Clare | William Michan, et al. | Digby. |
| La Have | Benjamin Wier | Halifax. |
| Bear River | Gilbert F. Troop, et al | Digby. |
| Little Arichat. | Joseph Gunner | Arichat. |
| Sable River. . . . | Howard Locke..... | Shelburne. |
| Sable River..... | James G. Allen..... | Shelburne. |
| Wilmot..... | John Spicer, et al..... | Annapolis. |
| Shelburne | John W. Lovett, et al. | Yarmouth. |
| Foreign | Amos Seaman..... | Parrsborough. |
| Liverpool. | S. & S. Kempton, et al | Liverpool, N. S. |
| Chezzetcook. | John Smith | Halifax. |
| Liverpool | M. A. Larn..... | Arichat. |
| Lunenburg. | Daniel Campbell | Arichat. |
| River Bourgeois. . | Euseleus Boudroit..... | Arichat. |
| Cornwallis | Ezra Churchill, et al..... | Windsor. |
| Parrsborough. | William Miller, et al..... | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Morning Star..... | ..... | Schooner | 39 | 1820 |
| Morning Star..... | ..... | Schooner | 24 | 1831 |
| Morning Star..... | ..... | Schooner | 19 | 1836 |
| Morning Star..... | ..... | Schooner | 19 | 1833 |
| Morning Star..... | ..... | Schooner | 49 | 1837 |
| Morning Star..... | ..... | Schooner | 18 | 1837 |
| Morning Star..... | ..... | Schooner | 17 | 1838 |
| Morning Star..... | ..... | Brigantine ... | 92 | 1840 |
| Morning Star..... | ..... | Schooner | 26 | 1840 |
| Morning Star | 36239 | Schooner | 42 | 1853 |
| Morning Star..... | ..... | Schooner | 73 | 1853 |
| Morning Star..... | ..... | Schooner | 17 | 1861 |
| Morning Star..... | ..... | Schooner ... | 9 | 1861 |
| Morning Star..... | ..... | Schooner | 18 | 1844 |
| Moses Black | 42006 | Schooner | 68 | 1858 |
| Mountaineer..... | ..... | Schooner | 99 | 1847 |
| Mozart | 46387 | Ship..... | 781 | 1863 |
| M. T. Ellsworth | 37860 | Brigantine.... | 116 | 1855 |
| Mulgrave | 37622 | Schooner | 68 | 1858 |
| Murdocks..... | ..... | Schooner | 40 | 1831 |
| Mutu..... | ..... | Schooner | 27 | 1851 |
| Murtilla..... | ..... | Schooner | 54 | 1833 |
| Myrtle | 36099 | Schooner | 40 | 1856 |
| Mystery..... | ..... | Schooner | 45 | 1852 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|---------------------------------------|-------------------|
| Tancook | Frederick Slauenwhite, et al. | Halifax. |
| Jeddore | Isaac Hopkins, et al. | Halifax. |
| La Have | Thomas Smith, et al. | Halifax. |
| Clam Harbor | William Webber, et al. | Halifax. |
| Kempt | James Morris, et al. | Halifax. |
| Blandford | John Mynor | Halifax. |
| Liscomb Harbor | George Langell | Halifax. |
| Argyle | Obediah Wilson, et al. | Halifax. |
| Canso | Daniel K. Harris | Halifax. |
| Newfoundland | Edward H. Nickerson | Halifax. |
| Chance Harbor | Robert Boak, Junr. | Halifax. |
| Chezzetcook | Charles Dumphey | Halifax. |
| Chezzetcook | Patrick Dempsey | Halifax. |
| Lunenburg | Benjamin Spencer | Halifax. |
| Wilmot | James Roy, et al. | Digby. |
| Salt River | Enos Churchill, et al. | Halifax. |
| Maitland | T. S. Harding, et al. | Windsor. |
| Cornwallis | Rufus Burgess, et al. | Windsor. |
| Chester | E. J. Robinson | Lunenburg. |
| Guysborough | Donald McGregor | Halifax. |
| Petpiswick | Charles Young, et al. | Halifax. |
| Parrsborough | Stephen Jenks | Halifax. |
| Shelburne | Cornelius White, et al. | Halifax. |
| Port Medway | C. Smith, et al. | Lunenburg. |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|--------------------------------|-------------------|
| Wallace | Rufus Rindress, et al. | Pugwash. |
| Shelburne. | William Perry, et al. | Halifax. |

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REGISTRY OF NOVA SCOTIA SHIPPING.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------|------------------|------|-------|-------------|
| | | | | |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

N.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------|------------------|----------------|-------|-------------|
| Naiad | 37681 | Brigantine ... | 93 | 1862 |
| Naiad | 48442 | Schooner ... | 56 | 1864 |
| Naked Truth | 42121 | Schooner ... | 38 | 1861 |
| Nancy | 38351 | Schooner ... | 16 | 1851 |
| Nancy | 38408 | Schooner ... | 46 | 1861 |
| Nancy | 36486 | Schooner ... | 64 | 1859 |
| Nancy ..... | ..... | Schooner ... | 23 | 1823 |
| Nancy ..... | ..... | Schooner ... | 45 | 1821 |
| Nancy ..... | ..... | Schooner ... | 37 | 1816 |
| Nancy ..... | ..... | Schooner ... | 12 | 1846 |
| Nancy | 35835 | Brig | 111 | 1847 |
| Nancy ..... | ..... | Schooner ... | 26 | 1844 |
| Nancy Anna | 37010 | Schooner ... | 35 | 1860 |
| Napier | 37950 | Brigantine... | 150 | 1854 |
| Napier | 35727 | Schooner ... | 40 | 1854 |
| Napier | 37526 | Schooner ... | 55 | 1854 |
| Napoleon ..... | ..... | Schooner ... | 6 | 1842 |
| Napoleon | 36092 | Schooner ... | 25 | 1857 |
| Nashwauk | 49097 | Barque. | 497 | 1864 |
| Native Lass | 38888 | Schooner ... | 101 | 1864 |
| Native Lass | 32200 | Schooner ... | 110 | 1851 |
| Native | 37020 | Schooner . . | 49 | 1861 |
| Native | 38647 | Schooner ... | 113 | 1864 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|----------------------------------|-------------------|
| Hopewell, C. B. . . | Joseph Reed, et al. | Digby. |
| Noel | John E. O'Brien, et al. | Windsor. |
| Parrsborough. . . . | Hugh Kerr | Windsor. |
| Poulamond. | Michael Fougere | Arichat. |
| Arichat. | J. McLeod, et al | Arichat. |
| Barrington | Samuel Smith, et al | Shelburne. |
| Tutamagouche . . . | Daniel Tead. | Halifax. |
| Cape Breton. | Charles Cooper | Halifax. |
| Cape Negro. | Howard Kenny. | Halifax. |
| Torbay | Louis Pebrine | Halifax. |
| Cornwallis | John Strachan | Halifax. |
| Tracadie. | William R. Cunningham. | Halifax. |
| Port Latour. | Peter D. Smith, et al | Shelburne. |
| Clare | J. W. Lovitt, et al | Yarmouth. |
| Blandford | John Publicover, et al. | Halifax. |
| Lunenburg. | William B. Hamilton. | Halifax. |
| Main-a-Dieu | James Glenie. | Halifax. |
| Spry Harbor | William Leslie. | Halifax. |
| River John. | Charles McLennan, et al. | Pictou. |
| Grand Ruisseau. . . | Paulicamp Gerrior. | Arichat. |
| Baddeck. | Charles J. Campbell. | Sydney, C. B. |
| Shelburne | James Muir | Shelburne. |
| Sydney. | Charles Muggah | Sydney, C. B. |

| Vessel's Name. | Official Number. | Rlg. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Native | 37275 | Schooner | 55 | 1852 |
| Native | 36994 | Schooner | 37 | 1861 |
| Native | 36984 | Schooner | 67 | 1860 |
| Nautilus | 23147 | Brig | 176 | 1851 |
| Nautilus | 37831 | Schooner ... | 75 | 1847 |
| Nautilus ..... | ..... | Schooner | 61 | 1844 |
| Necumtaw | 36104 | Schooner | 38 | 1857 |
| Nelson ..... | ..... | Schooner | 17 | 1853 |
| Nelson ..... | ..... | Schooner | 22 | 1825 |
| Nelly ..... | ..... | Schooner | 33 | 1817 |
| Neptune | 37346 | Schooner | 40 | 1851 |
| Neptune | 37470 | Schooner | 26 | 1839 |
| Neptune ..... | ..... | Schooner | 55 | 1833 |
| Ne Plus Ultra | 37402 | Schooner | 48 | 1857 |
| Nester..... | ..... | Schooner | 52 | 1828 |
| Nettle | 38943 | Schooner | 30 | 1865 |
| Nettle ..... | ..... | Schooner | 39 | 1850 |
| Neva | 42231 | Brigantine ... | 255 | 1862 |
| Neva | 48434 | Schooner | 100 | 1864 |
| New Dublin..... | ..... | Schooner | 40 | 1828 |
| New Providence | 38447 | Schooner | 26 | 1863 |
| Nightingale | 38581 | Schooner | 44 | 1841 |
| Nile | 48437 | Ship | 1336 | 1864 |
| Nile | 37770 | Brigantine ... | 119 | 1848 |

| Where built. | Registered Owner. | Port of Registry. |
|-------------------------|---|-------------------|
| Granville | Alexander Dodge, et al. | Digby. |
| La Have | William Darrow | Liverpool. |
| Broad Cove | Henry Foster | Halifax. |
| Maitland | David Freeze, et al. | Windsor. |
| Five Islands | Henry Lake | Windsor. |
| Argyle | Michael Cronan, et al | Halifax. |
| Necumtaw | William Smith | Halifax. |
| Ragged Island | A. McQuahae | Liverpool, N. S. |
| Petit | Jacob Sanford | Halifax. |
| P. E. Island | Henry Bates | Halifax. |
| Pubnico | Handly Payson, et al | Digby. |
| Lunenburg | A. Smith, et al. | Liverpool, N. S. |
| Shelburne | John Strachan | Halifax. |
| Cape Canso | Abraham Nowland Whitman | Guysborough. |
| New Brunswick | Samuel Green | Halifax. |
| P. E. Island | Thomas Hurd, et al | Halifax. |
| White Head | Abraham Nowland Whitman | Guysborough. |
| Granville | William Smith, et al | Annapolis. |
| Newport | Nicholas Mosher, Junr., et al | Windsor. |
| Lunenburg | Jacob Tanner | Halifax. |
| Cheticamp | P. Robins & Co. | Halifax. |
| Clam Harbor | Philip Bagnell, et al | Sydney, C. B. |
| Windsor | Bennett Smith, et al | Windsor. |
| Cornwallis | Richard McLearn | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------|------------------|----------------|-------|-------------|
| Nile..... | 42303 | Schooner | 17 | 1862 |
| Nimble..... | 38632 | Schooner | 75 | 1862 |
| Nimble..... | 42520 | Schooner | 59 | 1864 |
| Nimble..... | 42388 | Schooner | 44 | 1863 |
| Nimrod..... | ..... | Schooner | 75 | 1833 |
| Ninth of June..... | 36266 | Schooner | 93 | 1860 |
| N. Mosher..... | 48468 | Ship..... | 798 | 1864 |
| Noble..... | ..... | Schooner | 74 | 1851 |
| Noel..... | 37868 | Schooner .. | 67 | 1849 |
| Nonpareil..... | 37871 | Schooner | 74 | 1846 |
| Nora Creneia..... | ..... | Schooner | 19 | 1835 |
| Normanby..... | 43088 | Barque..... | 450 | 1863 |
| Normanby..... | 49082 | Brigantine ... | 198 | 1864 |
| North America..... | ..... | Schooner | 30 | 1839 |
| Northern Light..... | 41655 | Schooner | 69 | 1859 |
| Northern Light..... | 37340 | Schooner | 84 | 1847 |
| Northern Light..... | 41130 | Brigantine ... | 235 | 1856 |
| Norn..... | 42342 | Barque..... | 312 | 1863 |
| Nova Scotia..... | 34691 | Schooner | 60 | 1834 |
| Nova Scotia..... | ... . | Schooner | 23 | 1853 |
| Nymph..... | ..... | Schooner | 76 | 1818 |
| N. & E. Gardner..... | 48089 | Ship | 1465 | 1864 |

| | Where built. | Registered Owner. | Port of Registry. |
|------|--------------------|-----------------------------------|-------------------|
| 62 | Jeddore..... | Peter Mitchell..... | Halifax. |
| 62 | North Sydney ... | Thomas D. Archibald..... | Sydney, C. B. |
| 64 | Mahone Bay | L. & S. S. Zinck, et al.... | Lunenburg. |
| 63 | Jeddore | Henry Yeomans | Halifax. |
| 63 | Kempt | John Nutting Grant, et al | Halifax. |
| 60 | Antigonish | Placide Boudroit | Halifax. |
| 64 | Newt | Nicholas Mosher, Senr., et al.... | Windsor. |
| 51 | Sheet Harbor..... | John H. Chambers | Halifax. |
| 49 | Noel .. | William Bayley, et al | Windsor. |
| 46 | Horton | Daniel Coffill..... | Windsor. |
| 35 | New Harbor..... | William Pryor | Halifax. |
| 63 | New Glasgow..... | James W. Carmichael, et al | Pictou. |
| 64 | Wallace | Amos Pindy..... | Pictou. |
| 39 | Digby | Joseph Stoneman..... | Yarmouth. |
| 59 | Tracadie..... | James M. Hall..... | Pictou. |
| 47 | Granville .. | James Muritt..... | Digby. |
| 56 | Cape George | Isaac Willett | Halifax. |
| 63 | New Glasgow | William J. Stairs, et al | Halifax. |
| 34 | Newport | William Coffill, et al | Halifax. |
| 53 | Margaret's Bay ... | James Redman..... | Halifax. |
| 1818 | Cornwallis | Joseph Sandford, et al | Halifax. |
| 1864 | Asket | Samuel Killam..... | Yarmouth. |

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| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

O.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------|------------------|----------------|-------|-------------|
| O'Brien ..... | .. .. | Schooner | 76 | 1843 |
| Observer | 38034 | Barque | 537 | 1857 |
| Ocean Belle | 37608 | Schooner | 68 | 1858 |
| Ocean Belle | 41662 | Schooner | 49 | 1859 |
| Ocean Bird | 46488 | Schooner | 58 | 1865 |
| Ocean Bird | 48123 | Schooner | 112 | 1864 |
| Ocean Bird | 50791 | Schooner | 9 | 1865 |
| Ocean Bride | 37606 | Schooner | 37 | 1858 |
| Ocean Bride | 46295 | Schooner | 15 | 1864 |
| Ocean Bride | 37573 | Schooner .. . | 21 | 1855 |
| Ocean Bride | 36141 | Schooner | 40 | 1858 |
| Ocean Bride | 36267 | Schooner | 13 | 1860 |
| Ocean Bride | 36271 | Schooner | 16 | 1860 |
| Ocean Bride | 42398 | Schooner | 21 | 1863 |
| Ocean Bride | 37786 | Schooner | 75 | 1855 |
| Ocean Gem | 37513 | Schooner | 36 | 1858 |
| Ocean Pearl | 46030 | Brigantine . . | 140 | 1862 |
| Ocean Queen | 37579 | Schooner | 36 | 1847 |
| Ocean Star | 50795 | Schooner | 56 | 1865 |
| Ocean Star | 39213 | Schooner | 80 | 1857 |
| Ocean Star | 50766 | Brigantine . . | 236 | 1864 |
| Ocean Wave ..... | ..... | Schooner | 16 | 1860 |
| Ocean Wave ..... | .... | Schooner | 23 | 1856 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|----------------------------------|-------------------|
| Noel | John Nutting Parker..... | Halifax. |
| Yarmouth..... | Thomas Killam | Yarmouth. |
| Lunenburg..... | G. Seaboyer, et al | Halifax. |
| Antigonish | James Hounell..... | Halifax. |
| Petite Riviere | Isaac Ritcey, et al. | Lunenburg. |
| Londonderry..... | Joseph M. Youll, et al. | Halifax. |
| Onslow | David S. Higgins, et al. | Halifax. |
| Lunenburg..... | James Babcock | Halifax. |
| Port Mutton..... | J. Leslie, et al | Liverpool, N. S. |
| La Have | Alexander Myers..... | Halifax. |
| Sable River | John Giffin, et al. | Halifax. |
| Liscomb | Alexander Rudolph..... | Halifax. |
| Chezzetcook..... | Andrew Conrad..... | Halifax. |
| Ship Harbor..... | Conrad Marks, et al | Halifax. |
| Strait of Canso ... | Robt. R. Lonergan & J. B. Moody. | Yarmouth. |
| Sable River | L. P. Churchill, et al. | Liverpool, N. S. |
| Parrsborough..... | George Wilcox, et al. | Windsor. |
| La Have | J. Locke. | Halifax. |
| Torbay | Michael Pebrine, et al. | Halifax. |
| Rohl Bay..... | John E. Cummings | Halifax. |
| Richmond..... | John D. Cummings.. | Halifax. |
| Little Harbor..... | Robert Dickie | Shelburne. |
| La Have | M. B. Mitchell..... | Lunenburg. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|----------------------------|------------------|-----------------|-------|-------------|
| Ocean Wave ..... | ..... | Schooner | 29 | 1855 |
| Ocean | 42376 | Barque. | 733 | 1863 |
| Ocenia | 53557 | Schooner | 139 | 1865 |
| Oder | 43055 | Barque. | 332 | 1862 |
| Odessa ..... | ..... | Brigantine. ... | 142 | 1846 |
| Olive ..... | ..... | Schooner | 38 | 1831 |
| Olive ..... | ..... | Schooner | 14 | 1839 |
| Olive | 36189 | Schooner | 21 | 1848 |
| Olive ..... | ..... | Schooner | 19 | 1850 |
| Olive ..... | ..... | Schooner | 36 | 1849 |
| Olive Branch .. | ..... | Schooner ... | 19 | 1849 |
| Olive Branch ..... | ..... | Schooner | 40 | 1832 |
| Olive Branch | ... | Schooner | 39 | 1816 |
| Olive Branch ..... | ..... | Schooner | 30 | 1826 |
| Olive Branch | 36306 | Schooner | 16 | 1860 |
| Olive Matilda | 37657 | Schooner | 98 | 1859 |
| Olivia B. | 36604 | Schooner | 28 | 1859 |
| Olivia Pride | 36289 | Schooner | 35 | 1860 |
| Omar Pasha .. | ..... | Schooner | 17 | 1855 |
| Omer | 46668 | Brigantine ... | 189 | 1864 |
| Only Son | .. . | Schooner | 17 | 1838 |
| Only Son ..... | ..... | Schooner | 21 | 1826 |
| Only Son ..... | ..... | Schooner | 21 | 1826 |
| Only Son ..... | ..... | Schooner | 67 | 1829 |

| When built. | Where built. | Registered Owner. | Port of Registry. |
|-------------|--------------------------|--------------------------------------|-------------------|
| 1855 | La Have | Benjamin Wier, et al | Halifax. |
| 1863 | Digby | James H. Jones, et al | Halifax. |
| 1865 | Onslow | Thomas Lowden, et al. | Halifax. |
| 1862 | Pictou | Charles T. Irving. | Pictou. |
| 1846 | Newport | Robert Green, et al. | Halifax. |
| 1831 | Granville | William Mageney | Halifax. |
| 1839 | Parrsborough | John W. Morris. | Halifax. |
| 1848 | Barrington | Joshua Nickerson, et al. | Halifax. |
| 1850 | New Dublin | Thomas Bazely, et al | Halifax. |
| 1849 | Londonderry | David McLellan | Halifax. |
| 1849 | River John. | Daniel Goodwin | Pictou. |
| 1832 | Bay of Islands | Benjamin Bears, et al. | Halifax. |
| 1816 | Granville | John Mason, et al | Halifax. |
| 1826 | Barrington | Seth Coffin, et al | Halifax. |
| 1860 | Lawrencetown | Henry Lawson. | Halifax. |
| 1859 | Parrsborough. | Paoli Randoll, et al. | Parrsborough. |
| 1859 | Cheverie | David J. Harris. | Windsor. |
| 1860 | Country Harbor. | Ira Pride. | Halifax. |
| 1855 | Lunenburg | J. B. Fay | Lunenburg. |
| 1864 | Clare. | Helarion Theriau, et al | Digby. |
| 1838 | Chezzetcook | J. A. Ernst | Lunenburg. |
| 1826 | Lunenburg. | George Hermann, et al. | Halifax. |
| 1826 | Lunenburg. | Peter Hermann, Jnr., et al | Halifax. |
| 1829 | Annapolis | Henry Rutherford. | Halifax. |

| Vessel's Name. | Official Number. | Big. | Tons. | When built. |
|----------------|------------------|----------------|-------|-------------|
| Only Son ..... | ..... | Schooner | 16 | 1839 |
| Only Son ..... | .... | Schooner | 17 | 1847 |
| Only Son | 35707 | Schooner ... | 46 | 1849 |
| Ontario..... | 37481 | Brigantine ... | .... | 1856 |
| Onward | 37970 | Barque..... | 377 | 1855 |
| Onward | 37881 | Schooner | 91 | 1856 |
| Opal..... | 46909 | Schooner | 49 | 1865 |
| Orange ..... | ..... | Schooner ... | 22 | 1827 |
| Orbit | 37343 | Schooner | 53 | 1848 |
| Orbit ..... | ..... | Schooner | 20 | 1853 |
| Orb..... | ..... | Schooner | 72 | 1841 |
| Orion..... | ..... | Brigantine ... | 124 | 1846 |
| Orion..... | 37516 | Schooner | 86 | 1858 |
| Orion..... | 38254 | Barque..... | 587 | 1863 |
| Orion | 36031 | Schooner | 84 | 1851 |
| Orion.. .. | 36185 | Schooner | 38 | 1859 |
| Orion | 50761 | Brigantine ... | 172 | 1864 |
| Oriental ..... | ..... | Schooner | 94 | 1852 |
| Oriental | 48270 | Brigantine ... | 220 | 1864 |
| Oriental | 46029 | Schooner | 165 | 1862 |
| Oriental | 37331 | Schooner | 90 | 1857 |
| Oriental | 52031 | Schooner | 64 | 1865 |
| Crinoco | 46007 | Brigantine ... | 269 | 1863 |
| Orient..... | ..... | Schooner | 78 | 1828 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|------------------------------------|-------------------|
| Hillsborough | William Appleby | Halifax. |
| Beaver Harbor . . . | William H. Tidmarsh, et al | Halifax. |
| Mahone Bay | John Hawes | Halifax. |
| Shelburne | J. Snow, et al | Liverpool, N. S. |
| Yarmouth | John K. Ryerson, et al | Yarmouth. |
| Cornwallis | Levi W. Eaton | Windsor. |
| Guysborough | Abraham W. Hart | Port Hawkesbury. |
| La Have | Jacob Saite, et al | Halifax. |
| Granville | William B. Smith, et al | Digby. |
| La Have | Joseph Pentz | Halifax. |
| Kempt | Major Greeno, et al | Halifax. |
| Pictou | James Yorston | Pictou. |
| Liverpool | E. McLeod | Liverpool, N. S. |
| St. Mary's Bay . . . | W. H. Townsend, et al | Yarmouth. |
| Clements | Charles Cox | Halifax. |
| Barrington | Warren Doane, et al | Halifax. |
| Ship Harbor | John Bell, et al | Halifax. |
| Pugwash | Andrew N. Stevens | Pictou. |
| Sable River | James Y. Allen | Shelburne. |
| Hantsport | Joseph Lockhart, et al | Windsor. |
| Digby | Thomas A. Gavaza, et al | Pictou. |
| Wilmot | Asaph Bent, et al | Annapolis. |
| Parrsborough | J. N. B. Elderkin, et al | Parrsborough. |
| New Brunswick . . . | Peter Barker | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------------------|------------------|----------------|-------|-------------|
| Oriole (Steamer) | 42379 | Hermaphrodite | 218 | 1862 |
| Orris | 37304 | Schooner | 103 | 1856 |
| Osceolo | 48258 | Schooner | 69 | 1864 |
| Ospray (Steamer) | 35875 | Schooner | 176 | 1848 |
| Ospray | 35701 | Schooner | 60 | 1854 |
| Ospray | 50759 | Schooner | 19 | 1864 |
| Ostrich ..... | ..... | Schooner | 38 | 1838 |
| Ostrich | 36192 | Schooner | 23 | 1852 |
| Oswego | 37904 | Brigantine.... | 161 | 1857 |
| Ottawa | 50779 | Brig | 276 | 1865 |
| Otter | 53559 | Barque. | 327 | 1865 |
| Our Maggie | 46475 | Schooner | 99 | 1864 |

| Where built. | Registered Owner | Port of Registry. |
|-----------------------|----------------------------------|-------------------|
| Mystic, U. S. | Joseph Marie Trayer | Halifax. |
| Granville | Stephen Crosscup, et al. | Digby. |
| Shelburne | Israel K. Bruce, et al | Shelburne. |
| Glasgow, Scotland. | Samuel Cunard | Halifax. |
| La Have | Thomas Bazeley | Halifax. |
| P. E. Island. | David Petrie | Halifax. |
| Mahone Bay | Andrew Walker | Halifax. |
| Shelburne | Israel Wilson | Halifax. |
| Windsor | S. Morton | Liverpool, N. S. |
| Truro | Robert Smith, et al | Halifax. |
| Burnt Coat | Simon F. Barss, et al. | Halifax. |
| Mahone Bay | Thomas Crowell | Lunenburg. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------|------------------|------|-------|-------------|
| | | | | |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

P.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------------|------------------|----------------|-------|-------------|
| Pacific | 49106 | Barque..... | 291 | 1865 |
| Packet | 38328 | Schooner | 34 | 1846 |
| Packet..... | ..... | Schooner | 29 | 1841 |
| Packet | 41630 | Schooner | 13 | 1858 |
| Packet..... | ... . | Schooner | 38 | 1849 |
| Packet..... | ... . | Schooner | 50 | 1834 |
| Palmerston | 37393 | Schooner | 28 | 1857 |
| Palmira | 38024 | Barque..... | 586 | 1856 |
| Palestine..... | ..... | Barque..... | 533 | 1848 |
| Partner | 41534 | Schooner | 14 | 1858 |
| Partner .. | 35913 | Schooner | 24 | 1846 |
| Partners | 38462 | Schooner | 25 | 1865 |
| Paragon | 41922 | Schooner | 35 | 1853 |
| Paragon | . . . | Brig..... | 148 | 1838 |
| Paragon | 35609 | Schooner | 36 | 1835 |
| Paradigm | 37218 | Schooner .... | .... | 1850 |
| Pathfinder | 86912 | Schooner .. | 506 | 1861 |
| Patridge .... | ..... | Schooner | 33 | 1818 |
| Patience | 36062 | Schooner | 39 | 1848 |
| Pavilion | 52060 | Brigantine ... | 119 | 1865 |
| Pearl | 38856 | Schooner | 56 | 1851 |
| Pearl | 37224 | Schooner | 102 | 1855 |
| Fearl | 36237 | Schooner | 28 | 1852 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------------|---------------------------------------|-------------------|
| Pictou | John F. Ives, et al | Pictou. |
| Arichat | Edward Babir | Arichat. |
| Harbor au Bouche | Peter De Costa | Pictou. |
| River John | Cornelius Dwyer | Pictou. |
| Antigonish | William Owen Heffernan | Guysborough. |
| Dorchester | David Graham | Halifax. |
| Strait of Canso | John McGuire, et al | Guysborough. |
| Church Point | Andrew Lovitt, et al | Yarmouth. |
| Windsor | Bennet Smith, et al | Halifax. |
| Torbay | Penjamin Richards, et al | Guysborough. |
| Sable River | John B. Fay | Halifax. |
| River Bourgeois | Celstin Dugas, et al | Arichat. |
| Port Medway | Mason & Dolliver | Liverpool, N. S. |
| Yarmouth | Israel Lovitt, Senr., et al | Halifax. |
| Granville | William B. Fairbanks, et al | Halifax. |
| Hillsburgh | D. & J. Dunlop | Liverpool, N. S. |
| New Glasgow | William J. Crerar | Pictou. |
| Cape Breton | Samuel Condill | Halifax. |
| Chester | William Strachan, et al | Halifax. |
| Newport | George B. Fish, et al | Windsor. |
| Sheet Harbor | Allen McPherson, et al | Sydney, C. B. |
| Granville | Leander Ogilvie, et al | Digby. |
| Ship Harbor | William Hall, Junr | Halifax. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|---------------------------|------------------|----------------|-------|-------------|
| Pearl | 35995 | Schooner | 35 | 1843 |
| Pearl | 35970 | Schooner | 28 | 1850 |
| Pearl | 46876 | Schooner | 33 | 1836 |
| Peacock ..... | ..... | Schooner | 14 | 1837 |
| Peeler ..... | ..... | Schooner | 20 | 1833 |
| Peerless | 37910 | Brigantine.... | 202 | 1857 |
| Peggy ..... | ..... | Schooner | 41 | 1813 |
| Pembroke | 48432 | Barque | 336 | 1864 |
| Penile | 42216 | Schooner | 28 | 1862 |
| Perseverance | 48251 | Brigantine.... | 82 | 1863 |
| Perseverance | 37909 | Schooner | 29 | 1851 |
| Perseverance ..... | ..... | Schooner | 73 | 1838 |
| Perseverance ..... | ..... | Schooner | 29 | 1845 |
| Perseverance ..... | ..... | Schooner | 15 | 1846 |
| Perseverance | 35951 | Schooner | 18 | 1852 |
| Perseverance | 36182 | Schooner | 82 | 1858 |
| Persia | 37887 | Brigantine ... | 135 | 1856 |
| Peri ..... | ..... | Schooner | 46 | 1839 |
| Petrel ..... | ..... | Schooner | 23 | 1863 |
| Petrel | 38449 | Brigantine.... | 142 | 1850 |
| Petrel ..... | ..... | Schooner | 24 | 1845 |
| Petrel ..... | ..... | Schooner | 13 | 1838 |
| Petrel | 48459 | Brigantine.... | 237 | 1864 |
| Pet | 38591 | Schooner | 31 | 1857 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|----------------------------------|-------------------|
| La Have | Robert Turnbull | Halifax. |
| Barrington | William Cunningham, Junr., et al | Halifax. |
| Galisburg, U. S. . . | Richard Gorham, et al. | Halifax. |
| Ship Harbor | James Murphy | Halifax. |
| Digby. | John Lary, et al | Digby. |
| Kempt | Theophilus Cowen, et al | Windsor. |
| Lunenburg | Jacob Melvin. | Halifax. |
| Kempt | Francis G. Parker, et al | Windsor. |
| Liverpool | D. & J. Dunlop. | Liverpool, N. S. |
| Sable River. | William Williams | Shelburne. |
| Cornwallis | John Fullerton | Windsor. |
| Cornwallis | Philip Smith, et al. | Halifax. |
| Westport | George Smith | Halifax. |
| Horton | Isaac Anderson, et al. | Halifax. |
| Jeddore. | George F. Boutilier. | Halifax. |
| Cape Breton | Tom Battys. | Halifax. |
| Hantsport. | Lewis Henry Coxetter | Windsor. |
| Parrsborough. | Jacob F. DeWolf | Halifax. |
| L'Ardoise. | Anne Boudroit. | Arichat. |
| Tatamagouche . . . | Alexander Campbell, et al | Pictou |
| Ragged Island. | J. Hoffman | Liverpool, N. S. |
| Barrington | Eleazer Crowell, et al. | Halifax. |
| Maitland | Samuel D. Brown, et al | Windsor. |
| Cape North | James Challoner | Sydney, C. B. |

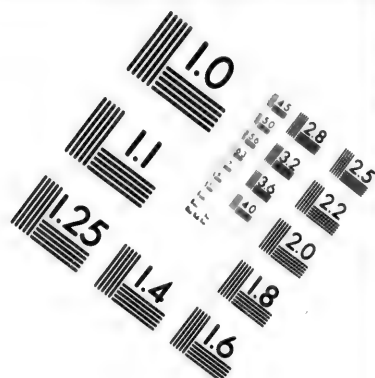
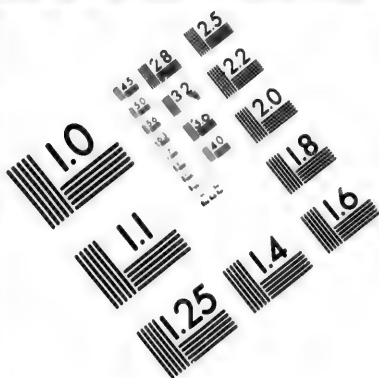
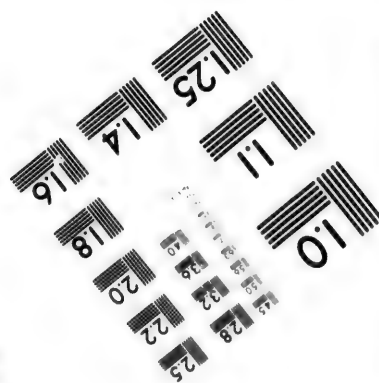
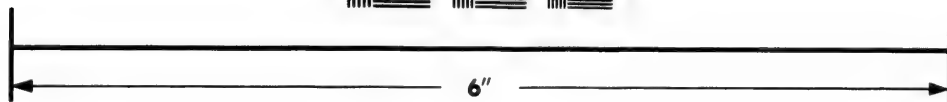
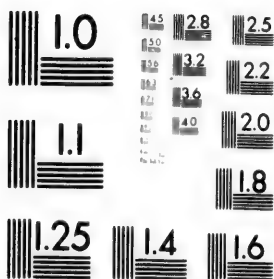


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| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------|------------------|---------------|-------|-------------|
| Petit Pan | 37616 | Schooner | 42 | 1858 |
| Petit Riviere | 37497 | Schooner | 40 | 1857 |
| Phantom | 37517 | Schooner | 73 | 1858 |
| Phantom | 36325 | Schooner | 85 | 1860 |
| Pheasant | 46651 | Schooner | 72 | 1860 |
| Philip Benest ..... | ..... | Schooner | 15 | 1846 |
| Philomel | 38414 | Schooner | 26 | 1861 |
| Phoenix | 38301 | Schooner | 35 | 1850 |
| Phoenix ..... | ..... | Schooner | 13 | 1845 |
| Phoenix | 42071 | Schooner | 42 | 1860 |
| Phoenix ..... | ..... | Schooner | 40 | 1815 |
| Phoebe ..... | ..... | Schooner | 53 | 1827 |
| Pictou | 43098 | Barque. | 299 | 1864 |
| Pictou Packet | 37737 | Schooner | 59 | 1850 |
| Pigeon | 46374 | Schooner | 21 | 1863 |
| Pilot | 37212 | Schooner | 27 | 1850 |
| Pilot ..... | ..... | Schooner | 35 | 1845 |
| Pilgrim | 34977 | Schooner | 40 | 1848 |
| Pilgrim ..... | ..... | Schooner | 41 | 1825 |
| Pilgrim ..... | ..... | Schooner | 36 | 1831 |
| Pink Seaflower ..... | ..... | Schooner | 15 | 1846 |
| Pioneer | 49001 | Schooner | 99 | 1864 |
| Pioneer | 38144 | Brig. | 230 | 1860 |
| Pioneer ..... | ..... | Schooner ... | 68 | 1846 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|-----------------------------------|-------------------|
| La Have | B. Westhaver, et al. | Lunenburg. |
| Petite Riviere | W. Falt, et al | Liverpool, N. S. |
| Liverpool | C. McLeod, et al. | Liverpool, N. S. |
| La Have | Archibald W. McLachlan. | Halifax. |
| | Benjamin Short. | Digby. |
| River Bourgeois .. | William Bourgere, et al | Arichat. |
| River Bourgeois... | Placid Duggas. | Arichat. |
| Little Arichat. | William Crichton | Arichat. |
| Parrsborough. | Allen McLellan. | Parrsborough. |
| Wilmot. | Robert McKenzie, et al | Digby. |
| New Brunswick... | John Anderson | Halifax. |
| Cape Breton | Thomas Simons | Halifax. |
| Pictou. | John T. Ives | Pictou. |
| Petite Riviere | Prudence Marmaud. | Halifax. |
| Cornwallis | John Johnson. | Windsor. |
| Digby. | James McMaster. | Digby. |
| Cape Breton | Joseph W. E. Darby. | Halifax. |
| Cornwallis | Daniel Coffill. | Windsor. |
| Digby. | Gilbert Lawrence, et al. | Halifax. |
| New Brunswick... | John York. | Halifax. |
| Cape Negro. | Samuel King. | Halifax. |
| Parrsborough. | Edward Vickery, et al | Parrsborough. |
| Shelburne. | Thomas Killam, et al. | Yarmouth. |
| Shelburne | John Robertson, et al | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Pioneer | 36063 | Schooner | 27 | 1848 |
| Pioneer | 36106 | Schooner | 12 | 1857 |
| Pioneer | 48474 | Schooner | 49 | 1865 |
| Pitho | 35762 | Brigantine ... | 119 | 1853 |
| Planet | 37545 | Schooner | 26 | 1852 |
| Planet | 37746 | Schooner | 57 | 1847 |
| Planet ..... | ..... | Schooner | 36 | 1848 |
| Planet | 36119 | Schooner | 33 | 1838 |
| Planet | 35953 | Schooner | 61 | 1856 |
| Planet | 37676 | Brigantine.... | 135 | 1861 |
| Plantagenet | 35665 | Brig | 215 | 1853 |
| Placid | 36233 | Schooner | 23 | 1859 |
| Pleiades | 46095 | Brigantine ... | 109 | 1863 |
| P. L. G | 38421 | Schooner | 67 | 1861 |
| Plover | 37524 | Schooner | 22 | 1855 |
| Plover | 35936 | Schooner | 44 | 1852 |
| Ploughboy ..... | ..... | Schooner | 39 | 1823 |
| Ploughboy ..... | ..... | Schooner | 9 | 1830 |
| Pluto (Steamer) ..... | ..... | Schooner | 37 | 1850 |
| Plymouth | 42841 | Schooner | 82 | 1862 |
| P. Martin | 36511 | Schooner ... | 19 | 1860 |
| Pollie Jones | 42097 | Brigantine ... | 147 | 1863 |
| Polly ..... | .. .. | Schooner | 17 | 1839 |
| Pomona ..... | ..... | Brigantine.... | 84 | 1847 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|---------------------------------------|-------------------|
| Eastport, U. S | Cornelius R. Henson | Halifax. |
| St. Peter's | Roderick G. Morrison | Halifax. |
| Cornwallis | David H. Clark, et al | Windsor. |
| Dartmouth | Nathaniel L. West, et al | Halifax. |
| Chester | W. Zwicker | Lunenburg. |
| Barrington | Joshua Nickerson, et al | Halifax. |
| Port Medway | Ferguson W. Foster | Halifax. |
| Cape Negro | Hiram Nickerson, et al | Halifax. |
| Truro | William Gormley, et al | Halifax. |
| Parrsborough | John Henry Tobin | Halifax. |
| Walton | T. S. Harding, et al | Windsor. |
| Chester | John McCuish | Halifax. |
| Shelburne | Samuel Muir, et al | Shelburne. |
| Little River | James Cruikshanks | Arichat. |
| Liverpool | Henry Caare | Arichat. |
| Sable River | John Smith, et al | Yarmouth. |
| Parrsborough | Daniel Steadman | Halifax. |
| Yarmouth | Samuel Foot | Halifax. |
| New Glasgow | Samuel Cunard | Pictou. |
| Parrsborough | Walter Wm. Barts, et al | Windsor. |
| Liverpool | John Murphy | Halifax. |
| Little River | Meudal Crocker, et al | Digby. |
| Ship Harbor | Patrick O'Brien, et al | Halifax. |
| Shelburne | Alexander McNaughton, et al | Halifax. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|------------------------------|------------------|---------------|-------|-------------|
| Pondora ..... | ..... | Brig | 112 | 1836 |
| Port of Spain ..... | ..... | Schooner | 69 | 1840 |
| Potosi | 52061 | Brigantine... | 239 | 1835 |
| Precilla | 37374 | Schooner | 18 | 1847 |
| Presto | 48081 | Barque | 621 | 1863 |
| President | 36097 | Brigantine .. | 105 | 1857 |
| President | 38121 | Schooner | 85 | 1860 |
| Princess | 38637 | Barque | 369 | 1863 |
| Princess Alice | 50796 | Schooner | 33 | 1846 |
| Princess Dagman | 37064 | Brigantine .. | 127 | 1863 |
| Princess Louisa ..... | ..... | Schooner | 43 | 1851 |
| Princess Royal | 38407 | Schooner | 29 | 1860 |
| Princess Royal | 38069 | Brig | 260 | 1858 |
| Princess Royal | 42847 | Barque | 263 | 1862 |
| Prince ..... | ..... | Schooner | 64 | 1849 |
| Prince Alfred | 36345 | Brigantine... | 138 | 1861 |
| Prince Albert | 46366 | Barque | 387 | 1863 |
| Prince Albert | ... | Schooner | 7 | 1840 |
| Prince Albert ..... | ..... | Schooner | 18 | 1840 |
| Prince Albert ..... | ..... | Schooner | 40 | 1844 |
| Prince Consort | 37605 | Schooner | 38 | 1857 |
| Prince Henry ..... | ..... | Schooner | 70 | 1844 |
| Prince Le Bor | 42014 | Schooner | 65 | 1846 |
| Prince Rupert ..... | ..... | Schooner | 60 | 1845 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|--------------------------------|-------------------|
| Yarmouth..... | Thomas Crossby | Halifax. |
| Ragged Island.... | J. Hoffman..... | Liverpool, N. S. |
| Windsor | Curry & Shand, et al..... | Windsor. |
| Canso | J. McKenzie | Liverpool, N. S. |
| Saulnerville | A. C. Robbins, et al | Yarmouth. |
| St. Mary's | George Hewett, et al..... | Halifax. |
| Tusket Wedge.... | Augustus Stoneman, et al. | Yarmouth. |
| North Bar, C. B.. | Thomas D. Archibald..... | Sydney, C. B. |
| New Dublin..... | John Hartlin | Halifax. |
| Strait of Canso .. | Benjamin Wier, et al | Guysborough. |
| La Have | Jacob Romkey, et al | Halifax. |
| L'Ardoise..... | Silvester Sampson, et al | Arichat. |
| Tusket | G. S. Brown, et al | Yarmouth. |
| Cornwallis | Jacob Lockhart, et al..... | Windsor. |
| P. E. Island..... | James McDaniel | Halifax. |
| Londonderry . . . | Barnabas Higgins, et al..... | Halifax. |
| Cornwallis | Albert Chase, et al | Windsor. |
| Wilmot..... | Robert Denham..... | Halifax. |
| Musquodoboit.... | William Stratford | Halifax. |
| Mahone Bay | Benjamin Mader, et al..... | Halifax. |
| Mahone Bay | S. Beck, et al..... | Lunenburg. |
| Weymouth | R. Manning, et al | Lunenburg. |
| Granville | John Kennedy, et al | Digby. |
| Barrington | Henry Doar, et al..... | Halifax. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|------------------------|------------------|---------------|-------|-------------|
| Prince of Wales..... | 36318 | Schooner | 42 | 1860 |
| Prima Donna ..... | ..... | Barque..... | 319 | 1854 |
| Primrose..... | 41641 | Schooner | 23 | 1859 |
| Primrose..... | 41776 | Schooner | 31 | 1858 |
| Primrose..... | ..... | Schooner | 13 | 1819 |
| Primrose..... | ..... | Schooner | 17 | 1827 |
| Primrose..... | 34734 | Schooner | 81 | 1854 |
| Prime | 37940 | Schooner | 58 | 1860 |
| P. R. McKenzie | 36450 | Schooner .. | 41 | 1860 |
| Pride of the North ... | 36067 | Schooner | 40 | 1857 |
| Proctor..... | 34718 | Schooner | 30 | 1855 |
| Progress | 35942 | Schooner | 59 | 1854 |
| Promenade | 42785 | Schooner | 71 | 1861 |
| Propellor | 36173 | Schooner | 25 | 1858 |
| Prosperity ..... | ..... | Schooner | 18 | 1825 |
| Prosperity ..... | ..... | Schooner | 20 | 1819 |
| Prospect ..... | ..... | Schooner | 22 | 1832 |
| Prospect ..... | ..... | Schooner | 21 | 1843 |
| Providence ... | 38322 | Schooner ... | 56 | 1852 |
| Providence ..... | ..... | Schooner | 23 | 1852 |
| Prowess..... | 35884 | Schooner | 39 | 1851 |
| Prowess..... | ..... | Schooner | 49 | 1827 |
| Purser | 38587 | Schooner | 72 | 1857 |
| Pursuit..... | 36058 | Schooner | 55 | 1852 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|----------------------------------|-------------------|
| Londonderry ... | Augustus Stayner, et al. | Halifax. |
| New Glasgow | Donald McDonald. | Pictou. |
| Point Brule | Isaac Langille, et al. | Pictou. |
| La Have | B. Wentzell, et al. | Lunenburg. |
| Newport | Caleb Cook, Senr. | Halifax. |
| Spry Harbor | John E. Fairbanks, et al. | Halifax. |
| Tangier | Simon Mayer, et al. | Halifax. |
| Argyle | George Killam. | Yarmouth. |
| Sable River. | Peter McKenzie, et al. | Shelburne. |
| Shelburne | Steelman Nickerson | Halifax. |
| Granville | Joshua Brenton, et al. | Digby. |
| Pubnico | J. B. Moody, et al. | Yarmouth. |
| Shelburne | Martin Doane, et al. | Shelburne. |
| Barrington | George Smith. | Halifax. |
| St. Mary's | Frederick Dauphiney | Halifax. |
| Newport | Caleb Cook, Senr. | Halifax. |
| Yarmouth | Edward B. Marshall, et al. | Halifax. |
| Douglass | Joseph McLellan. | Halifax. |
| Little Arichat. | Valentine Buntior | Arichat. |
| Arichat | Oliver Chassam. | Arichat. |
| La Have | D. Morim. | Lunenburg. |
| Clements | Thomas W. DeWolf, et al. | Halifax. |
| Mira, C. B. | James McDonald. | Sydney, C. B. |
| Barrington | Alexander Nickerson, et al. | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------|---------------------|---------------|-------|----------------|
| Pursuit | . . . | Schooner | 72 | 1852 |
| P. Blake | 37644 | Schooner | 103 | '856 |

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| Where built. | Registered Owner | Port of Registry. |
|---------------------|-----------------------------------|--------------------------|
| St. Mary's River.. | Israel Nickerson, et al | Halifax. |
| Parrsborough. | James Malcom, et al | Halifax. |

REGISTRY OF NOVA SCOTIA SHIPPING.

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| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

Q.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------------|------------------|----------------|-------|-------------|
| Quango | 38617 | Brig | 199 | 1860 |
| Queen | 42378 | Schooner | 13 | 1863 |
| Queen Charlotte ..... | ..... | Schooner | 82 | 1818 |
| Queen of Clippers | 41552 | Schooner | 172 | 1859 |
| Queen of the East | 36136 | Schooner | 24 | 1857 |
| Queen of the Forest | 41546 | Schooner | 22 | 1858 |
| Queen of the Fleet | 38058 | Barque | 595 | 1857 |
| Queen of the Scots | 48439 | Barque | 672 | 1864 |
| Queen Victoria ..... | ..... | Schooner | 15 | 1837 |
| Queen of the West | 37563 | Brigantine ... | 129 | 1855 |
| Quindora | 43080 | Barque | 195 | 1840 |
| Quickstep | 38569 | Schooner | 26 | 1856 |
| Quickstep | 36009 | Schooner | 99 | 1856 |
| Quickstep | 35993 | Schooner | 47 | 1856 |
| Quickstep | 46485 | Schooner | 40 | 1865 |
| Quito | 36290 | Schooner | 24 | 1859 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|-----------------------------------|-------------------|
| North Bar | Thomas D. Archbold. | Sydney, C. B. |
| Chezzetcook | Joseph Julian | Halifax. |
| Argyle | Ansel Nickerson, et al. | Halifax. |
| Newport. | Joseph Lockhart, et al. | Windsor. |
| Chezzetcook | George Smith. | Halifax. |
| Cornwallis | William V. Jones, et al. | Windsor. |
| Yarmouth. | Aaron Goudry. | Yarmouth. |
| Cornwallis | Jacob Lockhart, et al. | Windsor. |
| Porter's Lake | William Graham, et al. | Halifax. |
| Mahone Bay | Nathaniel L. West, et al. | Halifax. |
| Kingston, U. S. | Edwin Pinkham | Pictou. |
| St. Ann's, C. B. | William Ross. | Sydney, C. B. |
| Bedford Basin. | William J. Cook, et al. | Halifax. |
| St. Mary's | James B. Oxley | Halifax. |
| Mahone Bay | James B. Ernst, et al. | Lanenburg. |
| New Dublin. | Jacob Croft | Halifax. |

| Where built. | Registered Owner. | Port of Registry |
|--------------|-------------------|------------------|
| | | |

R.

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|----------------------------------|------------------|---------------|-------|-------------|
| Rachel A. Elizabeth..... | ..... | Schooner | 46 | 1851 |
| Rachel | 58562 | Schooner | 43 | 1851 |
| Rachel ..... | ..... | Schooner | 67 | 1834 |
| Racer | 42368 | Schooner | 76 | 1863 |
| Radama | 43043 | Barque..... | 349 | 1862 |
| Rainbow | 43045 | Barque..... | 485 | 1862 |
| Rainbow ..... | ..... | Schooner | 19 | 1831 |
| Rainbow ..... | ..... | Schooner | 55 | 1818 |
| Rainbow ..... | ..... | Schooner | 99 | 1845 |
| Rainbow | 38538 | Schooner | 12 | 1836 |
| Rainbow ..... | ..... | Schooner | 27 | 1853 |
| Rambler | .. . | Schooner | 24 | 1840 |
| Rambler | 49086 | Barque..... | 296 | 1864 |
| Rambler | 38559 | Schooner | 55 | 1849 |
| Rambler ..... | ..... | Schooner | 8 | 1847 |
| Rambler | 37314 | Schooner | 23 | 1846 |
| Rambler ..... | ..... | Schooner | 20 | 1841 |
| Rambler ..... | ..... | Schooner | 11 | 1839 |
| Rambler ..... | ..... | Schooner | 35 | 1830 |
| Rambler ..... | ..... | Schooner | 78 | 1839 |
| Ranger ..... | ..... | Schooner | 24 | 1859 |
| Randolph P. ..... | ..... | Schooner ... | 14 | 1861 |
| Ransom .. . | ..... | Schooner | 83 | 1852 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|---------------------------------|-------------------|
| Quaceo, N. B. | David Smith, et al. | Windsor. |
| Little Arichat | Thomas Roach. | Sydney, C. B. |
| Parrsborough. | David Loomer. | Halifax. |
| Clyde River. | Benjamin W. Salter, et al. | Halifax. |
| New Glasgow | William Forbes, et al. | Pictou. |
| River John. | James Kitchen, et al. | Pictou. |
| La Have. | John Countava. | Halifax. |
| Argyle. | Michael Michener. | Halifax. |
| Maitland. | Archibald McCullum. | Halifax. |
| Argyle. | Thomas Young, et al. | Halifax. |
| Jeddore. | Charles E. Wiswell. | Halifax. |
| Bedeque, P. E. I. . | James Purvis. | Pictou. |
| New Glasgow. | James McKinnon, et al. | Pictou. |
| Main-a-Dieu. | Henry Edward Townsend, | Sydney, C. B. |
| Digby. | David Hayden. | Digby. |
| Ragged Island. | C. Hackett. | Liverpool, N. S. |
| La Have. | Benjamin Wentzel, et al. | Lunenburg. |
| Cape Negro. | Josiah Swain. | Halifax. |
| La Have. | Daniel Smith. | Halifax. |
| New Brunswick. . | John Nutting Grant, et al. | Halifax. |
| Tatamagouche. | Peter Matellall. | Pictou. |
| Westport. | Maurice Haycock. | Digby. |
| Cornwallis. | William J. McNelly, et al. | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------|------------------|----------------|-------|-------------|
| Rapid ..... | .. .. | Schooner | 69 | 1858 |
| Rapid ..... | ..... | Schooner | 63 | 1854 |
| Rapid | 42380 | Brigantine ... | 144 | 1863 |
| Ray | 48120 | Schooner | 27 | 1863 |
| Reaper | 38431 | Brigantine.... | 155 | 1862 |
| Rebecca ..... | ..... | Schooner | 12 | 1836 |
| Rechalite | 41635 | Barque | 327 | 1858 |
| Redwing ..... | ..... | Schooner | 30 | 1846 |
| Redan | 36065 | Schooner | 24 | 1856 |
| Reform ..... | ..... | Schooner | 15 | 1854 |
| Reform ..... | ..... | Brigantine ... | 92 | 1832 |
| Reform ..... | ..... | Schooner | 25 | 1837 |
| Reform ..... | ..... | Schooner | 30 | 1853 |
| Regatta | 50757 | Brigantine ... | 112 | 1864 |
| Regent | 50756 | Schooner | 101 | 1864 |
| Regina | 42510 | Brigantine ... | 139 | 1862 |
| Regina | 48431 | Barque | 599 | 1864 |
| Regulator | 37369 | Schooner ... | 37 | 1852 |
| Regulator ..... | ..... | Schooner | 31 | 1845 |
| Regulator | 35643 | Schooner | 40 | 1851 |
| Reindeer | 36440 | Brigantine ... | 161 | 1859 |
| Reindeer | 42349 | Schooner | 72 | 1847 |
| Reindeer | 37126 | Schooner ... | 33 | 1856 |
| Reindeer ..... | ..... | Schooner | 37 | 1844 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|-----------------------------------|-------------------|
| North Sydney, C.B. | John Evans | Sydney, C. B. |
| Pope's Harbor | Abraham Bolong | Halifax. |
| Guysborough | Benjamin Wier, et al | Halifax. |
| La Have | James Bell | Halifax. |
| Arichat | F. Marmaud, et al | Arichat. |
| Merigomish | John Dunn | Halifax. |
| New Glasgow | John Printon | Pictou. |
| United States | Alexander McDonald | Halifax. |
| Marie Joseph | Charles W. Wright | Halifax. |
| Wallace | James Cairne | Pictou. |
| Clare | Charles Jones | Halifax. |
| St. Mary's | Elisha Pride | Halifax. |
| United States | William B. Hamilton | Halifax. |
| Barrington | John B. Hopkins, et al | Halifax. |
| Mira Bay | Roderick McDonald | Halifax. |
| La Have | E. & B. Rothenhiser | Lunenburg. |
| Windsor | George Armstrong, et al | Windsor. |
| Strait of Canso . . . | James R. L. McLean | Guysborough. |
| La Have | G. Salvano, et al | Lunenburg. |
| La Have | William Pryor | Halifax. |
| Shelburne | Joshua Snow, et al | Shelburne. |
| Five Islands | James Good | Parrsborough. |
| St. Martin's | John Rice, et al | Digby. |
| Newfoundland | James S. Cunningham | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------------|------------------|----------------|-------|-------------|
| Reindeer | 35377 | Brig..... | 130 | 1854 |
| Reindeer | 36348 | Schooner ... | 59 | 1861 |
| Reliance | 41802 | Schooner ... | 71 | 1858 |
| Reliance | 38066 | Schooner ... | 43 | 1858 |
| Reliance ..... | ..... | Schooner ... | 29 | 1830 |
| Relief | 42112 | Brig | 234 | 1860 |
| Relief ..... | ..... | Schooner ... | 39 | 1821 |
| Relief ..... | ..... | Schooner ... | 23 | 1838 |
| Relief ..... | ..... | Schooner ... | 45 | 1848 |
| Remembrancer ..... | ..... | Schooner ... | 27 | 1853 |
| Renown ..... | ..... | Schooner ... | 67 | 1837 |
| Renfrew | 42281 | Schooner ... | 41 | 1861 |
| Restless | 41803 | Brigantine... | 220 | 1858 |
| Research | 38190 | Ship | 1459 | 1861 |
| Rescue | 38012 | Brigantine ... | 239 | 1856 |
| Resolute ..... | ..... | Schooner ... | 13 | 1839 |
| Resolution | 49434 | Schooner ... | 24 | 1854 |
| Resolution ..... | ..... | Schooner ... | 14 | 1822 |
| Resolution ..... | ..... | Schooner ... | 30 | 1829 |
| Resolution ..... | ..... | Schooner ... | 43 | 1834 |
| Resolution ..... | ..... | Schooner ... | 13 | 1844 |
| Return | 38094 | Barque | 309 | 1859 |
| Return | 37868 | Schooner ... | 75 | 1846 |
| Return | 37868 | Schooner ... | 53 | 1845 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|---------------------------------------|-------------------|
| La Have | William B. Hamilton | Halifax. |
| Country Harbor . . . | James Rood | Halifax. |
| Granville | John Abbott, et al | Digby. |
| Beaver River | J. D. Brown | Yarmouth. |
| Country Harbor . . . | Thomas Bell | Halifax. |
| Burnt Coat | Nicholas Mosher, Junr., et al | Windsor. |
| Digby | James Norris, et al | Halifax. |
| La Have | William Barry | Halifax. |
| Barrington | Thomas Coffin, Junr., et al | Halifax. |
| Margaree | Peter Galland, et al | Arichat. |
| Parrsborough | William Glen, et al | Windsor. |
| Isaac's Harbor | John McMullan | Halifax. |
| Granville | Charles Wilson, et al | Annapolis. |
| Yarmouth | Thomas Killam | Yarmouth. |
| Yarmouth | Benjamin Rogers | Yarmouth. |
| Douglass | James B. Moody | Halifax. |
| La Have | R. Robertson, et al | Yarmouth. |
| Cape Negro | William Nickerson | Halifax. |
| La Have | William McLean | Halifax. |
| Cornwallis | James Cochran | Halifax. |
| Argyle | Samuel Reynolds, et al | Halifax. |
| Tusket | Samuel Killam | Yarmouth. |
| Cornwallis | Augustus Eagles, et al | Windsor. |
| Granville | William H. Church | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Return ..... | ..... | Schooner | 20 | 1840 |
| Return ..... | ..... | Schooner | 15 | 1841 |
| Retrieve ..... | ..... | Schooner | 49 | 1831 |
| Revival | .. . | Barque | 452 | 1853 |
| Revenge | 37909 | Schooner | 20 | 1857 |
| Revenge | ... | Schooner | 33 | 1830 |
| Revenge ..... | ..... | Schooner | 13 | 1838 |
| Reuben J. Hart | 53601 | Schooner | 62 | 1865 |
| Reward ..... | ..... | Brigantine.... | 144 | 1854 |
| Reward | 48435 | Schooner | 104 | 1864 |
| R. G. Noble | 36041 | Schooner | 16 | 1856 |
| R. Hains | 49490 | Schooner | 71 | 1865 |
| Rhuama | 49473 | Schooner | 46 | 1864 |
| Richard Brown | 38539 | Brig | 149 | 1847 |
| Richard Cobden | 37861 | Schooner | 83 | 1848 |
| Rifle | 39014 | Schooner | 40 | 1832 |
| Ringdove ..... | ..... | Schooner | 31 | 1834 |
| Ripple | 37007 | Schooner | 23 | 1860 |
| Ripple | 36137 | Schooner | 40 | 1858 |
| Ripple | 48132 | Schooner | 77 | 1864 |
| Ripple | 46486 | Schooner | 62 | 1865 |
| Rising Dawn | 38264 | Barque | 516 | 1863 |
| Rising Dawn | 36284 | Schooner | 45 | 1860 |
| Rising Sun | 24815 | Schooner | 15 | 1836 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|-------------------------------|-------------------|
| Lunenburg..... | Philip Gerhart..... | Halifax. |
| Lunenburg..... | John Halin, Junr., et al..... | Halifax. |
| Merigomish..... | James Purvis..... | Halifax. |
| Tatamagouche... | John Miller, et al..... | Pictou. |
| Cornwallis..... | Simon H. Loomer..... | Windsor. |
| Douglasa..... | Samuel Soley..... | Halifax. |
| Chezzetcook..... | James B. Oxley..... | Halifax. |
| Strait of Canso... | Gaspert Embree, et al..... | Port Hawkesbury. |
| Tatamagouche... | John Miller..... | Pictou. |
| Kempt..... | Daniel D. Martin, et al..... | Windsor. |
| Strait of Canso... | Francis Selff..... | Halifax. |
| Westport..... | Robert Harris, et al..... | Digby. |
| Westport..... | Barney Morrell..... | Digby. |
| St. Ann's, C. B... | John Young, et al..... | Sydney, C. B. |
| Londonderry..... | Benjamin Morrison, et al..... | Halifax. |
| Lunenburg..... | Henry Balcom..... | Halifax. |
| Indian Harbor.... | Rufus Perry, et al..... | Halifax. |
| Ragged Island... | Samuel Locke, Senr..... | Shelburne. |
| Dartmouth..... | Lovitt Willett..... | Halifax. |
| Isaac's Harbor.... | Benjamin Wier, et al..... | Halifax. |
| Lunenburg..... | Robert Lindsay, et al..... | Lunenburg. |
| Pleasant Cove.... | William Robertson, et al..... | Yarmouth. |
| Indian Harbor.... | Daniel Sergeant, et al..... | Halifax. |
| Barrington..... | Samuel Snow, et al..... | Halifax. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|-------------------------------|------------------|---------------|-------|-------------|
| Rising Sun | 35989 | Schooner | 29 | 1856 |
| Rise and Go | 42503 | Schooner | 34 | 1862 |
| Rival | 35893 | Schooner | 74 | 1855 |
| Rival..... | ..... | Schooner | 51 | 1825 |
| Robert | 38609 | Schooner ... | 45 | 1836 |
| Robert Boak | 38453 | Schooner | 28 | 1863 |
| Robert Boak | 50753 | Barque..... | 597 | 1864 |
| Robert Bruce | 38440 | Schooner | 26 | 1844 |
| Robert Bruce..... | ..... | Schooner | 13 | 1847 |
| Robert H. Dexter | 36610 | Schooner | 96 | 1860 |
| Robert Leonard | 3245 | Barque..... | 487 | 1855 |
| Robert Noble | 38652 | Schooner | 114 | 1864 |
| Robert Noble | 50784 | Schooner | 83 | 1865 |
| Robert Scot | 38568 | Schooner | 43 | 1853 |
| Robert Simons | 38275 | Barque..... | 617 | 1863 |
| Robert Stone | 37279 | Schooner | 71 | 1849 |
| Robert Wilson | ... | Schooner ... | 32 | 1814 |
| Robin | 38434 | Schooner | 67 | 1841 |
| Robin..... | ..... | Schooner | 15 | 1836 |
| Rob Roy | 41536 | Schooner | 32 | 1853 |
| Rob Roy | 35732 | Schooner | 74 | 1825 |
| Rob Roy | 49463 | Brig..... | 193 | 1865 |
| Robean..... | ..... | Schooner | 47 | 1830 |
| Robust..... | ..... | Schooner | 42 | 1807 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|--|-------------------|
| Country Harbor . . . | James G. A. Creighton | Halifax. |
| Mahone Bay | Edward Hiltz | Lunenburg. |
| Shelburne | D. & J. Dunlop | Liverpool, N. S. |
| Port Medway | James Clint, et al | Halifax. |
| United States | John McInnes, et al | Sydney, C. B. |
| St. Peter's | J. R. S. McLean | Arichat. |
| Truro | John Taylor, et al | Halifax. |
| Strait of Canso . . . | Henry Wolf | Arichat. |
| Sheet Harbor | Jacob W. Whitman | Guysborough. |
| Quaco, N. B. | Israel J. Dexter, et al | Windsor. |
| Yarmouth | Aaron Goudry, et al | Yarmouth. |
| Gabarus, C. B . . . | John Ormiston, et al | Sydney, C. B. |
| Mahone Bay | Robert G. Noble | Halifax. |
| Louisburg | Thomas Roach | Sydney, C. B. |
| Plymouth | W. H. Moody, et al | Yarmouth. |
| Wilmot | Isaac Roach | Halifax. |
| New Brunswick . . . | Solomon McDonald, et al | Halifax. |
| Salisbury, U. S. . . . | Simon Boudrois | Arichat. |
| Indian Harbor | Simon Fraser, et al | Halifax. |
| Guysborough | Donald McGregor | Guysborough. |
| Bermuda | George A. V. Paw | Halifax. |
| Wallace | John Shaw Foshner | Pugwash. |
| Spry Harbor | John J. Gerard, Senr., et al | Halifax. |
| Lunenburg | Jeffrey Power, et al | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------------|------------------|----------------|-------|-------------|
| Robust ..... | ..... | Schooner | 29 | 1835 |
| Rocket | 37008 | Schooner | 50 | 1860 |
| Rocket ..... | ..... | Schooner | 74 | 1853 |
| Roderick | 37694 | Brigantine.... | 131 | 1852 |
| Rodman Pratt | 49018 | Brig | 261 | 1865 |
| Romaine | 46015 | Brig | 302 | 1863 |
| Romeo | 36196 | Schooner | 21 | 1859 |
| Romp | 42205 | Schooner | 23 | 1861 |
| Romp | 38971 | Schooner | 90 | 1855 |
| Rosannah | 37442 | Brig | 133 | 1849 |
| Rosanna | 41785 | Schooner | 39 | 1859 |
| Rosanada | 37588 | Schooner | 29 | 1857 |
| Rosalie | 49440 | Schooner | 32 | 1865 |
| Rosalie ..... | ..... | Schooner | 14 | 1824 |
| Rosalie | 52006 | Schooner | 46 | 1853 |
| Rose | 37748 | Schooner | 107 | 1850 |
| Rose | 41790 | Schooner | 30 | 1859 |
| Rose ..... | ..... | Schooner | 80 | 1833 |
| Rose | 36252 | Schooner | 23 | 1831 |
| Rose ..... | ..... | Schooner | 29 | 1833 |
| Rose | 36303 | Schooner | 9 | 1856 |
| Rosebud ..... | ..... | Schooner | 13 | 1848 |
| Rosebud | 41640 | Schooner | 28 | 1859 |
| Roseway | 48259 | Brigantine ... | 208 | 1864 |

| Where built. | Registered Owner | Port of Registry. |
|-----------------------|-------------------------------------|-------------------|
| Porter's Lake | William Davison | Halifax. |
| Shelburne | Joshua Snow | Shelburne. |
| Granville | Robert Delap, et al | Halifax. |
| New Glasgow | Celestian Dowart | Arichat. |
| Five Islands | J. H. Pratt, et al | Parrsborough. |
| Parrsborough | John Blenkhorn, et al | Parrsborough. |
| Chezzetcook | John Manger | Halifax. |
| Liverpool | J. Kentshilla | Liverpool, N. S. |
| Bedeque, P. E. I. . . | Benjamin W. Salter, et al | Halifax. |
| Lunenburg | S. Collins | Liverpool, N. S. |
| Mahone Bay | Frederick Conrod | Lunenburg. |
| Lunenburg | Frederick Moser | Lunenburg. |
| Bellevue Cove . . . | Buono White | Yarmouth. |
| Clare | Crocker Hemeon, et al | Halifax. |
| Essex, U. S. | Whitman B. Long, et al | Shelburne. |
| Grand Ruisseau . . . | Charles Grines | Arichat. |
| Lunenburg | A. D. DeWolf | Lunenburg. |
| Wallace | Levi Stephens | Halifax. |
| Lunenburg | David Kiely | Halifax. |
| Ship Harbor | Elisha Hopkins, et al | Halifax. |
| Chezzetcook | George Conrod | Halifax. |
| Jeddore | Hugh Lyle | Halifax. |
| Tatamagouche . . . | Michael Boudroit | Halifax. |
| Shelburne | Joshua Snow, et al | Shelburne. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Roseway | 37350 | Schooner ... | 99 | 1858 |
| Roselia | 42211 | Brigantine ... | 166 | 1861 |
| Rosetta | 49088 | Brigantine ... | 291 | 1864 |
| Rothsay | 42201 | Brigantine... | 152 | 1860 |
| Rover | 37323 | Schooner ... | 50 | 1845 |
| Rover ..... | ..... | Schooner | 60 | 1818 |
| Rover ..... | ..... | Schooner | 13 | 1825 |
| Rover ... .. | ..... | Schooner | 25 | 1824 |
| Rover | 37798 | Schooner | 74 | 1856 |
| Rover | 35960 | Brig | 145 | 1856 |
| Roving Bird | 53551 | Schooner | 24 | 1865 |
| Royal Albert | 36478 | Schooner | 29 | 1859 |
| Royal Arch | 48445 | Schooner | 85 | 1864 |
| Royal Adelaide ..... | ..... | Schooner | 49 | 1832 |
| Royal George ..... | ..... | Schooner | 32 | 1831 |
| Royal Oak ..... | ..... | Schooner | 27 | 1835 |
| Royal Oak ..... | ..... | Schooner | 39 | 1822 |
| Royal Oak ..... | ..... | Schooner | 27 | 1827 |
| Royal Standard | 48045 | Brigantine ... | 207 | 1864 |
| Royal Tar | 37826 | Schooner | 73 | 1855 |
| Royal William ..... | ..... | Schooner | 20 | 1837 |
| Royal William | 35982 | Schooner | 30 | 1831 |
| Ruby | 36995 | Schooner | 52 | 1861 |
| Ruby | 46032 | Brigantine... | 127 | 1862 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------|---------------------------------|-------------------|
| Roseway..... | Robert McCoy, et al..... | Digby. |
| Port Medway..... | E. Cohoon, et al..... | Liverpool, N. S. |
| Tatamagouche.... | John T. West, et al..... | Pictou. |
| Liverpool... .. | A. Cowie & Sons..... | Liverpool, N. S. |
| Granville..... | Robert Harris..... | Digby. |
| Quaco, N. B..... | Stephen Jenks..... | Windsor. |
| Cornwallis..... | Nathaniel Bennett..... | Halifax. |
| Barrington..... | Joseph A. Smith, et al..... | Halifax. |
| Walton..... | Jas. O'Bryan, et al..... | Halifax. |
| Dartmouth..... | George P. Mitchell..... | Halifax. |
| Chezzetcook..... | George Romo..... | Halifax. |
| La Have..... | Matthew King..... | Lunenburg. |
| Cornwallis..... | George E. Eaton, et al..... | Windsor. |
| Mahone Bay..... | William Henry Tidmarsh, et al.. | Halifax. |
| Lunenburg..... | R. Robertson, et al... .. | Lunenburg. |
| Lunenburg..... | Henry Hartley, et al..... | Halifax. |
| Yarmouth..... | Anthony Landers..... | Halifax. |
| Lunenburg..... | Jacob Corkum..... | Halifax. |
| Clements..... | Arthur W. Corbitt..... | Annapolis. |
| Noel..... | Isaac O'Brien, et al..... | Windsor. |
| Mahone Bay..... | S. Corkum..... | Lunenburg. |
| Lunenburg..... | Gaspar Oxner..... | Halifax. |
| Lunenburg..... | J. M. Watson, et al..... | Lunenburg. |
| Kempt..... | Francis G. Parker, et al..... | Windsor. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------------|---------------------|---------------|-------|----------------|
| Ruby | 36084 | Schooner | 18 | 1846 |
| Rural Maid ..... | ..... | Schooner | 11 | 1852 |
| Ruth ..... | ..... | Schooner | 19 | 1847 |
| R. Wylde | 48044 | Brig | 276 | 1864 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|-----------------------------------|-------------------|
| Jordan River. . . | Thomas L. Banks | Halifax. |
| Hopewell | John Bennett | Digby. |
| Canso | J. McKeagney | Liverpool, N. S. |
| Bear River | Zebediah Crosscup, et al. | Annapolis. |

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| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------------|------------------|----------------|-------|-------------|
| Sable | | Schooner | 45 | 1831 |
| Safe Guide | 37519 | Schooner | 28 | 1858 |
| Saint Laurence | 35836 | Schooner | 54 | 1839 |
| Saint Patrick ..... | ..... | Schooner | 38 | 1846 |
| Saint Peter | 42310 | Schooner | 108 | 1862 |
| Saint Mary | 36523 | Schooner | 25 | 1861 |
| Saladin | 36614 | Schooner | 123 | 1860 |
| Salem | 37410 | Schooner .. | 77 | 1849 |
| Salem ..... | ..... | Schooner | 16 | 1844 |
| Sally and Nancy ..... | ..... | Schooner | 41 | 1830 |
| Sally ..... | ..... | Schooner | 12 | 1818 |
| Sally ..... | ..... | Schooner | 34 | 1823 |
| Sally ..... | ..... | Schooner | 18 | 1834 |
| Salome | 35967 | Schooner | 23 | 1849 |
| Salus | 48137 | Barque | 815 | 1864 |
| Samuel B. Johnston | 38099 | Brigantine ... | 104 | 1859 |
| Samuel Jones | 38356 | Schooner | 46 | 1853 |
| Samuel Noble ..... | ..... | Schooner ... | 16 | 1847 |
| Samuel Thomas | .. . | Schooner | 39 | 1849 |
| Samuel & Thomas | 35981 | Schooner | 14 | 1855 |
| Samuel & Thomas | 35962 | Schooner | 40 | 1849 |
| Samuel Geo. Troop | 42296 | Brigantine ... | 210 | 1861 |
| Samaritan ... .. | ..... | Schooner | 21 | 1842 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------------|------------------------------------|-------------------|
| Shelburne | Robert Kennedy, et al. | Halifax. |
| Jordan River | Donald Morrison, et al | Shelburne. |
| Cape Breton | Benjamin Wier, et al | Halifax. |
| Chezzetcook | Henry Myers | Halifax. |
| Strait of Canso | James Hyland | Halifax. |
| Cheticamp | Varsang Boudroit, et al | Port Hawkesbury. |
| Cornwallis | Rufus Burgess | Windsor. |
| Beaver River | S. Smith, et al | Liverpool, N. S. |
| Advocate Harbor | Nicholas Seaman | Halifax. |
| Douglass | Alexander McUsham | Halifax. |
| Petpiswick | Thomas Murphy | Halifax. |
| Lunenburg | John Payne | Halifax. |
| La Have | Michael Shingle, et al | Halifax. |
| Wood Harbor | David Smith, et al | Halifax. |
| Maitland | Thomas E. Terfrey, et al | Halifax. |
| Clementsport | Samuel Killam | Halifax. |
| Essex, U. S | S. Lawrence | Arichat. |
| Torbay | Thomas Richard, et al | Halifax. |
| Lunenburg | John Hiltz, et al | Lunenburg. |
| Ecum Secum | John McCartney | Halifax. |
| Merdenis Point | Henry Shellnutt | Halifax. |
| Londonderry | George McLellan, et al | Halifax. |
| Barrington | Thomas Crowell | Halifax. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|---------------------------|------------------|----------------|-------|-------------|
| San Fernando ..... | ..... | Schooner | 64 | 1845 |
| Sans Pareil ..... | ..... | Schooner | 28 | 1823 |
| Sandwich | 35733 | Schooner | 31 | 1845 |
| Sapphire | 38153 | Schooner | 23 | 1855 |
| Sarah ..... | ..... | Schooner | 25 | 1844 |
| Sarah | 43089 | Schooner | 156 | 1863 |
| Sarah ..... | ..... | Schooner | 56 | 1845 |
| Sarah | 34809 | Schooner | 23 | 1850 |
| Sarah | 37222 | Schooner ... | 24 | 1855 |
| Sarah | 41913 | Schooner | 38 | 1824 |
| Sarah ..... | ..... | Schooner | 33 | 1828 |
| Sarah ..... | ..... | Schooner | 40 | 1828 |
| Sarah ..... | ..... | Schooner | 80 | 1835 |
| Sarah | 35964 | Schooner | 41 | 1836 |
| Sarah ..... | ..... | Schooner | 30 | 1834 |
| Sarah ..... | ..... | Schooner | 44 | 1840 |
| Sarah | 35961 | Schooner | 42 | 1853 |
| Sarah | 35994 | Schooner | 38 | 1853 |
| Sarah | 35794 | Schooner | 75 | 1852 |
| Sarah | 36300 | Schooner ... | 46 | 1860 |
| Sarah | 37967 | Brigantine ... | 96 | 1855 |
| Sarah Ann ..... | ..... | Schooner | 48 | 1832 |
| Sarah Ann ..... | ..... | Schooner | 33 | 1828 |
| Sarah Ann ..... | ..... | Schooner | 14 | 1839 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|---------------------------------------|-------------------|
| Locke's Island | Jacob Locke, et al. | Halifax. |
| Indian Harbor | John F. Muncey | Halifax. |
| Mahone Bay | William B. Cobb | Halifax. |
| Clare | N. Lewis, et al. | Yarmouth. |
| Tatamagouche | Alexander Ross | Pictou. |
| Tracadie | Paul Landry, et al. | Pictou. |
| Granville | Christopher Pickle, et al. | Digby. |
| Clements | William A. Parker, et al. | Digby. |
| Westport | Samuel Teed | Digby. |
| Essex, U. S. | S. Purney | Liverpool, N. S. |
| Pictou | William Mortimer | Halifax. |
| Yarmouth | Ezekiel Ellis | Halifax. |
| Ragged Island | Samuel Locke | Halifax. |
| Barrington | Benjamin Perry | Halifax. |
| Plymouth | William Frank Potter, et al | Halifax. |
| Shubenacadie | John Woodworth, 2nd. | Halifax. |
| Indian Harbor | Thomas Laidlaw | Halifax. |
| Mahone Bay | John Duffus, et al | Halifax. |
| Sheet Harbor | John Hackett, et al | Halifax. |
| Sheet Harbor | Philip Townsend | Halifax. |
| Argyle | Benjamin W. Salter, et al. | Halifax. |
| New Brunswick . . . | Stephen Lake, et al. | Windsor. |
| Lunenburg | Michael Lonas, et al | Halifax. |
| Lunenburg | Jacob Peck | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------------|------------------|----------------|-------|-------------|
| Sarah Ann ..... | ..... | Schooner | 70 | 1842 |
| Sarah Ann | 36152 | Schooner | 20 | 1858 |
| Sarah A. Dudman | 49426 | Barque. | 459 | 1864 |
| Sarah Botsford ..... | ..... | Barque. | 297 | 1849 |
| Sarah Crowell | 42339 | Brigantine... | 147 | 1862 |
| Sarah Elizabeth | 38409 | Schooner | 94 | 1860 |
| Sarah Elizabeth ..... | ..... | Schooner | 30 | 1847 |
| Sarah Elizabeth ..... | ..... | Schooner | 30 | 1847 |
| Sarah Elizabeth | 50792 | Schooner | 40 | 1845 |
| Sarah & Elizabeth | 42340 | Schooner | 22 | 1834 |
| Sarah & Ellen ..... | ..... | Schooner | 20 | 1837 |
| Sarah Ellen | 48133 | Brig | 307 | 1864 |
| Sarah Jane | 37800 | Schooner ... | 23 | 1856 |
| Sarah Jane ..... | ..... | Schooner | 34 | 1829 |
| Sarah Johnson | 37232 | Schooner | 78 | 1852 |
| Sarah & Margaret | 35690 | Schooner | 23 | 1843 |
| Sarah M. Newhall | 42004 | Brigantine ... | 133 | 1858 |
| Sardonye .. | 48256 | Schooner .. | 67 | 1864 |
| Sax Gotha | 38889 | Schooner | 52 | 1852 |
| S. B. Johnnton | 38099 | Brigantine ... | 104 | 1859 |
| Sceptre | 35754 | Schooner | 32 | 1846 |
| Scottish Chief | 36514 | Schooner | 54 | 1860 |
| Scott ..... | ..... | Brigantine... | 126 | 1836 |
| S. C. Tupper | 51775 | Brigantine ... | 245 | 1865 |

| Where built. | Registered Owner. | Port of Registry |
|---------------------------|--------------------------------------|------------------|
| Wilmot | John Roop, Junr., et al. | Halifax. |
| New Dublin | Frederick Dorrack, et al. | Halifax. |
| Yarmouth | William K. Duchnan, et al | Yarmouth. |
| Sackville | George McKenzie | Pictou. |
| Barrington | Thomas Killam | Yarmouth. |
| Little Arichat | Benjamin Boudroit | Arichat. |
| Strait of Canso | William Owen Heffernan | Guysborough. |
| Gut of Canso | Alexander McGuire, et al. | Halifax. |
| Massachusetts | John B. Phillips, et al | Halifax. |
| Lunenburg | William E. Brine | Halifax. |
| Mahone Bay | George Eisanhauer | Halifax. |
| Londonderry | Robert Faulkner, et al | Halifax. |
| Tatamagouche | Hugh Hingley, et al | Pictou. |
| Guysborough | George Bears, Senr., et al | Halifax. |
| Hillsburgh | Joseph Babine, et al. | Digby. |
| Chester | Richard Richardson | Halifax. |
| St. Mary's Bay | Elisha Payson, et al. | Digby. |
| Barrington | Isaac Nickerson, et al | Shelburne. |
| P. E. Island | James Forrestall, et al. | Halifax. |
| Clements | Samuel Killam | Yarmouth. |
| La Have | Jacob Errald | Lunenburg. |
| Flat Rocks | D. McGorran, et al. | Liverpool, N. S. |
| Yarmouth | Ebenezer Scott, et al. | Halifax. |
| Liverpool | S. C. Tupper | Liverpool, N. S. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|---------------------|------------------|----------------|-------|-------------|
| Scioto | 49427 | Barque..... | 696 | 1864 |
| S. D. Ryerson | 38035 | Barque..... | 588 | 1857 |
| Sea Bird | 38383 | Schooner | 20 | 1849 |
| Sea Bird | ... | Brigantine ... | 110 | 1851 |
| Sea Bird ..... | ..... | Schooner | 124 | 1852 |
| Sea Bird | 37617 | Schooner | 22 | 1857 |
| Sea Bird | 37460 | Schooner | 61 | 1853 |
| Sea Breeze.. | 38583 | Schooner | 20 | 1852 |
| Sea Flower | 38297 | Schooner ... | 35 | 1837 |
| Sea Flower..... | ... | Schooner | 13 | 1825 |
| Sea Flower..... | ..... | Schooner | 37 | 1838 |
| Sea Flower..... | ..... | Schooner | 14 | 1840 |
| Sea Flower.... | ..... | Schooner ... | 19 | 1841 |
| Sea Flower..... | 53603 | Schooner | 25 | 1865 |
| Sea Foam .. ..... | ..... | Schooner | 53 | 1852 |
| Sea Foam | 42784 | Schooner | 45 | 1852 |
| Sea Lark | 37858 | Brigantine.... | 120 | 1855 |
| Sea Lark | 36571 | Schooner | 20 | 1859 |
| Sea Lilly | 41784 | Schooner | 26 | 1859 |
| Sea Lilly | 36985 | Schooner ... | 49 | 1860 |
| Sea Nymph | 51778 | Schooner | 43 | 1865 |
| Sea Slipper | 37612 | Schooner | 40 | 1858 |
| Sea Slipper | 36166 | Schooner | 54 | 1858 |
| Sea Serpent..... | 35712 | Schooner | 41 | 1855 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|----------------------------------|-------------------|
| Saulnierville | Abel C. Robbins, et al | Yarmouth. |
| Yarmouth | J. K. Ryerson, et al | Yarmouth. |
| Antigonish | Murdoch Bossassmac | Arichat. |
| Five Islands | William Ives | Pictou. |
| Antigonish | Roderick McDonald | Pictou. |
| La Have | George Conrod | Lunenburg. |
| Liverpool | M. Slattery | Liverpool. |
| United States | David Hinkley | Sydney, C. B. |
| Arichat | Annie Hubert | Arichat. |
| Yarmouth | Peter Sinat, et al | Halifax. |
| Canso | Abraham Whitman | Halifax. |
| Parrsborough | James Henry Pratt | Halifax. |
| Tracadie | Joseph Jerrior | Halifax. |
| Margaree | Dominick Doucet | Port Hawkesbury. |
| Sable River | L. P. Churchill, et al | Liverpool, N. S. |
| Sable River | Asa D. Crowell, et al | Shelburne. |
| Cornwallis | Hugh Brady, et al | Windsor. |
| Country Harbor | Solomon Fraser, et al | Halifax. |
| Lunenburg | C. Lordley, et al | Lunenburg. |
| Mahone Bay | John Kaulback, et al | Lunenburg. |
| Liverpool | S. Delisser, et al | Liverpool. |
| Mahone Bay | W. N. Zwicker | Lunenburg. |
| Shelburne | Joseph Reyno, et al | Halifax. |
| La Have | Henry Ross, et al | Lunenburg. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------------------|------------------|---------------|-------|-------------|
| Sea Star | 37754 | Schooner | 72 | 1850 |
| Sea Star | 42301 | Schooner | 21 | 1862 |
| Sea View | 36349 | Schooner | 87 | 1861 |
| Seaman ..... | ..... | Schooner | 67 | 1848 |
| Seal ..... | ..... | Schooner | 39 | 1834 |
| Sebastopol | 38394 | Brigantine... | 170 | 1859 |
| Seraph | 36235 | Schooner | 31 | 1859 |
| Serene | 34853 | Schooner | 14 | 1850 |
| Serenade | 35299 | Schooner | 30 | 1848 |
| Serocco | 46472 | Schooner | 56 | 1864 |
| Seven Sons ..... | ..... | Schooner | 53 | 1833 |
| Seymour ..... | ..... | Schooner | 13 | 1837 |
| Shade | 42518 | Schooner | 10 | 1864 |
| Shannon | 38312 | Schooner | 57 | 1842 |
| Shannon ..... | ..... | Schooner | 47 | 1829 |
| Shannon ..... | ..... | Schooner | 10 | 1844 |
| Shannon ..... | ..... | Schooner ... | 8 | 1845 |
| Shannon | 35932 | Schooner | 23 | 1851 |
| Shamrock ..... | ..... | Schooner | 12 | 1804 |
| Shamrock | 42391 | Schooner | 14 | 1846 |
| Sharon | 42392 | Brigantine... | 271 | 1863 |
| Sheffield | 48460 | Barque | 329 | 1864 |
| Shelburne ..... | ..... | Schooner | 56 | 1824 |
| Shelburne Packet ..... | ..... | Schooner | 33 | 1820 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|--------------------------------|-------------------|
| Tracadie | Charles Boudroit | Guysborough. |
| Torbay | Joseph Forgee | Halifax. |
| Sheet Harbor | Meriac & Martin | Liverpool, N. S. |
| New Brunswick ... | Thomas Ayer | Halifax. |
| Magdalen Islands . | Edward Sharman | Halifax. |
| Little Arichat | Charles Boudroit, et al . . | Halifax. |
| Country Harbor ... | Alexander Leary, et al. | Halifax. |
| Parrsborough | David Willager | Parrsborough. |
| Cornwallis | William Rising | Halifax. |
| Lunenburg | William Morash, et al. . . | Lunenburg. |
| Truro | Robert Nelson, et al | Halifax. |
| Economy | William E. Wadman | Halifax. |
| Chester | Hiram Walker | Lunenburg. |
| Little Arichat | P. King | Arichat. |
| Petit | William Rolf, et al. | Halifax. |
| Porter's Lake | Robert A. Innes | Halifax. |
| Londonderry | Joseph Geddes | Halifax. |
| Bedford Basin ... | John Esson | Halifax. |
| Liverpool | Judah Kendrick | Halifax. |
| La Have | Edward J. Stayner, et al. | Halifax. |
| Truro | John B. Dickie, et al. | Halifax. |
| Cornwallis | Stephen Sheffield | Windsor. |
| Green Harbor | Colin Campbell | Halifax. |
| Shelburne | William Hudson, et al. | Halifax. |

| Vessel's Name. | Official Number. | Rfg. | Tons. | When built. |
|---------------------------------|------------------|---------------|-------|-------------|
| Sherbrooke | 50786 | Schooner | 71 | 1865 |
| Shooting Star | 36454 | Schooner | 55 | 1860 |
| Shooting Star | 36521 | Schooner ... | 36 | 1860 |
| Shooting Star | 37510 | Schooner | 61 | 1854 |
| Siberia..... | ..... | Schooner | 57 | 1842 |
| Sidonia | 37668 | Schooner | 74 | 1849 |
| Silver Arrow | 46474 | Schooner | 38 | 1864 |
| Silver Dart | 41787 | Schooner | 30 | 1859 |
| Silver Star | 49479 | Schooner | 149 | 1864 |
| Sims..... | ..... | Schooner | 42 | 1839 |
| Sir Colin Campbell | 36102 | Schooner | 43 | 1839 |
| Sir Charles Ogle.... | ..... | Schooner ... | 10 | 1842 |
| Sir Howard Douglas .... | ..... | Schooner | 80 | 1824 |
| Sir John Moore | 46910 | Schooner | 59 | 1865 |
| Sir Peregrine ..... | ..... | Schooner | 46 | 1832 |
| Sir Rupert..... | ..... | Schooner | 20 | 1840 |
| Sir Rupert D. George ... | 36183 | Schooner | 27 | 1848 |
| Sirene | 42211 | Schooner | 64 | 1847 |
| Sisters | 38624 | Schooner | 39 | 1861 |
| Sisters ..... | ..... | Schooner | 21 | 1839 |
| Sisters ..... | .... | Schooner | 13 | 1840 |
| Sisters | 36169 | Schooner | 32 | 1858 |
| Sissiboo | 46657 | Schooner . . | 125 | 1863 |
| Sissiboo | 49482 | Barque..... | 540 | 1864 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|-------------------------------------|-------------------|
| Sherbrooke | William Mnrdoch, et al. | Halifax. |
| Shelburne | James Muir, et al. | Shelburne. |
| Strait of Canso . . . | J. & W. LeVesconte. | Arichat. |
| Port Medway | John Peters, et al. | Lunenburg. |
| Sheet Harbor | John Bollong, Senr., et al. | Halifax. |
| Parrsborough | Charles Tupper Taylor. | Windsor. |
| Mahone Bay | Frederick Ernst, et al. | Lunenburg. |
| La Have | George Gerhart, et al. | Lunenburg. |
| Digby | Churchill & Taylor, et al. | Digby. |
| Granville | John Johnson, et al. | Halifax. |
| Hingham, Mass. . . . | Silvanus Morton. | Port Hawkesbury. |
| Truro | Samuel Soley. | Halifax. |
| New Brunswick . . . | John Allison, et al. | Halifax. |
| Harbor au Bouche. . | John Moore | Port Hawkesbury. |
| La Have | Daniel Gorman, et al. | Halifax. |
| Musquodoboit | Charles Young. | Halifax. |
| Sheet Harbor | William Carlisle. | Halifax. |
| Ragged Island | C. Boucher. | Liverpool, N. S. |
| Round Island, C.B. . | Clement H. Harrington. | Sydney, C. B. |
| Westport | John Peters. | Halifax. |
| Barrington | Collins Newall. | Halifax. |
| Bay of Islands | John Cluett. | Halifax. |
| Clare | Jevite LeBlanc, et al. | Digby. |
| Yarmouth | William Hankinson. | Digby. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------|------------------|----------------|-------|-------------|
| Six Brothers | 37580 | Schooner | 30 | 1831 |
| S. I. Sanderson | 48072 | Brig | 312 | 1863 |
| S. K. Gale | 38648 | Schooner | 111 | 1863 |
| Skylark | 36472 | Schooner | 27 | 1859 |
| S. L. Tilley | 35068 | Ship | 785 | 1856 |
| S. Morton | 46304 | Barque | 417 | 1864 |
| Smith Eldridge | 38386 | Schooner | 73 | 1859 |
| Snowbird ..... | ..... | Schooner | 40 | 1833 |
| Snowdrop | 37523 | Schooner | 30 | 1858 |
| Snowsquall .. | 38353 | Schooner | 46 | 1857 |
| Solferino | 42024 | Schooner | 103 | 1860 |
| Somerville | 49444 | Barque | 333 | 1865 |
| Sons | 34843 | Schooner | 17 | 1850 |
| Sophia | 38387 | Schooner | 19 | 1859 |
| Sophia | 37003 | Brigantine ... | 98 | 1860 |
| Sophia | 35706 | Schooner | 61 | 1854 |
| Sophia | 37560 | Schooner | 16 | 1846 |
| Sophia | 38247 | Schooner | 110 | 1863 |
| Sophia ..... | ..... | Schooner | 19 | 1838 |
| Sophia ..... | ..... | Schooner | 27 | 1816 |
| Sophia ..... | ..... | Schooner | 46 | 1810 |
| Sophia ..... | ..... | Schooner | 40 | 1817 |
| Sophia ..... | ..... | Schooner | 25 | 1828 |
| Sophia ..... | ..... | Schooner | 13 | 1833 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|-----------------------------|-------------------|
| La Have | James Hearn..... | Halifax. |
| Clements | Gilbert Sanderson.. | Yarmouth. |
| United States..... | Peter Lantz..... | Yarmouth. |
| La Have | James Parker | Lunenburg. |
| St. John, N. B.... | Thomas Killam, et al | Yarmouth. |
| Liverpool | S. Morton, et al..... | Liverpool, N. S. |
| Little Arichat.... | Frederick Chassam..... | Arichat. |
| Shelburne..... | Joshua Snow | Halifax. |
| Shelburne..... | J. Snow, et al..... | Liverpool, N. S. |
| Cheticamp | Placide Chassan | Arichat. |
| St. Mary's Bay ... | Benjamin Davis..... | Digby. |
| Port Gilbert..... | St. Clair Jones, et al..... | Yarmouth. |
| St. Mary's Bay ... | Thomas Mills..... | Digby. |
| River Bourgeois... | Placide Burgess..... | Arichat. |
| Shelburne..... | Joseph Banks, et al..... | Shelburne. |
| Tracadie..... | William Bigelow, et al..... | Guysborough. |
| La Have | F. Johnas..... | Lunenburg. |
| Yarmouth..... | George Killam..... | Yarmouth. |
| Granville | John Givan | Windsor. |
| River Bourgeois .. | Joseph Bissett | Halifax. |
| La Have | Martin Pentz..... | Halifax. |
| La Have | John Gruser..... | Halifax. |
| Lunenburg | John Lohnas | Halifax. |
| Lunenburg | Thomas Henritey..... | Halifax. |

| Vessel's Name. | Official Number. | Tons. | When built. |
|-------------------------------|------------------|----------------|-------------|
| Sophia ..... | ..... | Schooner | 23 1845 |
| Sophia | 33673 | Schooner | 29 1845 |
| Sophia Catherine ..... | ..... | Schooner | 34 1843 |
| Sophia Catherine ..... | ..... | Schooner | 34 1843 |
| Sophia Elizabeth ..... | ..... | Schooner | 53 1853 |
| South Boston | 37759 | Brigantine ... | 197 1854 |
| Southerner | 37508 | Brigantine... | 76 1857 |
| Southern Cross | 49010 | Schooner | 138 1864 |
| Sovereign ..... | ..... | Schooner | 20 1818 |
| Sparkler | 36532 | Schooner | 51 1863 |
| Spartan | 36025 | Brig | 213 1856 |
| Spanish Main | 37433 | Brigantine... | 97 1854 |
| Speedy | 37939 | Schooner | 102 1852 |
| Speed | 38625 | Schooner ... | 34 1861 |
| Speed | 37243 | Schooner | 12 1852 |
| Speed | 36248 | Schooner | 29 1859 |
| S. P. Eagle | 37263 | Schooner | 10 1853 |
| Spec | 41901 | Schooner ... | 36 1852 |
| Speculator | 49429 | Ship | 747 1864 |
| Speedwell .. | ..... | Schooner | 48 1803 |
| Speedwell ..... | ..... | Schooner | 38 1814 |
| Speedwell ..... | ..... | Schooner | 23 1839 |
| Speedwell ..... | ..... | Schooner | 34 1834 |
| Speedwell ..... | ..... | Schooner | 42 1849 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|------------------------------------|-------------------|
| Antigonish | James E. Cutler | Halifax. |
| Magdalen Islands.. | Benjamin Wier, et al | Halifax. |
| Lunenburg | D. Day, et al. | Liverpool, N. S. |
| Lunenburg | Leonard Vogler, et al. | Halifax. |
| Strait of Canse . . . | J. G. McKeen | Arichat. |
| New Glasgow | William Elliot | Pictou. |
| Sable River | J. P. Churchill | Liverpool, N. S. |
| Parrsborough | Winfield Y. King, et al. | Parrsborough. |
| Barrington | Seth Coffin, Senr., et al. | Halifax. |
| Strait of Canse . . . | Angus Embree, et al. | Port Hawkesbury. |
| Truro | Mary Gleeson | Halifax. |
| Shelburne | Thomas C. Kinnear, et al | Halifax. |
| Grand Ruisseau . . . | Jeffery LeBlanc | Arichat. |
| Leeche's Creek, C.B | John L. Ingraham | Sydney, C. B. |
| Digby | Charles Risteen | Digby. |
| Barrington | Heman Kenny, Junr., et al. | Halifax. |
| Granville | Benjamin H. Hagarty | Digby. |
| Souris, P. E. I. . . . | Walter Murray, et al. | Port Hawkesbury. |
| Yarmouth | Thomas Killam, et al | Yarmouth. |
| Argyle | Josiah Snow, et al | Halifax. |
| Petite Riviere | William McDaniel | Halifax. |
| Lunenburg | Henry Henritey | Halifax. |
| La Have | William Sorkum | Halifax. |
| Port Medway | Thomas Vogler, et al. | Halifax. |

| Vessel's Name. | Official Number. | Rig | Tons. | When built. |
|--------------------------------|------------------|----------------|-------|-------------|
| Speedwell..... | ..... | Schooner | 37 | 1839 |
| Speedwell..... | ..... | Schooner | 24 | 1847 |
| Speedwell | 36087 | Schooner | 17 | 1849 |
| Spey .. .. . | ..... | Schooner | 15 | 1847 |
| Spey | 41167 | Schooner | 83 | 1857 |
| Spirit of the Day | 49118 | Schooner | 33 | 1865 |
| Spitfire | 42348 | Schooner | 27 | 1843 |
| Sprightly | 35705 | Schooner | 37 | 1851 |
| Spray .. .. . | ..... | Schooner | 18 | 1853 |
| Spray | 41547 | Schooner | 78 | 1858 |
| Spray ..... | ..... | Schooner | 70 | 1832 |
| Spray ..... | ..... | Schooner | 17 | 1853 |
| Spratt | 38396 | Schooner | 16 | 1859 |
| Spring Bird.. .. . | ..... | Schooner | 20 | 1845 |
| Spring Bok | 46395 | Brig | 241 | 1863 |
| Spy | 42782 | Schooner | 15 | 1861 |
| Spy ..... | ..... | Schooner .. | 34 | 1841 |
| Spy ..... | ..... | Schooner | 25 | 1851 |
| Squando | 36321 | Schooner | 48 | 1860 |
| S. S. Nelson | 48145 | Schooner | 132 | 1864 |
| St. Anne | 41979 | Schooner | 104 | 1859 |
| St. Dominique | 38444 | Barque..... | 378 | 1863 |
| St. Mary's | 46664 | Schooner | 20 | 1853 |
| St. Mary's | 49476 | Brigantine ... | 164 | 1864 |

| Where built. | Registered Owner | Port of Registry. |
|--------------------------|--------------------------------------|-------------------|
| La Have | John H. McNab | Halifax. |
| Pope's Harbor | Edward Middlemas | Halifax. |
| Tangier | John S. McLean, et al | Halifax. |
| Petpiswick | Isaac Naufls | Halifax. |
| New Glasgow | Benjamin Wier, et al | Halifax. |
| Merigomish | Thomas Patton, et al | Pictou. |
| Barrington | Freeman Atwood, et al | Halifax. |
| La Have | Eli Mader, et al | Lunenburg. |
| Ragged Island | J. Lloyd | Liverpool, N. S. |
| Cornwallis | Silvanus Whitney | Windsor |
| Economy | Edwin Tomlinson | Halifax. |
| Ragged Island | Henry Darney | Halifax. |
| Cape Breton | William Munroe | Halifax. |
| Shelburne | Judah Nickerson, et al | Halifax. |
| Windsor | P. M. Cunningham, et al | Windsor. |
| Shelburne | Alexander McNaughton | Shelburne. |
| Granville | William Payson | Digby. |
| Barrington | Moses Crowell, et al | Halifax. |
| Isaac's Harbor | Reuben I. Hart, et al | Halifax. |
| Truro | John Alexander, et al | Halifax. |
| Canada East | W. Hutchin, et al | Arichat. |
| River John | Dominique Givroir | Arichat. |
| United States | Joseph R. Morehouse, et al | Digby. |
| Sandy Cove | Jones Morehouse, et al | Digby. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built |
|------------------------------|------------------|----------------|-------|------------|
| St. Peter | 38403 | Brigantine ... | 164 | 1860 |
| St. Vincent | 37383 | Brigantine ... | 162 | 1856 |
| Star | 41416 | Brigantine ... | 150 | 1857 |
| Star | 41808 | Schooner .. | 23 | 1860 |
| Star | 37378 | Schooner | 24 | 1854 |
| Star ..... | ..... | Schooner | 20 | 1818 |
| Star ..... | ..... | Schooner | 56 | 1821 |
| Star ..... | ..... | Schooner | 32 | 1835 |
| Star ..... | ..... | Schooner | 18 | 1840 |
| Star Castle | 42317 | Brigantine... | 114 | 1862 |
| Starlight | 37518 | Schooner | 71 | 1858 |
| Starlight | 42103 | Brigantine ... | 151 | 1860 |
| Star of the Sea | 46011 | Schooner ... | 65 | 1863 |
| Startled Fawn | 46086 | Brigantine ... | 114 | 1862 |
| Standard | 37859 | Brigantine ... | 112 | 1855 |
| Stanley | 36105 | Brigantine ... | 132 | 1857 |
| Staffa | 43093 | Barque. | 378 | 1863 |
| Statira | 42386 | Barque. | 327 | 1854 |
| Stella | 38243 | Barque. | 484 | 1863 |
| Stephen Johnson | 49435 | Schooner | 48 | 1865 |
| Stephen McKeane | 46480 | Schooner | 73 | 1864 |
| Sterling | 42393 | Schooner ... | 79 | 1845 |
| Sterling | ... | Brig | 161 | 1859 |
| Sterling | 31424 | Schooner | 54 | 1847 |

| Where built. | Registered Owner. | Port of Registry. |
|-------------------------|---|-------------------|
| Arichat | Peter Grines. | Arichat. |
| Tracadie | Simon Bobin, et al | Arichat. |
| Merigomish | D. LeBlanc, et al | Arichat. |
| Clements | Cerino Purdy, et al. | Annapolis. |
| Little River | Donald McGregor. | Guysborough. |
| St. John, N. B. | Nehemiah Merritt, et al | Halifax. |
| New Brunswick | William Pearsons | Halifax. |
| Granville | James Smith, Junr., et al | Halifax. |
| Clyde River | William Greenwood, et al. | Halifax. |
| Five Islands | Andrew Thomson | Halifax. |
| Liverpool | Freeman Johnstone | Liverpool, N. S. |
| Newport | Nicholas Mosher, Junr., et al | Windsor. |
| Little River | Charles O. Regan | Parrsborough. |
| Shelburne | William Muir, et al. | Shelburne. |
| Cornwallis | Daniel Huntley | Windsor. |
| Shelburne | Thomas C. Kinnear, et al. | Halifax. |
| River John | Dominique Givroir | Arichat. |
| United States | Charles Hall | Halifax. |
| Yarmouth | W. K. Dudman | Yarmouth. |
| Plymouth | Stephen Johnston | Yarmouth. |
| La Have | James A. McKeane | Lunenburg. |
| Cornwallis | Stephen Stoddart, et al | Halifax. |
| Yarmouth | Reuben Clements, et al. | Halifax. |
| Tatamagouche | Charles Landry, et al | Halifax. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|--------------------------|------------------|----------------|-------|-------------|
| Steward | 35005 | Schooner | 60 | 1842 |
| Storm Cloud | 36509 | Schooner | 43 | 1860 |
| Storm Nymph | 41920 | Brigantine.... | 169 | 1858 |
| Stranger | 48269 | Brigantine ... | 249 | 1864 |
| Stranger | 37774 | Schooner | 86 | 1853 |
| Stranger ..... | ..... | Schooner | 20 | 1829 |
| Stranger | 42369 | Brigantine ... | 197 | 1863 |
| Streamlet ..... | .... | Schooner | 51 | 1847 |
| Stromness | 37670 | Brigantine.... | 135 | 1860 |
| Sturgeon | 36493 | Schooner | 32 | 1860 |
| Sultan | 35837 | Schooner | 80 | 1854 |
| Summer | 46393 | Ship | 1019 | 1863 |
| Sunbeam | 38597 | Schooner | 28 | 1858 |
| Superb | 37744 | Brigantine.... | 131 | 1847 |
| Superb ..... | ..... | Schooner | 22 | 1829 |
| Superb | 35644 | Schooner | 54 | 1942 |
| Superior ..... | ..... | Schooner | 18 | 1825 |
| Superior ..... | ..... | Schooner | 46 | 1828 |
| Superior ..... | ..... | Schooner | 31 | 1838 |
| Superior | 36003 | Schooner | 44 | 1852 |
| Superior ..... | ..... | Schooner | 15 | 1841 |
| Superior ..... | ..... | Schooner | 15 | 1841 |
| Superior | 42516 | Schooner | 120 | 1863 |
| Surprise ..... | ... . | Schooner | 45 | 1943 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|-----------------------------------|-------------------|
| Minudie | Hazen Cohoon | Halifax. |
| Port Medway | S. Foster & Sons | Liverpool, N. S. |
| Liverpool | S. Freeman & Sons | Liverpool, N. S. |
| Shelburne | H. E. Vaun, et al | Shelburne. |
| Granville | William Miller, et al | Digby. |
| Shelburne | Peter Spearwater, et al | Halifax. |
| Maitland | Joseph Montieth | Halifax. |
| Shelburne | Thomas Joseph Crowell | Halifax. |
| Parrsborough | Jeremiah Kennedy | Parrsborough. |
| Lunenburg | Elkanah Koch, et al | Lunenburg. |
| St. Mary's | John Day | Halifax. |
| Newport | Frederick Curry, et al | Windsor. |
| Mira | Clement H. Harrington | Sydney, C. B. |
| P. E. Island | Francis Westaway | Arischat. |
| Clare | Charles E. Rachford | Halifax. |
| Sheet Harbor | Chapman Swain, et al | Halifax. |
| Barrington | R. D. Oaks | Lunenburg. |
| Douglass | David Faulkner, et al | Halifax. |
| Beaver Harbor | Robert Morrison | Halifax. |
| La Have | Thomas Smith, et al | Halifax. |
| Sable River | James Palmer | Halifax. |
| Sable River | Philip Bayers | Halifax. |
| La Have | Edward Mulloch, et al | Lunenburg. |
| Louisburg | Solomon Tully, et al | Sydney, C. B. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|------------------|----------------|-------|-------------|
| Surprise ..... | ..... | Schooner | 33 | 1820 |
| Surprise | 42274 | Brigantine ... | 204 | 1861 |
| Susan .. | 39877 | Brigantine.... | 102 | 1844 |
| Susan ..... | ..... | Schooner | 37 | 1843 |
| Susan ..... | ..... | Schooner | 15 | 1852 |
| Susan ..... | ..... | Schooner | 25 | 1851 |
| Susan ..... | ..... | Schooner | 43 | 1832 |
| Susan ..... | ..... | Schooner ... | 46 | 1839 |
| Susan | 38957 | Schooner | 25 | 1819 |
| Susan ..... | ..... | Schooner | 16 | 1844 |
| Susan ..... | ..... | Schooner | 16 | 1845 |
| Susan ..... | ..... | Schooner | 16 | 1849 |
| Susan | 36191 | Schooner | 23 | 1853 |
| Susan | 35619 | Schooner | 74 | 1855 |
| Susan | 36241 | Schooner | 34 | 1859 |
| Susan | 49103 | Brigantine.... | 134 | 1864 |
| Susan Ann | 36298 | Schooner | 17 | 1857 |
| Susan L. Campbell | 46653 | Barque | 599 | 1863 |
| Susan M. Dudman | 38221 | Barque | 745 | 1862 |
| Susannah | ... | Schooner | 17 | 1853 |
| Susannah ..... | ..... | Schooner | 11 | 1846 |
| Susannah ..... | ..... | Schooner | 13 | 1849 |
| Susannah ..... | ..... | Schooner | 68 | 1852 |
| Susannah ... | 37217 | Schooner | 62 | 1845 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------------|-----------------------------------|-------------------|
| Argyle | James McMullen | Halifax. |
| Londonderry | John Wier, et al | Halifax. |
| Shelburne | William Full | Halifax. |
| Cheticamp | John Chassam | Arichat. |
| River Inhabitants | Francis Grant | Arichat. |
| Rocky Bay | J. Carmichael | Arichat. |
| Cornwallis | Samuel Cox, et al | Halifax. |
| Sheet Harbor | Patrick Quillman, et al | Halifax. |
| Cape Breton | Richard Hawes | Halifax. |
| Jeddore | Joseph Dukes | Halifax. |
| Bay of Islands | John Whitman | Halifax. |
| Chezzetcook | George Ferguson | Halifax. |
| Clam Harbor | Edward Perry, et al | Halifax. |
| Sheet Harbor | Daniel Lang | Halifax. |
| Newdiquoddy | John Poole | Halifax. |
| Tatamagouche | Charles Reilly | Pictou. |
| Petpiswick | James Young | Halifax. |
| Weymouth | Colin Campbell, Junr | Digby. |
| Tusket | W. K. Dudman, et al | Yarmouth. |
| River Bourgeois | Celestia Cordeon | Arichat. |
| Antigonish | Edward Mudge | Pictou. |
| River John | Edward Spinney, et al | Pictou. |
| Big Bras d'Or, C. B. | Michael Burke | Sydney, C. B. |
| Hillsburgh | Andrew H. Harris, et al | Digby. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------|------------------|---------------|-------|-------------|
| Susannah ..... | ..... | Schooner | 25 | 1821 |
| Susannah ..... | ..... | Schooner | 35 | 1820 |
| Susannah ..... | ..... | Brigantine... | 162 | 1836 |
| Susannah Marie ..... | ..... | Schooner | 37 | 1831 |
| Susie A. | 46903 | Schooner | 45 | 1864 |
| Sutcliffe ..... | ..... | Brig | 206 | 1850 |
| Swallow | 38324 | Schooner | 24 | 1842 |
| Swallow ..... | ..... | Schooner | 15 | 1839 |
| Swallow | 48111 | Schooner | 29 | 1862 |
| Swan | 37621 | Schooner | 46 | 1858 |
| Swan ..... | ..... | Schooner | 18 | 1840 |
| Swan | .. . | Schooner ... | 17 | 1844 |
| Swan | 36117 | Schooner | 19 | 1849 |
| Swan | 36312 | Schooner | 73 | 1860 |
| Swift ..... | ..... | Schooner | 15 | 1843 |
| Swift ..... | ..... | Schooner | 29 | 1794 |
| Swift ..... | ..... | Schooner ... | 19 | 1828 |
| Swift ..... | ..... | Schooner | 22 | 1840 |
| Swift Sure ..... | ..... | Schooner | 37 | 1817 |
| Switchen | 35944 | Schooner | 52 | 1855 |
| Swordfish | 36444 | Schooner | 69 | 1859 |
| Sylph ..... | ..... | Schooner | 51 | 1847 |
| Sylph | 35734 | Schooner | 25 | 1848 |
| Sylphide | 35924 | Schooner | 57 | 1851 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|---------------------------------|-------------------|
| Chedabueto Bay .. | Francis Cook | Halifax. |
| Granville | C. Petit | Halifax. |
| Londonderry | Thomas Corbitt, et al | Halifax. |
| Spring Harbor.... | Gasper Glawson, et al | Halifax. |
| Strait of Canso ... | Daniel Clough, et al | Port Hawkesbury |
| New Brunswick... | Thomas Coffin, Junr., et al ... | Halifax. |
| Poulamond | William LeVesconte... .. | Arichat. |
| Lunenburg | Leonard Jauner | Halifax. |
| Chezzetcook . . . | William Hare | Halifax. |
| La Have | Frederick Publicover | Lunenburg. |
| Barrington | Levi Nickerson, et al | Halifax. |
| Jeddore | John Peter Webber | Halifax. |
| Green Harbor.... | John B. Fay | Halifax. |
| Jeddore | William Pitts | Halifax. |
| L'Ardoise | Juste Monbugust, et al | Arichat. |
| Lunenburg | John Godkin | Halifax. |
| La Have | Philip Fraser | Halifax. |
| P. E. Island | Henry Yeomans | Halifax. |
| Lunenburg | Michael Salsman, et al | Halifax. |
| Port Medway | Thomas Smith, et al | Halifax. |
| Shelburne | James Muir | Shelburne. |
| Guysborough | Edward J. Cunningham | Halifax. |
| La Have | William Beverley | Halifax. |
| Shelburne | Andrew McNab | Halifax. |

| Vessel's Name. | Official Number. | Rig | Tons. | When built. |
|----------------------|------------------|----------------|-------|-------------|
| Sylvia | 37581 | Schooner | 52 | 1855 |
| Syntax | 41923 | Schooner | 19 | 1859 |
| Syria ..... | ..... | Schooner | 139 | 1843 |
| Sussex | 48464 | Brigantine.... | 249 | 1864 |

| Where built. | Registered Owner | Port of Registry. |
|------------------|-----------------------------|-------------------|
| Lunenburg..... | Benjamin Wier | Lunenburg. |
| Port Medway..... | J. Picard | Arichat. |
| New Brunswick... | Robert Masters, et al. | Halifax. |
| Horton | John B. North, et al | Windsor. |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|----------------|---------------------|------|-------|----------------|
| | | | | |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
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| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Talent | 46397 | Schooner | 68 | 1863 |
| Tally Ho ..... | ..... | Schooner | 22 | 1846 |
| Teaser | 34992 | Schooner | 14 | 1851 |
| Teaser ..... | ..... | Schooner | 15 | 1847 |
| Teaser ..... | ..... | Schooner | 27 | 1837 |
| Teaser ..... | ..... | Schooner | 18 | 1837 |
| Teaser | 42320 | Schooner | 31 | 1854 |
| Tecumseh | 46291 | Barque..... | 635 | 1863 |
| Telegraph | 34736 | Schooner | 34 | 1850 |
| Telegraph..... | ..... | Brigantine ... | 106 | 1850 |
| Telegraph | 42119 | Schooner ... | 149 | 1861 |
| Telegraph | 35649 | Schooner | 59 | 1852 |
| Temperance | 38461 | Schooner | 18 | 1842 |
| Temperance | 38416 | Schooner | 131 | 1861 |
| Temperance ..... | ..... | Schooner | 48 | 1831 |
| Temperance ..... | ..... | Schooner | 47 | 1833 |
| Temperance Banner ... | 37863 | Schooner | 42 | 1853 |
| Thames | 42204 | Schooner | 25 | 1861 |
| Thebes ..... | ..... | Schooner | 49 | 1843 |
| Thetis | 42322 | Schooner | 78 | 1862 |
| Thetis | 49113 | Brigantine... | 265 | 1865 |
| Thistle | 38345 | Brigantine ... | 185 | 1856 |
| Thistle | 36441 | Brigantine ... | 83 | 1859 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|-------------------------------------|-------------------|
| Parrsborough. | Walter R. Meriam, et al. | Halifax. |
| Margarce, C. B. . . . | John B. Fay | Halifax. |
| Newport. | Samuel Dyer Card | Windsor |
| Cape Negro. | Thomas Fahey | Halifax. |
| Barrington | Moses Ross, et al. | Halifax. |
| Lunenburg. | Thomas Morish, et al | Halifax. |
| Maine, U. S. | Thomas Brackett, et al | Halifax. |
| Liverpool | L. A. Sponagle, et al | Liverpool, N. S. |
| Granville | James Campbell | Digby. |
| Shelburne. | J. Doma. | Liverpool, N. S. |
| Parrsborough. | Charles R. Northup, et al. | Windsor. |
| St. Mary's | Andrew McNab | Halifax. |
| River Bourgeois . . | J. & W. LeVesconte | Arichat. |
| Grand Ruissena. . . | Constant Fouchong | Arichat. |
| Sheet Harbor. . . . | James McPhee, et al | Halifax. |
| Kempt | Walter Basto | Halifax. |
| Cornwallis | George Kirkpatrick | Windsor. |
| Liverpool | J. McLeod. | Liverpool, N. S. |
| Granville | Israel Letteney, et al. | Halifax. |
| Marie Joseph. | John Poole | Halifax. |
| Merigomish | Robert S. Copeland, et al | Pictou. |
| Little Arichat. | Peter Crichton, et al. | Halifax. |
| Shelburne. | Robert McKay, et al. | Shelburne. |

| Vessel's Name. | Official Number. | Reg. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Thistle | 37001 | Schooner | 37 | 1860 |
| Thistle ..... | ..... | Schooner | 40 | 1828 |
| Thistle | 42372 | Schooner | 49 | 1854 |
| Thistle ..... | ..... | Schooner | 38 | 1847 |
| Thomas ..... | ..... | Schooner | 32 | 1819 |
| Thomas | 50765 | Brigantine.... | 187 | 1864 |
| Thomas Albert | 42794 | Brigantine ... | 112 | 1862 |
| Thomas Bagley | 37502 | Schooner | 60 | 1857 |
| Thomas Dickson | 37874 | Schooner | 79 | 1849 |
| Thomas Killam | 34787 | Barque..... | 569 | 1855 |
| Thomas Whitney | 29390 | Barque..... | 280 | 1850 |
| Three Brothers | 38333 | Schooner | 74 | 1848 |
| Three Brothers | 38429 | Schooner | 28 | 1844 |
| Three Brothers | 37650 | Schooner | 52 | 1858 |
| Three Brothers | 41909 | Schooner | 24 | 1860 |
| Three Brothers ..... | ..... | Schooner | 18 | 1814 |
| Three Brothers ..... | ..... | Schooner | 28 | 1827 |
| Three Brothers ..... | ..... | Schooner | 44 | 1831 |
| Three Brothers | .. . | Schooner | 20 | 1836 |
| Three Brothers ..... | .... | Schooner | 23 | 1837 |
| Three Brothers ..... | ..... | Schooner | 25 | 1840 |
| Three Brothers ..... | ..... | Schooner | 12 | 1846 |
| Three Brothers | 33412 | Schooner | 55 | 1851 |
| Three Brothers | 36305 | Schooner | 27 | 1860 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|--------------------------------|-------------------|
| Shelburne..... | James Muir..... | Shelburne. |
| Port Medway..... | William Foster, et al..... | Halifax. |
| Essex, U. S..... | Alfred Larder..... | Halifax. |
| Wilmot. | George Rumman, et al.... | Halifax. |
| Port Medway.. | James McDonald, et al..... | Halifax. |
| Five Islands..... | Thomas R. Morrison, et al..... | Halifax. |
| Barrington..... | Thomas W. Wilson, et al..... | Shelburne. |
| Port Medway..... | B. & D. McConnell..... | Liverpool, N. S. |
| Parrsborough..... | John Allison, et al..... | Windsor. |
| Yarmouth.... | J. K. Ryerson, et al..... | Yarmouth. |
| United States..... | J. C. Kelly..... | Yarmouth. |
| Little Arichat.... | Constant Boudroit, et al. | Arichat. |
| River Inhabitants.. | William Hutchins..... | Arichat. |
| Parrsborough..... | David Willager..... | Shelburne. |
| Cheticamp..... | Pollicap Chassau..... | Port Hawkesbury. |
| Cape Breton . . . | Elisher Pride..... | Halifax. |
| Lunenburg..... | Jacob Baker, et al..... | Halifax. |
| P. E. Island..... | George McAndrew..... | Halifax. |
| La Have..... | James Parker..... | Halifax. |
| Mahone Bay..... | George Westhaver..... | Halifax. |
| Torbay... . | John Boudroit..... | Halifax. |
| La Have..... | Leonard Sawhard..... | Halifax. |
| Main-a-Dieu, C. B. | Thomas Gallant, et al..... | Halifax. |
| Cheticamp .. | Moses Porrier..... | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| Three Brothers | 38567 | Schooner | 55 | 1856 |
| Three Brothers | 42508 | Schooner | 14 | 1856 |
| Three Brothers..... | ..... | Schooner | 22 | 1854 |
| Three Sisters..... | ..... | Schooner | 20 | 1844 |
| Three Sisters | 37320 | Schooner | 52 | 1853 |
| Three Sisters..... | ..... | Schooner | 39 | 1820 |
| Three Sisters..... | ..... | Schooner | 61 | 1831 |
| Three Sisters..... | ..... | Schooner | 32 | 1815 |
| Three Sisters..... | ..... | Schooner | 30 | 1827 |
| Three Sisters..... | ..... | Schooner | 16 | 1836 |
| Three Sisters..... | ..... | Schooner | 17 | 1848 |
| Three Sons..... | ..... | Schooner | 11 | 1838 |
| Tiber | 35906 | Schooner | 59 | 1845 |
| Tiber | 53563 | Brig..... | 268 | 1865 |
| Tickler | 38576 | Schooner | 54 | 1856 |
| Tigress | 42107 | Schooner | 162 | 1860 |
| Tigress..... | ..... | Schooner .. | 28 | 1844 |
| Tiger..... | ..... | Schooner | 17 | 1848 |
| Timandra | 49487 | Brigantine ... | 132 | 1865 |
| Time | 37850 | Brigantine... | 130 | 1855 |
| Time..... | ..... | Schooner | 24 | 1847 |
| Tirol..... | ..... | Schooner | 17 | 1848 |
| Token | 36086 | Brigantine ... | 85 | 1857 |
| Tom Sayers | 36311 | Schooner | 126 | 1860 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|--|-------------------|
| Fouchette, C. B. . . . | Isaac Hurst | Halifax. |
| Lunenburg | William Philips | Halifax. |
| Torbay | Robert Noble | Halifax. |
| Rocky Bay | J. Martell | Arichat. |
| New Edinbro' | Stephen Marshall | Digby. |
| Tusket River | David Dill | Halifax. |
| St. Mary's | Thomas Byrons | Halifax. |
| Jeddore | John Baker, et al. | Halifax. |
| Beaver Harbor . . . | John Whitman, et al. | Halifax. |
| La Have | James Muir | Halifax. |
| Jeddore | Dennis McKay | Halifax. |
| Argyle | David Randall, et al | Halifax. |
| Port Medway | Paul Vigneau | Halifax. |
| Clementsport | William Miller, et al | Halifax. |
| North Sydney, C.B. | Thomas D. Archibold | Sydney, C. B. |
| Hantsport | Bennet Smith, et al. | Windsor. |
| Barrington | Gamaliel Kenny | Halifax. |
| Barrington | John M. Sholes, et al. | Halifax. |
| Port Gilbert | J. S. Hawkesworth, et al. | Digby. |
| Maitland | Charles Aubery DeWolf, et al | Windsor. |
| Salmon River | John G. Whitman | Halifax. |
| Barrington | Josiah Swain | Halifax. |
| Barrington | Isaac Smith, et al. | Halifax. |
| Napan | James Fullerton, et al | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------|------------------|----------------|-------|-------------|
| Topaz | 38645 | Schooner | 91 | 1864 |
| Topsy | 53552 | Schooner | 20 | 1865 |
| Topsy | 46663 | Schooner | 88 | 1854 |
| Topsy | 35778 | Schooner | 55 | 1854 |
| Torrid Zone | 52009 | Brigantine.... | 172 | 1865 |
| Tracey Jane | 49115 | Schooner | 110 | 1865 |
| Trackless .. | 46087 | Brigantine.... | 126 | 1862 |
| Traffic..... | ..... | Schooner | 41 | 1863 |
| Transit | 42910 | Schooner | 107 | 1862 |
| Transit .. | 36443 | Schooner | 60 | 1859 |
| Traveller ..... | ..... | Schooner | 28 | 1822 |
| Traveller | . . . | Schooner | 108 | 1822 |
| Traveller .. | 50769 | Schooner | 106 | 1864 |
| Trial | 34771 | Schooner | 47 | 1845 |
| Trial..... | ..... | Schooner | 28 | 1853 |
| Trial..... | ..... | Schooner | 18 | 1853 |
| Trial..... | ..... | Schooner | 29 | 1824 |
| Trial..... | ..... | Schooner | 27 | 1816 |
| Trial..... | ..... | Schooner | 40 | 1818 |
| Trial..... | ..... | Schooner | 44 | 1821 |
| Trial ..... | ..... | Schooner | 18 | 1829 |
| Trial ..... | ..... | Schooner | 41 | 1836 |
| Trial ..... | ..... | Schooner | 9 | 1835 |
| Trial ..... | ..... | Schooner ... | 47 | 1836 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|----------------------------------|-------------------|
| North Sydney, C.B. | John L. Ingraham..... | Halifax. |
| Torbay..... | Simon Pebrine..... | Halifax. |
| Wilmot..... | W. B. Ruggles, Junr., et al..... | Digby. |
| Barrington..... | James Bollong..... | Halifax. |
| Shelburne..... | Samuel Muir, et al..... | Shelburne. |
| Little River..... | Porphyre Petitpas, et al..... | Pictou. |
| Shelburne..... | Henry S. Henry..... | Shelburne. |
| Wilmot..... | James Campbell..... | Annapolis. |
| P. E. Island..... | John George English..... | Pictou. |
| Sable River..... | Jonathan Locke, et al..... | Shelburne. |
| La Have..... | Jacob Henritsy, Junr..... | Halifax. |
| New Brunswick... | James Burgess, et al..... | Halifax. |
| Moser River..... | James A. Moren..... | Halifax. |
| Antigonish..... | Simon Babin, et al..... | Aricbat. |
| Tatamagouche.... | George Millard..... | Pictou. |
| Grand Mire, C. B.. | Timothy Ayles, et al..... | Sydney, C. B |
| Seal Island..... | John Starr, et al..... | Halifax. |
| Argyle..... | Henry Roberts, et al..... | Halifax. |
| Canso..... | Michael Webb..... | Halifax. |
| Oak Bay..... | William H. Roach..... | Halifax. |
| Yarmouth..... | John Bullerwell, et al..... | Halifax. |
| St. Mary's..... | James McDaniel, et al..... | Halifax. |
| Chester..... | William Graves, et al..... | Halifax. |
| P. E. Island..... | Ann Cleaveland..... | Halifax. |

| Vessel's Name. | Official Number. | Rig | Tons. | When built. |
|----------------------------|------------------|----------------|-------|-------------|
| Trial | 35886 | Schooner | 43 | 1855 |
| Trial ..... | ..... | Schooner | 20 | 1837 |
| Trial ..... | ..... | Schooner | 32 | 1825 |
| Trial ..... | ..... | Schooner | 22 | 1841 |
| Trial | 36259 | Brigantine .. | 183 | 1859 |
| Trial | 36272 | Schooner | 14 | 1859 |
| Trial | 36335 | Schooner | 22 | 1860 |
| Trial | 37615 | Schooner | 23 | 1858 |
| Trident ..... | ..... | Schooner | 71 | 1841 |
| Trident | 48038 | Barque..... | 421 | 1864 |
| Triton | 49424 | Brigantine ... | 132 | 1864 |
| Triumph | 37591 | Schooner | 22 | 1857 |
| Triumph | 42109 | Schooner | 77 | 1860 |
| Triumph ..... | ..... | Schooner ... | 26 | 1822 |
| Triumph | 36307 | Brigantine ... | 125 | 1850 |
| Triumph | 35708 | Schooner | 58 | 1851 |
| Trinidad | 41916 | Brigantine ... | 79 | 1858 |
| Tribune ..... | ..... | Schooner | 17 | 1851 |
| Trio | 35941 | Schooner | 79 | 1846 |
| Tropic Bird | 48268 | Brigantine... | 201 | 1864 |
| Tropic Bird | 36981 | Schooner | 46 | 1860 |
| T. R. Patillo | 36515 | Barque..... | 210 | 1830 |
| Trusty | 35858 | Schooner | 53 | 1846 |
| Trusty ..... | ..... | Schooner | 22 | 1826 |

| Where built. | Registered Owner. | Port of Registry. |
|----------------------|--------------------------------------|-------------------|
| Mahone Bay | George Eisener | Halifax. |
| La Have | William Parks | Halifax. |
| New Dublin | William Cohoon, et al | Halifax. |
| Lunenburg | Simon Scott | Halifax. |
| Antigonish | James Forrestall, et al | Halifax. |
| St. Margaret's Bay. | John H. Fader | Halifax. |
| Petpiswick | Joseph Ritecy | Halifax. |
| La Have | Edward Horn, et al | Halifax. |
| Ragged Island . . . | Samuel B. Locke, et al | Halifax. |
| Granville | William D. Pickup, et al | Annapolis. |
| Yarmouth | Joseph B. Stoneman, et al | Yarmouth. |
| Lunenburg | S. & B. Lonas | Lunenburg. |
| Cornwallis | Levi Rand, et al | Windsor |
| New Brunswick . . | Amos Seaman, Junr | Halifax. |
| New Brunswick . . | John Whitman | Halifax. |
| Guysborough | Thomas Byrns, et al | Halifax. |
| Ragged Island . . . | S. Locke, Junr., et al | Liverpool, N. S. |
| Chezzetcook | Dennis Smith | Halifax. |
| Walton | George Densmore | Halifax. |
| Jordan River | Samuel Locke, Junr., et al | Shelburne. |
| Mahone Bay | Peter Young, et al | Lunenburg. |
| Liverpool | T. R. Patillo | Liverpool, N. S. |
| Sheet Harbor | Charles Presscott, et al | Halifax. |
| Cape Negro | David Palmer | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| True Brothers ..... | ..... | Schooner | 32 | 1846 |
| Truro | 36172 | Schooner | 119 | 1858 |
| Tubal Cain | 42793 | Barque | 222 | 1862 |
| Turk ..... | ..... | Schooner | 17 | 1817 |
| Tuscan ..... | ..... | Brigantine ... | 161 | 1837 |
| Tweed | 46383 | Barque | 340 | 1863 |
| Twilight | 38036 | Schooner | 14 | 1866 |
| Twin Sons .. | 38450 | Schooner | 97 | 1863 |
| Twins ..... | ..... | Schooner | 39 | 1816 |
| Two Brothers ..... | ..... | Schooner | 15 | 1828 |
| Two Brothers ..... | ..... | Schooner | 15 | 1820 |
| Two Brothers ..... | ..... | Schooner | 29 | 1821 |
| Two Brothers ..... | ..... | Schooner | 49 | 1825 |
| Two Brothers .. | ..... | Schooner | 15 | 1842 |
| Two Brothers ..... | ..... | Schooner | 25 | 1836 |
| Two Brothers ..... | ..... | Schooner | 31 | 1838 |
| Two Brothers ..... | ..... | Schooner | 28 | 1842 |
| Two Brothers ..... | ..... | Schooner | 16 | 1834 |
| Two Brothers | 34964 | Schooner | 33 | 1845 |
| Two Brothers ..... | ..... | Schooner | 16 | 1846 |
| Two Brothers ..... | ..... | Schooner | 16 | 1848 |
| Two Elizabeths ..... | ..... | Schooner | 31 | 1838 |
| Two Marys | 49486 | Brigantine... | 204 | 1865 |
| Two Sisters ..... | ..... | Schooner ... | 33 | 1847 |

| Where built. | Registered Owner | Port of Registry. |
|----------------------|-------------------------------------|-------------------|
| La Have | Frederick Gerhart, et al. | Halifax. |
| Truro | Charles Cox, et al. | Halifax. |
| Shelburne | William Muir, et al. | Shelburne. |
| Grand Passage . . . | Charles Innes | Halifax. |
| United States. . . . | Reuben J. Hart, et al | Halifax. |
| Maitland | William McDougal, et al. | Windsor. |
| Long Island. | Edward Spring, et al. | Yarmouth. |
| Ship Harbor | Felix Forest. | Arichat. |
| Barrington | William Whitman, et al | Halifax. |
| L'Ardoise. | P. DeCarteret, et al | Arichat. |
| Yarmouth. | Benjamin Bingay, et al | Halifax. |
| Chester | John Gasper, et al. | Halifax. |
| New Brunswick . . | John Duncan, et al | Halifax. |
| Chezzetcook | William James Munroe. | Halifax. |
| Chester. | John Strachan | Halifax. |
| Lunenburg. | Philip Smellzer, et al. | Halifax. |
| Bay of Islands. . . | John Smith, et al. | Halifax. |
| Lunenburg. | Frederick Publicover, Senr. | Halifax. |
| Chester | Walter Larkin, et al | Halifax. |
| Three Fath. Harbor | Charles E. Wiswell | Halifax. |
| Torbay | Philip Whitman | Halifax. |
| Argyle | William McKinlay | Halifax. |
| Port Gilbert. . . . | Colin Campbell, Junr | Digby. |
| Grand Ruisseau. . . | Joseph Bontier. | Arichat. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|--------------------------|------------------|----------------|-------|-------------|
| Two Sisters | 37764 | Schooner | 14 | 1855 |
| Two Sisters | 42087 | Schooner | 14 | 1862 |
| Two Sisters | 37394 | Schooner | 16 | 1853 |
| Two Sisters..... | ..... | Schooner | 20 | 1819 |
| Two Sisters..... | ..... | Schooner | 16 | 1839 |
| Two Sisters | 48127 | Schooner | 11 | 1864 |
| Two Sisters..... | ..... | Schooner | 42 | 1848 |
| Two Jews..... | ..... | Schooner ... | 20 | 1818 |
| Tyro ... | 38255 | Ship | 795 | 1863 |
| Tyro ..... | ..... | Schooner | 10 | 1849 |
| Tyro | 36322 | Brigantine.... | 135 | 1860 |
| Tyro ..... | ..... | Schooner | 49 | 1844 |
| Tyro | 35827 | Schooner | 41 | 1855 |
| Tyrian | 36179 | Barque..... | 241 | 1858 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|-------------------------------------|-------------------|
| Merigomish | Thomas B. Smith | Pictou. |
| St. Mary's Bay | Dominic M. Doucett, et al | Digby. |
| Strait of Canso . . . | John Balcam | Guysborough. |
| Lunenburg | John Henry Hiltz | Halifax. |
| Mahone Bay | John Hiltz, et al | Halifax. |
| Musquodoboit | John Mosher | Halifax. |
| Chester | John Courteney, et al | Halifax. |
| La Have | Ambrose Allen | Halifax. |
| Beaver River | Freeman Dennis, et al | Yarmouth. |
| Douglass | Charles Taylor | Windsor. |
| Cornwallis | Whitman Holmes | Windsor. |
| Granville | John Greenwood, et al | Halifax. |
| La Have | Henry Smith, et al | Halifax. |
| Barrington | Thomas Crowell | Halifax. |

U.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------|------------------|----------------|-------|-------------|
| Uber | 49016 | Schooner | 103 | 1865 |
| Ulai ..... | ..... | Schooner ... | 75 | 1848 |
| Ulrica | 42072 | Schooner ... | 20 | 1860 |
| Umpire | 50784 | Schooner | 64 | 1865 |
| Uncle Tom | 35939 | Schooner | 66 | 1853 |
| Uncas | 46298 | Brigantine.... | 301 | 1864 |
| Undaunted | 37271 | Brigantine ... | 156 | 1855 |
| Undine | 36013 | Schooner | 27 | 1856 |
| Union | 46297 | Barque | 303 | 1864 |
| Union | 38313 | Schooner | 32 | 1849 |
| Union | 36901 | Barque | 310 | 1861 |
| Union | 35950 | Schooner | 35 | 1844 |
| Union | 37308 | Schooner | 74 | 1856 |
| Union ..... | ..... | Schooner | 20 | 1851 |
| Union | 37057 | Schooner | 36 | 1854 |
| Union | 38467 | Schooner | 29 | 1865 |
| Union ..... | ..... | Schooner | 26 | 1844 |
| Union | 37862 | Brigantine ... | 108 | 1855 |
| Union ..... | ..... | Schooner | 9 | 1832 |
| Union | 36186 | Schooner | 41 | 1833 |
| Union ..... | ..... | Schooner | 41 | 1836 |
| Union ..... | ..... | Schooner | 64 | 1836 |
| Union | 35753 | Schooner | 31 | 1845 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|--------------------------------|-------------------|
| Parrsborough. | Peter Connors, et al. | Parrsborough. |
| Pope's Harbor. | James Furlong, | Halifax. |
| Westport. | Hubbard Davis, | Digby. |
| Barrington. | Warren Doane, et al. | Halifax. |
| Guysborough. | George Duffett, | Halifax. |
| Liverpool. | Jabish Snow, et al. | Liverpool, N. S. |
| Clare. | Anselm F. Corneau, et al. | Digby. |
| Newfoundland. | Henry Heckman. | Halifax. |
| Liverpool. | R. Mulhall, et al. | Liverpool, N. S. |
| Arichat. | Francis LaVashe. | Arichat. |
| Pictou. | Isidore LeBlanc, et al. | Arichat. |
| Mahone Bay. | Thomas Perry. | Shelburne. |
| Clare. | William Meehan, et al. | Digby. |
| Strait of Canso. | John McKenzie. | Guysborough. |
| Essex, U. S. | Benjamin Wier. | Guysborough. |
| River Bourgeois. | G. H. Bissett. | Arichat. |
| Lunenburg. | M. Eisenhauer, et al. | Lunenburg. |
| Cornwallis. | Henry Porter, et al. | Windsor. |
| Yarmouth. | Jonathan Raymond, et al. | Halifax. |
| Lunenburg. | James Croucher. | Halifax. |
| Beaver Harbor. | Benjamin Perry, et al. | Halifax. |
| Argyle. | Joseph Shaw, et al. | Halifax. |
| Lunenburg. | John Smith. | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-------------------------|---------------------|----------------|-------|----------------|
| Union ..... | ..... | Schooner | 30 | 1847 |
| Union | 36160 | Schooner | 22 | 1833 |
| Union ..... | ..... | Schooner | 30 | 1847 |
| Union | 35898 | Brigantine ... | 101 | 1855 |
| Union | 37544 | Schooner ... | 59 | 1817 |
| Union | 42358 | Schooner | 33 | 1838 |
| Unionist | 49108 | Brigantine... | 290 | 1865 |
| Union Jack ..... | ..... | Schooner | 67 | 1834 |
| Union Star | 50776 | Schooner | 33 | 1865 |
| Unice ..... | ..... | Schooner | 25 | 1839 |
| Unicorn ..... | ..... | Schooner | 22 | 1850 |
| Unicorn | 37333 | Schooner | 69 | 1857 |
| Unicorn ..... | ..... | Schooner | 46 | 1834 |
| Unicorn | 36263 | Schooner | 19 | 1860 |
| Unicorn | 52055 | Brigantine ... | 150 | 1865 |
| Unity ..... | ..... | Schooner | 76 | 1842 |
| Unity | 35980 | Schooner | 26 | 1856 |
| Uriel | 37240 | Brigantine... | 85 | 1855 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|--------------------------------------|-------------------|
| La Have | John Romkey, et al. | Halifax. |
| Lunenburg. | Gaspar Glawson, et al. | Halifax. |
| Lunenburg. | Thomas Munro | Halifax. |
| Londonderry | John Cook, et al | Halifax. |
| Chester. | Benjamin Wier. | Halifax. |
| Argyle | William Hutcheson | Halifax. |
| Antigonish | William Thomson, et al | Halifax. |
| Argyle | James White, et al. | Halifax. |
| Torbay. | Joseph Richards, et al. | Halifax. |
| La Have | Frederick Publicover, et al. | Halifax. |
| Londonderry | Allan McLellan | Parrsborough. |
| Granville | John Fullerton, et al. | Digby. |
| Kempt | George Lake, et al | Halifax. |
| Chezzetcook. | Dennis LaPierre. | Halifax. |
| Kempt | Isaac Lake, et al. | Windsor. |
| Sheet Harbor | James McDaniel, et al. | Halifax. |
| Margaree | Belloni White, et al. | Halifax. |
| Clare | Maharine Robichau. | Digby. |

REGISTRY OF NOVA SCOTIA SHIPPING.

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| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
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| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

V.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|-----------------------|------------------|----------------|-------|-------------|
| Valorous | 43068 | Brigantine ... | 191 | 1863 |
| Valiant | 46002 | Schooner ... | 122 | 1863 |
| Valiant | 49425 | Schooner ... | 39 | 1852 |
| Van | 36983 | Schooner ... | 58 | 1860 |
| Vegete | 36991 | Schooner ... | 32 | 1861 |
| Velocity | 42414 | Schooner ... | 71 | 1863 |
| Velocity ..... | ..... | Schooner ... | 57 | 1843 |
| Velocity | 35667 | Schooner ... | 69 | 1852 |
| Venus | 49024 | Brigantine ... | 130 | 1865 |
| Venus | 48631 | Barque..... | 477 | 1863 |
| Venus ..... | ..... | Schooner ... | 35 | 1834 |
| Venus | 38655 | Schooner ... | 117 | Unk'n. |
| Vernant | 38615 | Schooner ... | 17 | 1860 |
| Vermont | 37429 | Schooner ... | 57 | F'gn. |
| Vesta ..... | ..... | Brigantine... | 133 | 1853 |
| Vesta | 48040 | Barque..... | 500 | 1864 |
| Veteran | 41914 | Brigantine... | 177 | 1858 |
| Vetoy | 48079 | Schooner ... | 46 | Unk'n. |
| Victory ..... | ..... | Schooner ... | 33 | 1831 |
| Victory | 42008 | Schooner ... | 9 | 1859 |
| Victory | 36752 | Schooner ... | 13 | 1859 |
| Victory | 37056 | Schooner ... | 37 | 1861 |
| Victory | 36528 | Schooner ... | 73 | 1862 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|--------------------------------|-------------------|
| Pictou..... | James Lorway..... | Pictou. |
| Parrsborough..... | David Willager, et al..... | Parrsborough. |
| Hampden, U. S.... | George Killam..... | Yarmouth. |
| East Medway..... | Christopher Romkey..... | Lunenburg. |
| Lunenburg..... | John Herman..... | Lunenburg. |
| Wallace..... | James Foote, et al..... | Pictou. |
| Ship Harbor..... | George Shelnut..... | Halifax. |
| Pugwash..... | Mary Ann Lyle, et al..... | Halifax. |
| Parrsborough..... | Patrick Blake..... | Parrsborough. |
| Granville..... | John Mills, et al..... | Annapolis. |
| Argyle..... | Mark Armiro, et al..... | Halifax. |
| Parrsborough..... | James Muggah..... | Sydney, C. B. |
| St. Ann's..... | Kenneth Morrison..... | Sydney, C. B. |
| Unknown..... | J. Sheridan, et al..... | Liverpool, N. S. |
| Wallace..... | Amos Purdy..... | Pictou. |
| Granville..... | Robert Mills (2nd), et al..... | Annapolis. |
| Liverpool... .. | C. Gamage, et al..... | Liverpool, N. S. |
| United States..... | Samuel Ryerson..... | Yarmouth. |
| Little Arichat..... | Lewis Terrio, et al..... | Arichat. |
| Beaver River..... | Samuel Hawshan, et al..... | Digby. |
| Sherbrooke..... | Alexander McDonald..... | Guysborough. |
| Guysborough..... | William Malcolm..... | Port Hawkesbury. |
| Margaree..... | James Thompson..... | Port Hawkesbury. |

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|-----------------------|------------------|----------------|-------|-------------|
| Victory ..... | ..... | Schooner | 24 | 1828 |
| Victory ..... | ..... | Schooner | 22 | 1817 |
| Victory ..... | ..... | Schooner .. | 49 | 1833 |
| Victory ..... | ..... | Schooner | 24 | 1839 |
| Victory ..... | .... | Schooner | 21 | 1842 |
| Victory ..... | ..... | Schooner | 34 | 1827 |
| Victory | 36080 | Schooner | 14 | 1844 |
| Victory ..... | ..... | Schooner | 10 | 1845 |
| Victory ..... | ..... | Schooner ... | 23 | 1844 |
| Victory ..... | ..... | Schooner | 16 | 1841 |
| Victory | 33838 | Schooner | 29 | 1842 |
| Victory .. | 37714 | Brigantine... | 130 | 1846 |
| Victoria ..... | .... | Schooner | 14 | 1847 |
| Victoria | 34695 | Schooner ... | 34 | 1839 |
| Victoria | 37282 | Schooner | 28 | 1856 |
| Victoria ..... | ..... | Schooner | 12 | 1837 |
| Victoria | 38175 | Schooner | 107 | 1861 |
| Victoria | 46028 | Ship | 780 | 1862 |
| Victoria .. | 46361 | Brigantine ... | 215 | 1863 |
| Victoria ..... | ..... | Schooner | 71 | 1832 |
| Victoria ..... | ..... | Schooner | 12 | 1838 |
| Victoria ..... | ..... | Schooner | 21 | 1837 |
| Victoria ..... | ..... | Schooner | 15 | 1840 |
| Victoria | 37471 | Schooner | 33 | 1840 |

| Where built. | Registered Owner | Port of Registry. |
|-----------------------------|--------------------------------------|-------------------|
| Louisburg | Adam Thuler. | Halifax. |
| La Have | William Connors, et al. | Halifax. |
| Barrington | Nehemiah Crowell, et al. | Halifax. |
| Mahone Bay | Peter Oxner, et al. | Halifax. |
| Margaree, C. B. | Joseph Henry Harvey. | Halifax. |
| Lunenburg | Jacob Publicover, et al. | Halifax. |
| Pope's Harbor. | Michael O'Leary. | Halifax. |
| Apple River. | John Vaughan. | Halifax. |
| Jeddore | John E. Fairbanks | Halifax. |
| Ship Harbor. | William B. Fairbanks, et al. | Halifax. |
| Cape Breton | Benjamin W. Salter | Halifax. |
| Bay Fortune, P.E.I. | Noel LeBlanc | Pictou. |
| Rocky Bay. | P. LeMarshall. | Arischat. |
| Grand Manan | Andrew Munro | Digby. |
| Petit Passage. | Simoridro Outhouse, et al. | Digby. |
| Lunenburg. | George Hirtle | Lunenburg. |
| Digby. | W. K. Dudman, et al. | Yarmouth. |
| Newport. | George Munroe, et al. | Windsor. |
| Cornwallis | Ezra Churchill, et al. | Windsor. |
| Lunenburg. | John Heckman. | Halifax. |
| Bay of Islands. | John Battist. | Halifax. |
| Lunenburg. | Nicholas Power. | Halifax. |
| Lunenburg. | Jacob Colp. | Halifax. |
| Mahone Bay | George Langille | Halifax. |

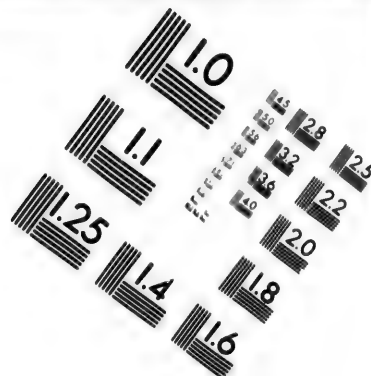
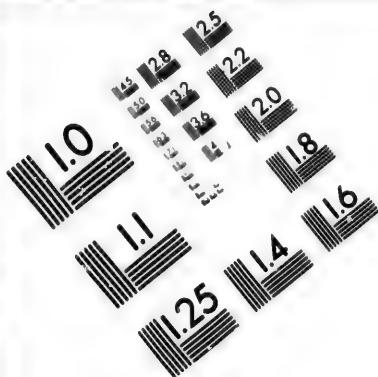
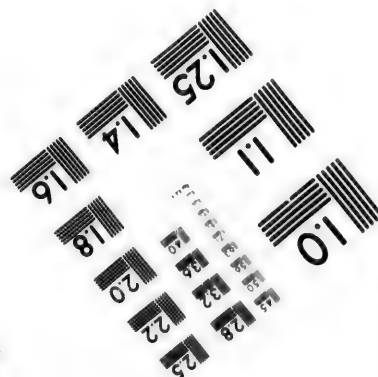
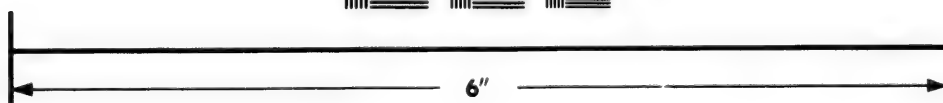
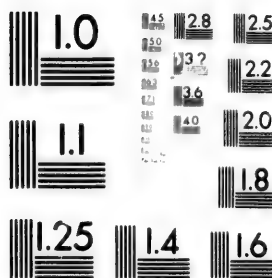


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| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Victoria | | Schooner | 9 | 1842 |
| Victoria ..... | ..... | Schooner | 37 | 1838 |
| Victoria ..... | ..... | Schooner | 33 | 1842 |
| Victoria ..... | ..... | Schooner | 15 | 1843 |
| Victoria | 36096 | Schooner | 41 | 1838 |
| Victoria | 35788 | Schooner .. | 51 | 1838 |
| Victoria | 35816 | Schooner | 37 | 1840 |
| Victoria ..... | ..... | Schooner | 22 | 1839 |
| Victoria | 35935 | Brig | 114 | 1841 |
| Victoria | 36050 | Schooner | 33 | 1857 |
| Victoria | 42286 | Schooner | 24 | 1861 |
| Victoria Ursula | 38277 | Brigantine ... | 205 | 1855 |
| Victor | 38308 | Schooner | 77 | 1853 |
| Victor | 38450 | Schooner | 75 | 1863 |
| Victor ..... | ..... | Schooner | 86 | 1853 |
| Victor | 42513 | Schooner | 131 | 1863 |
| Victor | 41928 | Schooner | 40 | 1859 |
| Victor ..... | ..... | Brig | 121 | 1821 |
| Victorious ..... | ..... | Schooner | 21 | 1824 |
| Vienna | 48104 | Schooner | 22 | 1833 |
| Vigilant | 37623 | Schooner | 55 | 1858 |
| Viking | 43057 | Barque | 598 | 1862 |
| Village Belle | 49027 | Brigantine ... | 174 | 1865 |
| Village Belle | 42222 | Schooner | 20 | 1860 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|---------------------------------------|-------------------|
| Porters Lake | Robert Innes | Halifax. |
| Bridgetown | John Ogilvie | Halifax. |
| Londonderry | James Campbell | Halifax. |
| Chezzetcook | William Frith | Halifax. |
| United States. . . . | Peter Cartile | Halifax. |
| Clyde River. | Thomas Coffee, Senr., et al | Halifax. |
| Pope's Harbor. . . . | Alexander Fraser | Halifax. |
| Halifax. | John Ward | Halifax. |
| Restigouche, N. B.. | William Pryor, et al | Halifax. |
| Spry Harbor | William Leslie | Halifax. |
| Bay of Islands. . . . | William Watt | Halifax. |
| Arichat. | Thomas LeKain | Arichat. |
| Canoe | Michael Boudroit | Arichat. |
| Arichat. | Sebastian Kneo, et al | Arichat. |
| Canoe | Abraham M. Whitman. | Guysborough. |
| La Have | Robert West. | Lunenburg. |
| Liverpool | C. H. Whitman. | Liverpool, N. S. |
| Granville | Dennis Daley | Halifax. |
| St. Mary's | William Rude, et al | Halifax. |
| Essex, U. S | Samuel McDonald | Halifax. |
| La Have | W. & J. Corkum | Lunenburg. |
| New Glasgow | William J. Starrs, et al | Pictou. |
| Parrsborough. | James Gillaspie, et al | Parrsborough. |
| Wilmot. | Gilbert H. Troop | Annapolis. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------|------------------|----------------|-------|-------------|
| Village Belle | 36128 | Schooner | 63 | 1854 |
| Village Belle | 42280 | Brigantine.... | 196 | 1861 |
| Village Belle | 36049 | Schooner | 23 | 1856 |
| Village Belle | 42381 | Brig..... | 199 | 1863 |
| Village Belle | 50782 | Schooner | 27 | 1865 |
| Village Queen | 42792 | Schooner | 54 | Unk'n. |
| Villager | 35894 | Schooner | 65 | 1849 |
| Villetta | 37595 | Schooner | 41 | 1856 |
| Vincent | 38389 | Schooner | 32 | 1859 |
| Vine | 37418 | Schooner | 23 | 1838 |
| Vine ..... | ..... | Schooner | 18 | 1847 |
| Virgin | 37543 | Schooner | 41 | 1848 |
| Virgin | 38325 | Schooner | 17 | 1855 |
| Virginia | 33629 | Schooner | 108 | 1860 |
| Virginia | 46036 | Ship | 934 | 1862 |
| Visit | 36504 | Schooner | 45 | 1859 |
| Vista | 36149 | Schooner | 51 | 1858 |
| Vision | 36148 | Schooner | 63 | 1858 |
| Vivid | 49477 | Schooner | 131 | 1864 |
| Vivid | 37603 | Schooner | 31 | 1853 |
| Vivid ..... | ..... | Schooner | 38 | 1852 |
| Vivid | 36109 | Schooner | 20 | 1851 |
| Vivid | 36197 | Schooner | 75 | 1858 |
| Vivid | 36215 | Schooner | 29 | 1859 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|----------------------------------|-------------------|
| La Have | William Young, et al..... | Halifax. |
| Londonderry | William Douglas, et al | Halifax. |
| Terence Bay | Samuel Shatford | Halifax. |
| Sherbrooke | John Cumming, et al | Halifax. |
| Jeddore | Colin Mitchell | Halifax. |
| United States..... | Edward H. Nickerson, et al | Shelburne. |
| Sheet Harbor..... | William Watt..... | Halifax. |
| Liverpool | George Lonas, et al..... | Lunenburg. |
| Cheticamp | Stanislaus Boudroit | Arichat. |
| Sable River..... | N. Munro..... | Liverpool, N. S. |
| Barrington..... | Joseph Atwood, et al..... | Halifax. |
| P. E. Island..... | David Gromby..... | Arichat. |
| Leonard Passage.. | J. & W. LeVesconte..... | Arichat. |
| Gaspe, C. B. | Alexander McDonald .. | Sydney, C. B. |
| Newport..... | George Armstrong, et al..... | Windsor. |
| Petite Riviere | J. Heminsman..... | Liverpool, N. S. |
| Margaret's Bay... | Patrick Kennedy, et al | Halifax. |
| Ship Harbor..... | William Leahy | Halifax. |
| Bear River..... | H. H. Choete, et al..... | Digby. |
| La Have | G. Shinkel..... | Lunenburg. |
| Petite Reviere.. | Falt & Sperry | Liverpool, N. S. |
| Indian Harbor.... | John R. Webber | Halifax. |
| Sheet Harbor.. | George Hewitt, et al..... | Halifax. |
| La Have | Conrod Romkey..... | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------|------------------|---------------|-------|-------------|
| Vivoce | 46477 | Schooner | 57 | 1864 |
| Vixon | 38618 | Schooner | 43 | 1860 |
| Vixon | 46001 | Sloop | 9 | 1859 |
| Volant | 43071 | Brigantine... | 242 | 1863 |
| Volant | 37006 | Schooner | 43 | 1860 |
| Volant | 37000 | Brigantine... | 121 | 1861 |
| Volant | 37312 | Schooner | 23 | 1846 |
| Volunteer | 36274 | Schooner | 42 | 1854 |
| Volunteer | 42829 | Schooner | 120 | 1861 |
| Volunteer | 46367 | Schooner | 101 | 1863 |
| Volunteer | 37459 | Schooner | 16 | 1861 |
| Vulture | 36328 | Schooner | 16 | 1844 |
| Vulture | 33435 | Schooner | 82 | Unk'n. |
| Vulture ..... | ..... | Schooner | 83 | Unk'n. |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|--------------------------------|-------------------|
| La Have | J. & G. Romkey | Lunenburg. |
| North Bar, C. B. . | Thomas D. Archibald | Sydney, C. B |
| Nappan | Rufus Seaman | Parrsborough. |
| Merigomish | Edward Ganrine | Arichat. |
| Ragged Island.... | Samuel Locke | Shelburne. |
| La Have | T. Churchill | Lunenburg. |
| Shelburne | Samuel Smith, et al. | Halifax. |
| Barrington | Snow & Taylor, et al. | Liverpool, N. S. |
| Cornwallis | William H. Harris, et al. | Windsor. |
| Kempt .. | Nathan Crosby, et al | Windsor. |
| Chester | Francis Munro, et al | Halifax. |
| Barrington | G. Nickerson, et al | Halifax. |
| United States. | Sereil Joice | Halifax. |
| Newburyport, U. S. | Anne Drummmond | Port Hawkesbury. |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

W.

| Vessel's Name. | Official Number. | Rig | Tons. | When built. |
|-----------------------------|------------------|----------------|-------|-------------|
| W. A. Black | 42395 | Brigantine... | 139 | 1863 |
| W. A. Henry | 33400 | Schooner | 96 | 1854 |
| Waity Johnston ..... | ..... | Schooner | 39 | 1826 |
| Wallace ..... | ..... | Brigantine ... | 124 | 1853 |
| Walrus | 46018 | Schooner | 59 | 1863 |
| Walrus | 46658 | Schooner | 93 | 1863 |
| Wanderer | 36986 | Schooner | 30 | 1860 |
| Wanderer | 35636 | Schooner | 54 | F'g'n. |
| Wanderer | 34782 | Brigantine ... | 128 | 1844 |
| Wanderer ..... | ..... | Brig | 192 | 1847 |
| Wanderer | 36530 | Brigantine ... | 128 | 1862 |
| Wanderer | 48453 | Brigantine ... | 179 | 1864 |
| Warrior | 43099 | Barque. | 616 | 1864 |
| Ward..... | ..... | Schooner | 30 | 1840 |
| Wasp | 35681 | Schooner | 46 | 1853 |
| Wasp | 34772 | Schooner | 39 | 1849 |
| Wasp..... | ..... | Schooner ... | 20 | 1827 |
| Watchmate | 48033 | Brigantine ... | 152 | 1863 |
| Water Lilly ..... | ..... | Schooner | 21 | 1839 |
| Water Lilly ..... | ..... | Schooner | 49 | 1838 |
| Waterloo ..... | ..... | Brigantine ... | 72 | Unk'n. |
| Waterloo .. | ..... | Schooner | 35 | 1834 |
| Wavelet | 49119 | Barque. | 335 | 1865 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|-------------------------------------|-------------------|
| Londonderry | George N. McLellan, et al | Halifax. |
| Antigonish | Marine Boudrot, et al | Halifax. |
| Granville | John Johnston, et al | Halifax. |
| Wallace | Archibald Scott, et al | Pictou. |
| Parrsborough. . . . | David M. Pettis, et al | Parrsborough. |
| Granville | Asa Porter, et al | Digby. |
| La Have | H. Slawenwhite, et al | Lunenburg. |
| Unknown | J. Harding, et al | Liverpool, N. S. |
| Cornwallis | Ezra Churchill, et al | Windsor. |
| Clare | Thomas W. Wilson, et al | Halifax. |
| Low Point. | Reuben I. Hart, et al | Halifax. |
| Maitland | George Smith, et al | Windsor. |
| River John | James Kitchen | Pictou. |
| Barrington | Joshua Smith, et al | Halifax. |
| New Dublin | H. B. Sarty | Lunenburg. |
| P. E. Island | James Curry | Windsor. |
| Unknown | Joseph Kendrick, et al | Halifax. |
| Clements | John K. Bent | Halifax. |
| Lunenburg | James Henneberry | Lunenburg. |
| Shelburne | Samuel Balcom | Halifax. |
| Unknown | Snow, Parker, et al | Halifax. |
| Ship Harbor | George Glawson | Halifax. |
| Merigomish | David Patterson, et al | Pictou. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------|------------------|----------------|-------|-------------|
| Wave ..... | ..... | Schooner | 100 | 1849 |
| Wave | 35731 | Schooner | 41 | 1853 |
| Wave | 46091 | Schooner | 15 | 1862 |
| Wave | 37249 | Schooner | 45 | 1834 |
| Wave | 42105 | Schooner | 33 | 1860 |
| Wave ..... | ..... | Schooner | 19 | 1836 |
| Wave ..... | ..... | Schooner | 11 | 1845 |
| Wave | 35737 | Schooner | 62 | 1852 |
| Wave | 37941 | Schooner | 37 | 1852 |
| W. B. Forest | 48471 | Brigantine ... | 147 | 1865 |
| W. B. King ..... | ..... | Schooner | 41 | 1853 |
| W. D. Bickford | 42023 | Schooner | 43 | 1860 |
| W. D. Bickford | 38451 | Schooner | 130 | 1863 |
| W. D. Smith | 36524 | Schooner | 67 | 1861 |
| Weazel ..... | ..... | Schooner | 8 | 1838 |
| Weazel ..... | ..... | Schooner | 32 | 1819 |
| Weeping Willow | 36005 | Schooner | 16 | 1855 |
| Welcome ..... | ..... | Schooner | 12 | 1828 |
| Welcome Home | 46005 | Schooner | 107 | 1863 |
| Welcome Return | 36515 | Schooner | 24 | 1846 |
| Welcome Return ... .. | ..... | Schooner | 69 | 1845 |
| Welcome Return ..... | . . . . | Schooner | 48 | 1848 |
| Welcome Return ..... | ..... | Schooner | 19 | 1840 |
| Wellington ..... | ..... | Schooner | 42 | 1829 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------------|--------------------------------------|-------------------|
| Antigonish | John McKinnon | Pictou. |
| Mahone Bay | I. & W. LeVesconte | Arichat. |
| Jordan River | William Thorburn | Shelburne. |
| Clements | Frederick W. Ruggles, et al. | Digby. |
| Noel | Andrew G. O'Brien, et al. | Windsor. |
| Ragged Islands | Joshua Chadsey, et al | Halifax. |
| Musquodoboit | Peter Duval, et al. | Halifax. |
| St. Mary's | Joseph Evans | Halifax. |
| Yarmouth | John J. Wooden | Halifax. |
| Cornwallis | Elijah N. Forsythe, et al. | Windsor. |
| Parrsborough | Charles Brewster | Parrsborough. |
| Hillsburgh | James A. Steeves | Digby. |
| Little Arichat | Victor Landry | Arichat. |
| Mabou | C. D. Grant | Port Hawkesbury. |
| Brier Island | Matthias Byrns | Halifax. |
| Lunenburg | John Walsh, et al. | Halifax. |
| New Harbor | Andrew Sangster, et al. | Halifax. |
| Londonderry | David Lowden, et al. | Halifax. |
| Parrsborough | Charles Hatfield, et al. | Parrsborough. |
| Port Herbert | J. McDonald, et al | Liverpool, N. S. |
| St. Mary's | John Hewit, et al. | Halifax. |
| Mahone Bay | Rupert G. Zwicker | Halifax. |
| Barrington | Job Atkinson | Halifax. |
| New Edinburg | Thomas Fanning | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------------|------------------|---------------|-------|-------------|
| Wellington ..... | ..... | Schooner | 47 | 1828 |
| Wellington | 48476 | Schooner ... | 94 | 1865 |
| Wentworth | 38344 | Schooner | 26 | 1848 |
| Wentworth | 48085 | Barque..... | 678 | 1863 |
| Wentworth | 36600 | Schooner | 100 | 1859 |
| Wentworth ..... | ..... | Schooner | 52 | 1836 |
| West Wind | 49008 | Barque..... | 287 | 1864 |
| Western Packet | 33794 | Schooner | 68 | 1853 |
| Weymouth ..... | ..... | Schooner | 22 | 1838 |
| W. Greenough | 43077 | Brigantine... | 252 | 1863 |
| W. H. Bigelow | 46008 | Brig | 280 | 1863 |
| W. H. Harris | 46026 | Brigantine... | 190 | 1862 |
| W. H. Townsend | 38075 | Brig | 218 | 1858 |
| Whitehall | 49022 | Barque..... | 422 | 1865 |
| White Squall | 37386 | Brigantine... | 151 | 1856 |
| White Wing | 43070 | Barque..... | 300 | 1863 |
| Who 'd have thought it. ..... | ..... | Schooner | 13 | 1827 |
| Wide Awake | 43066 | Schooner | 18 | 1863 |
| Wild Flower | 52056 | Schooner | 141 | 1865 |
| William ..... | ..... | Schooner | 21 | 1850 |
| William ..... | ..... | Schooner | 27 | 1851 |
| William ..... | ..... | Schooner | 40 | 1846 |
| William ..... | ..... | Schooner | 19 | 1816 |
| William ..... | ..... | Schooner | 30 | 1814 |

| Where built. | Registered Owner. | Port of Registry. |
|-----------------------|--------------------------------|-------------------|
| La Have | Edward Mullock | Halifax. |
| Canning | Wellington Eagles, et al | Windsor. |
| Poulamond | Edward Porrier, et al | Arichat. |
| Weymouth | Dennis Horton, et al..... | Yarmouth. |
| Newport..... | Samuel Sweet, et al... .. | Windsor. |
| Yarmouth..... | Ansel Crossby, et al | Halifax. |
| Minudie | Alexander McFarlane..... | Parrsborough. |
| Antigonish | James McPherson..... | Halifax. |
| Weymouth | Elias Gates | Halifax. |
| New Glasgow ... | John R. Noonan, et al.... | Pictou. |
| Parrsborough.... | W. H. Payzant, et al .. | Parrsborough. |
| Cornwallis | Ebenezer Bigelow, et al | Windsor. |
| Digby | W. H. Townsend..... | Yarmouth. |
| Parrsborough.... | James Merriam, et al..... | Parrsborough. |
| St. Mary's | Malcolm Livingston | Guysborough. |
| Pictou..... | Daniel Ward Job.... | Pictou. |
| Barrington | Nehemiah Crowell..... | Halifax. |
| River John | Thomas McDonald..... | Pictou. |
| Walton | Francis Parker, et al..... | Windsor. |
| River Bourgeois .. | G. E. Bissett..... | Arichat. |
| P. E. Island..... | Thomas LeNoir..... | Arichat. |
| Murray Har., P. E. I. | Alexander Russell, et al | Pictou. |
| Digby..... | Silas Rice..... | Halifax. |
| Digby..... | Walter Mills | Halifax. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|------------------------------------|------------------|----------------|-------|-------------|
| William..... | ..... | Schooner ... | 39 | 1818 |
| William..... | ..... | Schooner ... | 36 | 1816 |
| William..... | ..... | Schooner ... | 35 | 1832 |
| William..... | ..... | Schooner ... | 95 | 1835 |
| William..... | ..... | Schooner ... | 49 | 1820 |
| William..... | ..... | Schooner ... | 28 | 1836 |
| William..... | ..... | Schooner ... | 19 | 1823 |
| William | ... | Schooner ... | 12 | 1834 |
| William | 39015 | Schooner ... | 30 | 1843 |
| William | 49423 | Schooner ... | 51 | 1833 |
| William Aikins | 36244 | Brigantine ... | 107 | 1859 |
| William Henry | 43048 | Schooner ... | 34 | 1862 |
| William Henry | 37260 | Brigantine ... | 68 | 1845 |
| William Henry..... | ..... | Schooner ... | 33 | 1830 |
| William Henry..... | ..... | Schooner ... | 10 | 1845 |
| William Henry | 37449 | Schooner ... | 29 | 1850 |
| William Henry ... | 35811 | Schooner ... | 28 | 1853 |
| William James..... | ..... | Schooner ... | 49 | 1827 |
| William H. Jenkins | 49404 | Barque..... | 721 | 1864 |
| William Kandick | 68644 | Schooner ... | 53 | 1863 |
| William Muir | 52001 | Brigantine ... | 191 | 1864 |
| William & Mary | 38337 | Schooner ... | 36 | 1856 |
| William & Margaret.... | ..... | Schooner .. | 26 | 1814 |
| William McKeen | 38339 | Schooner ... | 76 | 1856 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------------|------------------------------------|-------------------|
| P. E. Island | William Mortimer | Halifax. |
| Lunenburg | James Bond | Halifax. |
| Brier Island | William Potter | Halifax. |
| Beaver Harbor | Benjamin DeWolf | Halifax. |
| New Brunswick | William Nisbitt | Halifax. |
| La Have | William McKaine, et al | Halifax. |
| Pope's Harbor | William Abrill | Halifax. |
| Digby | Joseph W. E. Darby | Halifax. |
| Sable River | William Manning | Halifax. |
| Portsmouth, U. S. | Jedediah Goodwin, et al | Yarmouth. |
| Five Islands | Joseph Doyle, et al | Halifax. |
| Tracadie | William Delorey, et al | Pictou. |
| Digby | Charles Morehouse, et al | Digby. |
| New Brunswick | Hossadier Locke | Halifax. |
| Cornwallis | John Robinson | Halifax. |
| Barrington | William M. Clough | Halifax. |
| Ship Harbor | John Poole | Halifax. |
| Sable River | Robert Currie, et al | Halifax. |
| Digby | Young & Baker | Yarmouth. |
| Forchie, C. B | Josiah Hooper, et al | Sydney, C. B. |
| Shelburne | William Muir, et al | Shelburne. |
| Strait of Canso | William Philpot | Port Hawkesbury. |
| Unknown | James McKenzie | Halifax. |
| Mabou | William Graham | Arichat. |

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|----------------------------------|------------------|----------------|-------|-------------|
| William McKeen | 42501 | Schooner | 39 | 1862 |
| William Nash | 48455 | Brigantine ... | 124 | 1864 |
| William Nelson..... | ..... | Schooner | 24 | 1843 |
| William Noble | 37381 | Schooner .. | 20 | 1853 |
| William H. Prentice | 37283 | Schooner | 91 | 1849 |
| William G. Putnam | 46027 | Barque.... | 716 | 1862 |
| William A. Rogers | 38231 | Brigantine.... | 162 | 1862 |
| William Rennie | 42799 | Barque | 271 | 1862 |
| William & Sarah ..... | ..... | Schooner | 31 | 1834 |
| William Taylor | 36510 | Schooner . . | 45 | 1860 |
| Will o' the Wisp | 37684 | Schooner | 26 | 1862 |
| Will o' the Wisp | 37613 | Schooner | 110 | 1858 |
| Wilberforce ..... | ..... | Brigantine ... | 102 | 1849 |
| Wild Hunter | 4600 | Schooner | 103 | 1864 |
| Wild Hunter | 41910 | Brigantine.... | 182 | 1861 |
| Wild Hunter | 41910 | Brigantine ... | 181 | 1861 |
| Wild Hunter | 48467 | Barque..... | 338 | 1864 |
| Wild Horse | 22897 | Barque..... | 308 | 1854 |
| Wild Horse | 48450 | Schooner | 165 | 1864 |
| Wild Rose | 37552 | Schooner | 36 | 1858 |
| Wild Wave | 41017 | Schooner | 17 | 1860 |
| Wild Wave | 41929 | Schooner | 35 | 1859 |
| Wilmot Jane | 41905 | Schooner | 21 | 1853 |
| Wilmot Packet | 34769 | Schooner | 32 | 1846 |

| Where built. | Registered Owner. | Port of Registry. |
|---------------------|--------------------------------|-------------------|
| La Have | John McKeen, et al..... | Lunenburg. |
| Cornwallis | William Burgess..... | Windsor, |
| Pugwash | Abraham Vanbuskirk | Pictou. |
| Torbay..... | Robert Noble..... | Halifax. |
| Granville..... | Joshua Crosseup .. | Digby. |
| Maitland..... | William Lawrence, et al..... | Windsor. |
| Shelburne..... | William Rogers..... | Yarmouth. |
| Shelburne..... | William A. Decker, et al..... | Shelburne. |
| Barrington..... | Donald Campbell, et al..... | Halifax. |
| Liverpool.... | William Watt | Halifax. |
| Five Islands | John Roderick, et al | Parrsborough. |
| Lunenburg | John Lithgow | Halifax. |
| Cornwallis | George Young..... | Halifax. |
| Parrsborough.... | J. H. B. Elderkin, et al..... | Parrsborough. |
| Strait of Canso ... | Benjamin Wier, et al | Port Hawkesbury. |
| Port Hawkesbury.. | Thomas C. Kinnear, et al..... | Halifax. |
| Windsor | William Fitch, et al | Windsor. |
| Cornwallis | Ezra Churchill, et al..... | Windsor. |
| Maitland..... | John McCumber | Windsor. |
| Green Harbor | J. H. Dowling, et al..... | Liverpool, N. S. |
| Granville | Jacob Christopher, et al | Digby. |
| Liverpool.... | Parker & Hemeon..... | Liverpool, N. S. |
| Strait of Canso.... | John Williams..... | Port Hawkesbury. |
| Wilmot | Andrew Harris | Halifax. |

| Vessel's Name. | Official Number. | Rig | Tons. | When built. |
|----------------------------|------------------|----------------|-------|-------------|
| Wilmington | 42108 | Brigantine... | 227 | 1860 |
| Wimburn | 49093 | Barque..... | 489 | 1864 |
| Wintaramorsh ..... | ..... | Schooner | 127 | 1852 |
| Winter Bird | 48254 | Schooner | 84 | 1864 |
| Winslow Locke | 37451 | Brigantine ... | 75 | 1855 |
| Windsor | 34865 | Schooner | 108 | 1849 |
| W. N. Z. | 46490 | Brigantine ... | 160 | 1865 |
| Wonder | 41759 | Schooner | 30 | 1858 |
| Woodbine . | ..... | Brigantine ... | 114 | 1838 |
| Woodbine ..... | ..... | Brig | 145 | 1839 |
| W. S. McLeod | 36291 | Schooner | 34 | 1860 |
| W. Stairs | 41921 | Schooner | 89 | 1859 |

| Where built. | Registered Owner. | Port of Registry. |
|--------------------|---------------------------------|-------------------|
| Newport..... | Nelson Woolaver, et al..... | Windsor. |
| New Glasgow | James W. Carmichael, et al..... | Pictou. |
| Cornwallis | Charles W. Barteaux | Digby. |
| Shelburne..... | Joshua Snow, et al. | Shelburne. |
| Ragged Islands ... | L. P. Churchill | Liverpool, N. S. |
| Advocate Harbor.. | John Alexander, et al..... | Windsor. |
| Lunenburg..... | W. N. Zwicker, et al..... | Lunenburg. |
| P. E. Island..... | John Garrett | Halifax. |
| Barrington..... | Obadiah Wilson, Junr..... | Halifax. |
| Yarmouth..... | Joseph Shaw, et al..... | Halifax. |
| Liverpool | Samuel Perry, et al..... | Halifax. |
| Vogler's Cove | William Pryor, et al..... | Halifax. |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |

Y.

| Vessel's Name. | Official Number. | Rig. | Tons. | When built. |
|--------------------------------|---------------------|---------------|-------|----------------|
| Yarmouth Packet ..... | ..... | Schooner | 45 | 1834 |
| Youth ..... | ..... | Schooner . . | 16 | 1848 |
| Young Henry ..... | ..... | Schooner | 51 | 1848 |
| Young Novascotian | 36982 | Schooner | 66 | 1860 |

| Where built. | Registered Owner. | Port of Registry. |
|-------------------|--------------------------|-------------------|
| Yarmouth..... | Joseph Tooker, Junr..... | Halifax. |
| Barrington..... | Paul Crowell..... | Halifax. |
| Margaree | Michael Galland | Arichat. |
| East Medway. | James Ruser, et al..... | Lunenburg. |

Z.

| Vessel's Name. | Official Number. | Rtg. | Tons. | When built. |
|----------------------|------------------|----------------|-------|-------------|
| Zaidee | 51777 | Schooner | 55 | 1865 |
| Zambezi | 36449 | Schooner | 31 | 1860 |
| Zambesi | 38601 | Brig | 199 | 1858 |
| Zealous | 37277 | Schooner | 53 | 1835 |
| Zebra | 36477 | Schooner | 141 | 1859 |
| Zebra .... | ..... | Schooner | 31 | 1841 |
| Zebra | 37775 | Brigantine ... | 100 | 1847 |
| Zebra ..... | ..... | Schooner | 20 | 1850 |
| Zebulum | 36609 | Brigantine ... | 136 | 1860 |
| Zeland | 37262 | Schooner | 60 | 1849 |
| Zenobia | 35729 | Schooner | 47 | 1850 |
| Zephyr | 34708 | Schooner | 22 | 1851 |
| Zephyr ..... | ..... | Schooner | 39 | 1838 |
| Zephyr ..... | ..... | Schooner | 47 | 1841 |
| Zephyr | 50755 | Brig | 293 | 1864 |
| Zero | 42117 | Brigantine ... | 194 | 1861 |
| Zillah | 41780 | Schooner | 23 | 1858 |
| Zone | 42335 | Schooner | 28 | Unk'n. |
| Zorilla | 36075 | Schooner | 40 | 1856 |
| Zulicka | 42355 | Brig | 195 | 1855 |
| Zilpha | 50777 | Schooner | 52 | 1865 |

| Where built. | Registered Owner. | Port of Registry. |
|------------------------|------------------------------------|-------------------|
| Liverpool, N. S. . . . | J. F. Forbes | Liverpool, N. S. |
| Jordan River. . . . | Bradford Page. | Shelburne. |
| North Sydney, C.B. | Thomas D. Archibald. | Sydney, C. B. |
| Granville | George B. Potter, et al. | Digby. |
| La Have | L. Rothenhizer, et al. | Lunenburg. |
| La Have | Henry Smith, et al | Halifax. |
| Shelburne. | Isaac Dart | Halifax. |
| La Have | William B. Hamilton. | Halifax. |
| Windsor. | Benjamin Sweet | Windsor. |
| Argyle | William Maher, et al | Digby. |
| La Have | McConnell & Martin. | Liverpool. |
| Hillsburgh | George Newcomb, et al | Digby. |
| Argyle | Augustus Vernan | Halifax. |
| Parrsboro. | William Rolfe, et al | Halifax. |
| Maitland. | George McCulloch | Halifax. |
| Clifton | Matthew C. Norris, et al | Windsor. |
| La Have | J. T. Gieser | Lunenburg. |
| Unknown. | Peter Ross | Halifax. |
| Shelburne. | Jacob Hewitt, et al. | Halifax. |
| Richmond, U. S. . . | Charles Hall | Halifax. |
| Petite Reviere. . . . | Judah Crowell. | Halifax. |

| Where built. | Registered Owner. | Port of Registry. |
|--------------|-------------------|-------------------|
| | | |